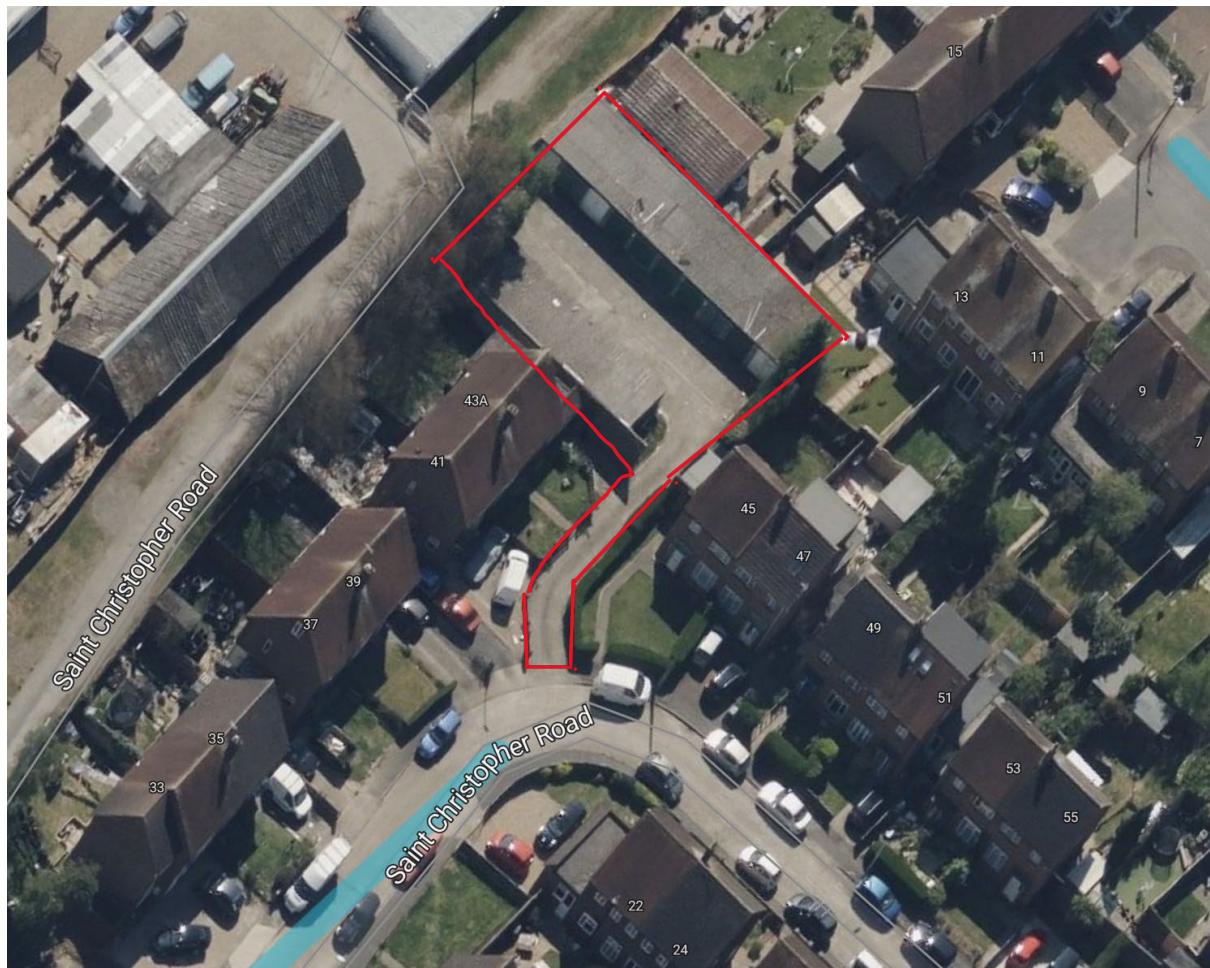




## HILLINGDON LONDON

### DESIGN AND ACCESS STATEMENT



**For Residential Development At:  
Garages Site at St Christopher Road  
Uxbridge  
UB8 3SG**

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## 1.0 INTRODUCTION

### 1.1 The Statement:

This Design and Access Statement is intended to support a full planning application for redevelopment of a disused garage site at St Christopher Road, Uxbridge UB8 3SG

The proposal is to construct 2 x 3 bedroom 6 person, 2 storey semi-detached houses with all associated parking spaces, private garden amenity, hard and soft landscaping and external works.

The site is principally a rectangular shape with an offshoot access (vehicular and pedestrian) off St Christopher Road.

### 1.2 Background:

The existing use comprises of 2 rows of lock-up garages accommodating total of 20 garages. The garages were part of the housing estate to accommodate residents' cars. Movement of 20 cars through single width access road. Over the time the garages have been unoccupied with residents preferring to park their vehicles within their front garden forecourt or off street. Most of the garages are in poor condition and attracting anti-social behavior and fly tipping. The garages formerly rented were used mainly for general storage.

The east side of the plot is flanked by Manor Farm and Livery. The rest of the site is surrounded by two storey housing, some with dormers.

Planning History- Except for the current use as garages site there are no known previous planning applications for this site.

### 1.3 Drawing List:

This Design and Access Statement should be read in conjunction with the following drawing numbers:

DRAWING NOS	DRAWING TITLE
2025/D/353/P/01	Site Location Plan
2025/D/353/P/02 rev A	Existing Site Plan
2025/D/353/P/03 rev A	Proposed Site Plan
2025/D/353/P/04 rev A	Proposed Plans
2025/D/353/P/05 rev A	Proposed Elevations
2025/D/353/P/06 rev A	Proposed Section

### 1.4 Specialist Statements:

This Design and Access Statement is supported with three specialist statements as follows:

- a) Accessibility Statement (4.0)
- b) Secure by Design Statement (5.0)
- c) Transport and Pedestrian Statement (6.0)

- d) Biodiversity Net Gain (BNG) – under separate report with planning application.

## 2.0 SITE ASSESSMENT

### **PHYSICAL CHARACTERISTICS:**

#### 2.1 **Site Location:**

The site is a rectangular shaped plot located off St Christopher Road between houses 43 and 45, and 13 and 15 St Lukes Close. The development plot is defined by red line shown in Fig 1 – Location Plan Ref: 2025/D/353/P01

Garages Site at St Christopher Road, Uxbridge. UB8 3SG

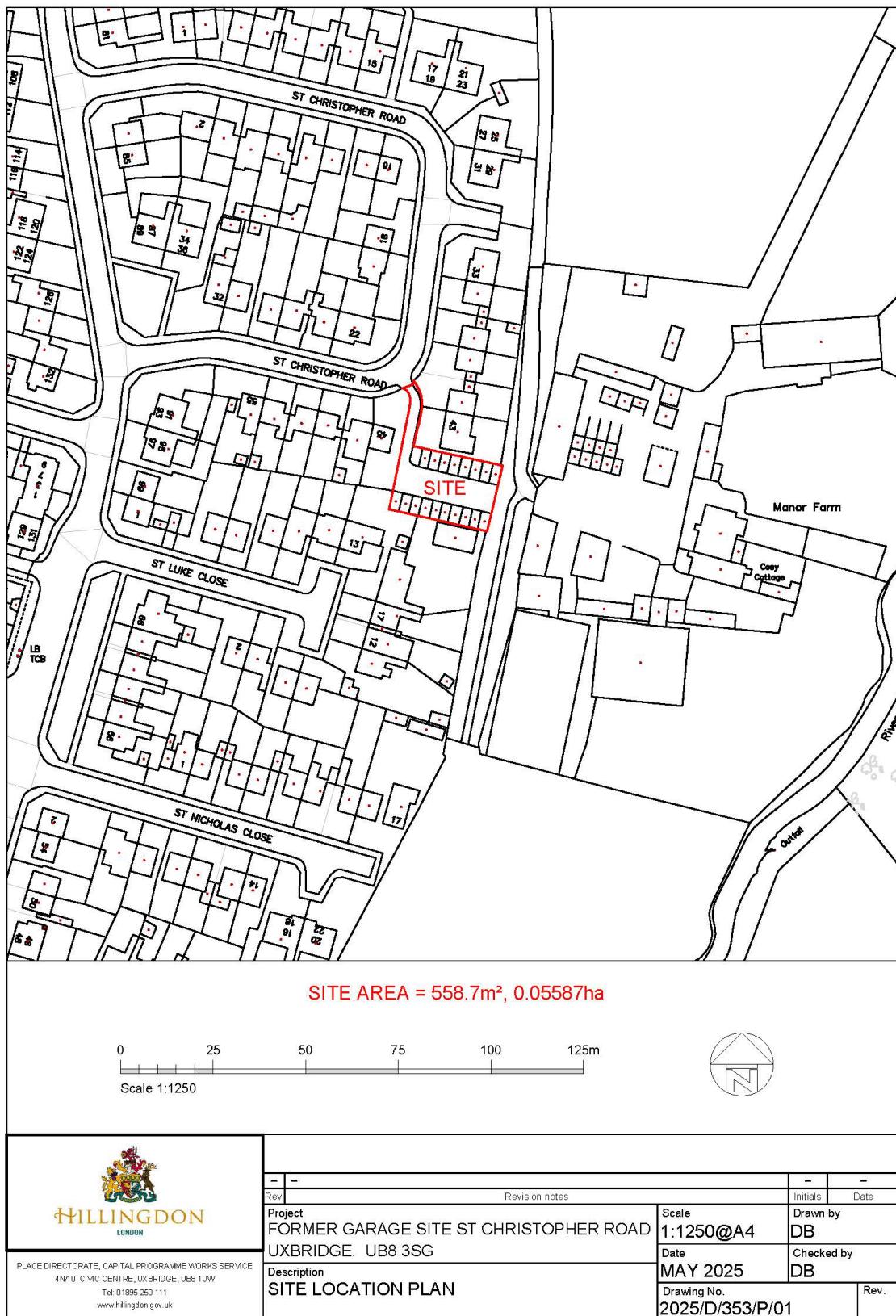
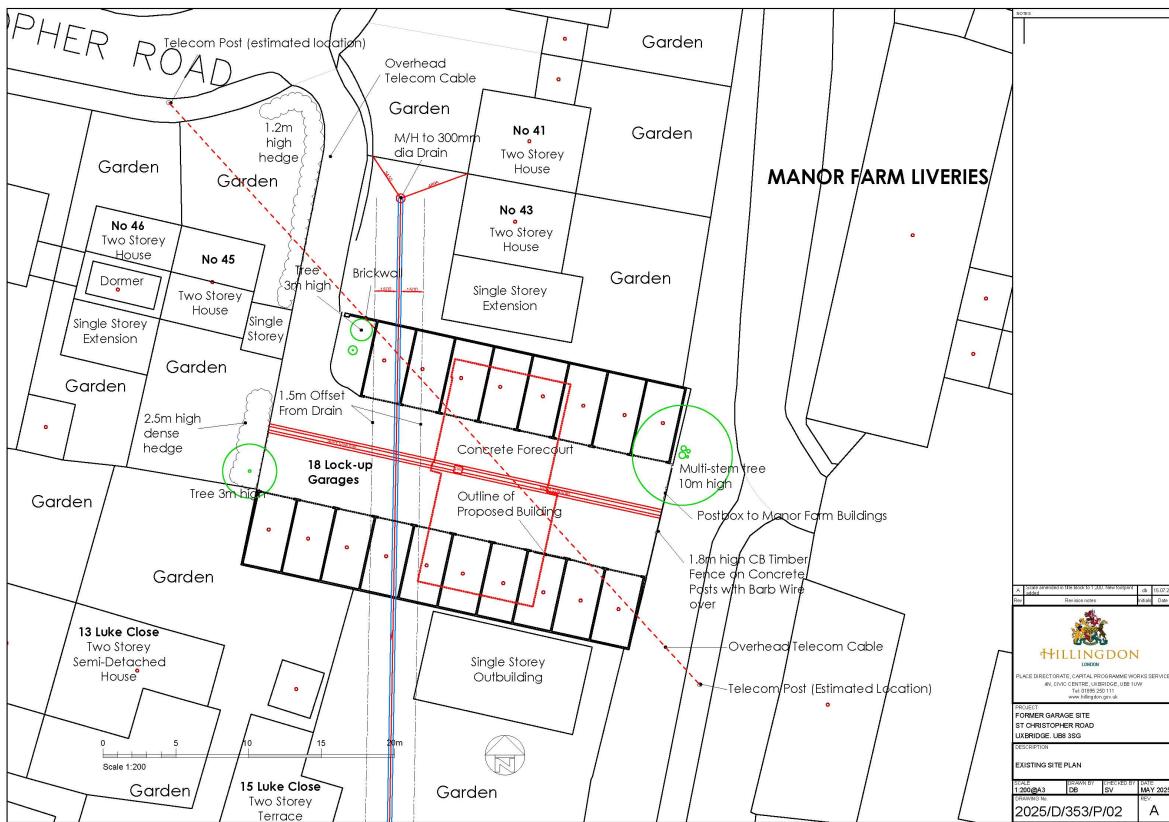


Fig 1 – Location Plan Ref:2025/D/353/P01



**Fig 2 – Existing Site Plan Ref:2025/D/353/P02 rev A**

## 2.2 Surrounding Land Uses:

The site is located within predominately residential area.

The site located along a well-established traditional residential street with modest sized front gardens with hedges, soft planting, and some with off street parking.

The residential character of the surrounding area is made up of semi-detached houses with gable roofs in traditional construction.

The land eastwards is flanked by Manor Farm and Liveries

The site is ideally located near Cowley High Street for local shopping and other facilities and Uxbridge Town centre is located approximately 2.2km.

## 2.3 Movement:

Vehicular and pedestrian access to the new development will remain as existing but with alterations to surface road treatment.

St Christopher Road comprising of road and footpaths on both sides is adopted highway maintained by the Council and is not designated as "classified" road according to the Highways listing of roads.

The following elements are used to define the parking/movement strategy:

1. The parking layout is designed as allocated spaces to meet the adopted parking standards for the residential developments.
2. Car parking space are designed to be adaptable DDA compliant car parking space for owners with mobility issues.
3. Each parking bay can accommodate one standard size car but is sized at a non-standard 2.5m wide bay.

Council operates household and garden waste collections on designated days and the arrangements will remain as at present for the new development shown on 2025/D/353/P03 rev A. Residents are expected leave their waste bags and sacks at the end of the path for weekly collection.

Permeable paving has been colour contrasted to zone pedestrian route from and parking spaces.

The new development will have individual bike stores located in back gardens. These bike stores will be to secure by design requirements and standards.

The nearest bus stops to are located approximately 650m away on High Road which leads to Heathrow and Uxbridge including other towns. Modern buses incorporate wheelchair access.

### **3.0 DESIGN**

#### **3.1 Use:**

##### **Existing:**

The site accommodates 18 lock-up garages, 8 garages on the north and 10 on the south side, and are vacant compounding to inefficient use of a valuable plot within the residential area. The site is prone to antisocial behaviour including vandalism and fly tipping

Access to the garages is via a single width access road of 18m length, but drivers can see approaching vehicle easily from each end.

The garages are in poor condition and over the years the demand for such garages have declined. The residents prefer to park their cars near to the house on the street or in the front forecourt of their house.

The site has been identified as a potential redevelopment land for much needed housing in the borough.

There site have overhead cables including underground Thames Water surface water sewer running across the site.

##### **Proposed:**

The proposal is to develop the plot to provide 2 x 3-bedroom 6-person semi-detached houses in a form of a 2-storey block in keeping with the residential nearby street scene.

The proposal accords with the Government's commitment to speed up the delivery of new housing across the country on previously developed sites. The Government's ambition is to increase housing density by making the most of the use of the land. The demand for such dwellings that can accommodate

families is high and, in the locality, there are number of nearby schools: Cowley St Laurence & Children's Centre, Rabbsfarm School and Whitehall Infant and Junior school.

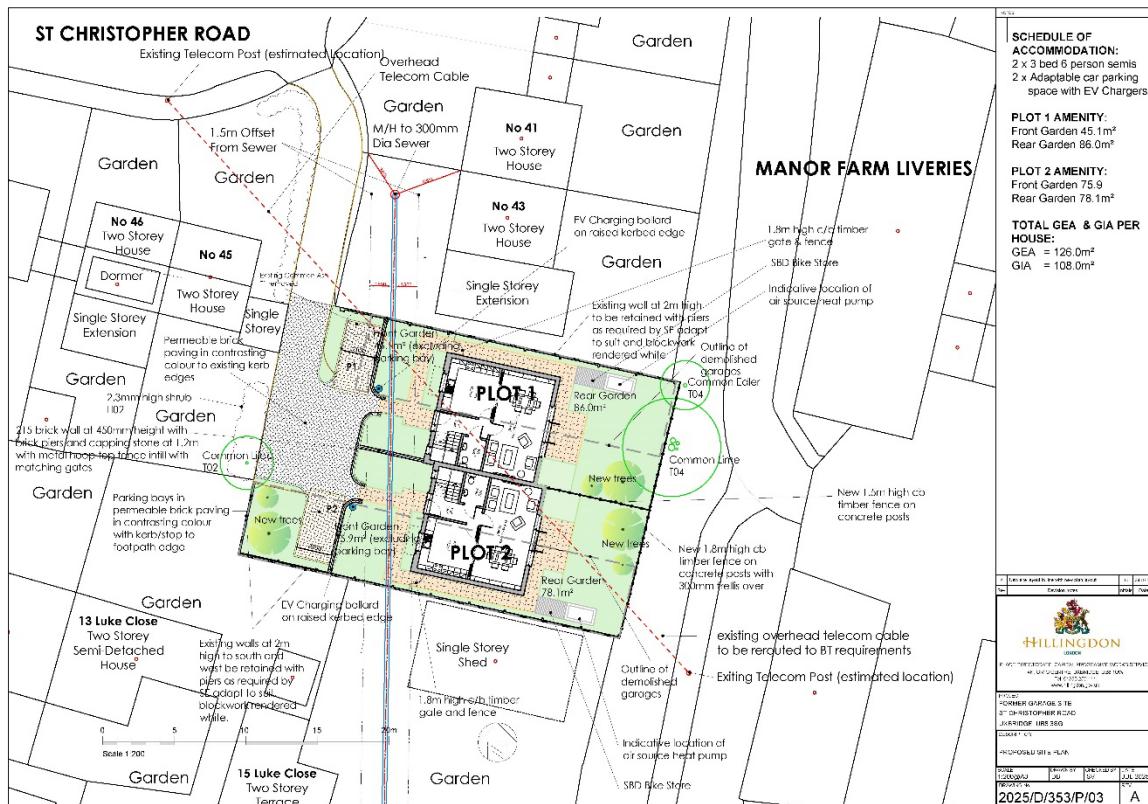


Fig 3 – Proposed Site Plan Ref: 2022/D/353/P/03 rev A

### 3.2 Amount:

The total site area is 0.05587 hectares.

The overall gross internal floor area of each flat is given in the table below: -

House Number	Bedrooms	Parking Spaces	Gross Internal Floor area (per unit) in m <sup>2</sup>	Private Rear Garden in m <sup>2</sup>	Remarks
Plot 1	3 Bed, 6 person	1	108.0	86.0	1 car parking space could be DDA adaptable
Plot 2	3 Bed, 6 person	1	108.0	75.9	1 car parking space could be DDA adaptable

The GIA of the proposed houses meets the space standards as set out by the Mayors London Plan and falls between minimum and best practice.

Total of private amenity for each house is shown in the table above. With regards London Borough of Hillingdon's local amenity standards Plot 1 and Plot 2 is fully compliant.

### 3.3 Layout:

The site layout is dictated by the shape and context of the existing site surroundings and typology of two storey houses and flats. The key constraint is the 300mm diameter Thames Water (TW) sewer, 2.5m deep, running across the site. Guidance given by TW has been followed to keep new build 1.5m away from the sewer for future maintenance and repairs.

The most efficient use of the plot is to have a minimum area for car parking at front, simple access to the houses, sympathetic to the existing building line, deep sewer and provide maximum garden area suitable for family houses. The site is essentially tucked away off the corner of the road, adjacent to no 43 and side of no 45.

The new build does not conflict with any planning restrictions in terms of scale and proportion and where required to meet planning guidance, these have been overcome by design solutions.

Overlooking from the front elevation at first floor level has been dealt with primarily using obscured glazing to a height of 1.7m above finished floor.

Additionally, the layout also coordinated with the Mayor of London's London Plan

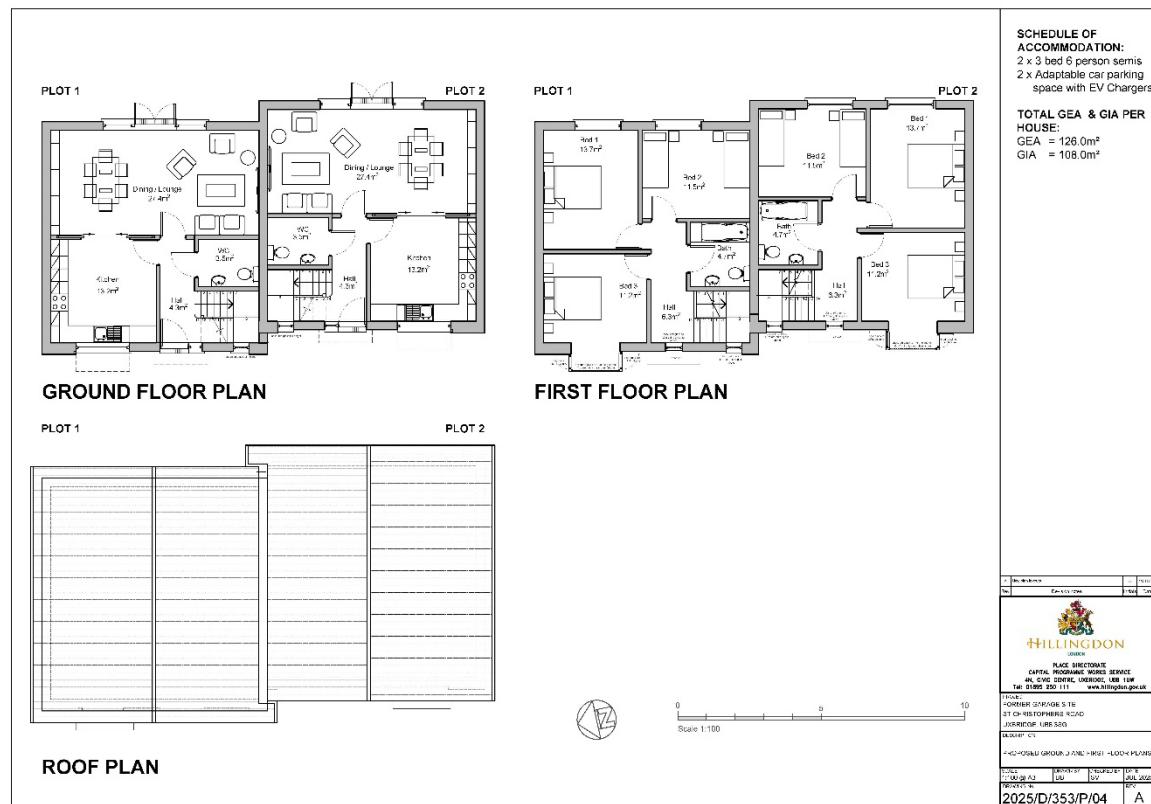


Fig 4 – Proposed Floor Plans Ref:2025/D/353/P04 rev A.

### **3.4 Scale:**

The scale of the development is in character with the vicinity of the area and would not be visually obstructive, nor detrimental to the visual amenities of the neighbouring residents. The proposed development is 2 storeys, similar to the houses on either side and directly opposite. The overall mass of the building matches the scale and language of existing neighbouring houses.

The overall impact of the development will not adversely affect the adjacent houses or surrounding. The development will also alleviate the antisocial behaviour and fly-tipping currently experienced there now.

### **3.5 Landscaping:**

The existing concrete garage court has one small tree (T1 grade C1), and the site is not known to be within Tree Preservation order zone. There is a small grass area on one side of the access into the site.

The proposal incorporates as much soft planting as possible at front around the parking spaces, trees and grass are also planted in front and rear gardens. The planting will be further enhanced following the completion of Biodiversity Net Gain (BNG) reports recommendations.

Areas of hard landscaping have been kept to a minimum and range of permeable brick or concrete permeable pavers are proposed to delineate between road, parking and paths.

Appropriate hedges, soft planting and trees will be specified to meet local authority and BNG requirements. The final landscaping proposal can be dealt with through appropriate condition.

The front will have low brick wall base to 450mm height with metal hoop tops rails to 1.2m height overall. The north and south boundaries will comprised of the existing retained brick walls of garages with additional piers to provide structural stability.

The intermediate fence between gardens will be 1.5m close board timber fences.

### **Biodiversity Net Gain (BNG)**

BNG is a measure of existing biodiversity on the site against the proposed design which should be at least a 10% increase over the existing status. Given that the existing site is concrete road surface and garage structures and in the proposed scheme almost half the site is given back to green/planted areas, the required 10% increase in the BNG will be easily achieved on or offsite.

A specialist consultant has been commissioned, and this is under a separate report.

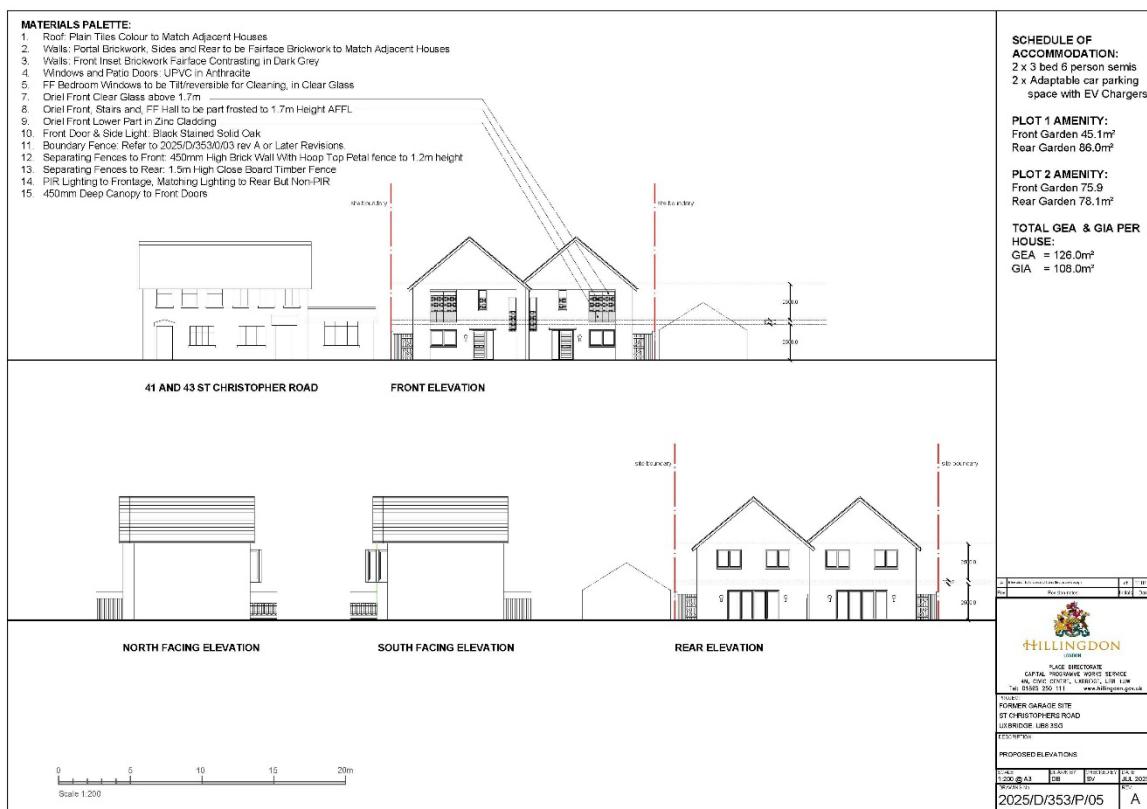
### 3.6 Flood Risk:

The site is not in a flood plain. The hard surfaces of the development will be made of permeable paving blocks with the required subbase so will assist any surface water run offs.

### 3.7 Appearance:

The proposed development is in a form of 2 semi-detached houses in traditional style and construction to sympathetically match adjacent houses.

To match the existing material street palette the external finishes are kept simple by using plain tiles, fairfaced brickwork to match surrounding houses with contrasting colour dark grey UPVC window frames in matt finish. Refer to elevation drawing below: 2025/D/353/P/05 rev A this also shows the street elevation.



**Fig 5 – Proposed Elevations Ref:2025/D/353/P05 rev A**

Obscured glazing has been used to the first-floor windows on the front elevation to prevent overlooking to a height of 1.7m above finished floor level with clear glazing above to allow for high views and sky views. The oriel side windows to the Juliet balconies are full height and clear glazing to allow for views and still avoid overlooking.

## 4.0 ACCESS

### 4.1 Accessibility Statement:

The scheme has been reviewed favourably by LBH's DDA officer. The ground floor wc/wet room is DDA accessible with a 750mm transfer space to the wc pan and level access into the shower/wet room. The ground floor wc could be adapted fully with handrails if required.

The development is compliant with the access requirements as well as The London Plan. All the external door openings will have level access thresholds wide enough for people with mobility difficulties and wheelchair users.

The new houses will be constructed to Volume 1-Dwellings-Access to and use of buildings, Part M4(1) of the Building Regulations 2010.

Each car parking space is DDA adaptable should the need arise.

## 5.0 SECURED BY DESIGN STATEMENT

Secure by Design has now been incorporated into Building Control Approved Documents; typically, this means that external doors will be to PAS24 standard with added security, and all windows will be to BS 7950.

These new UPVC windows will be certified to prove that the products and installation accords with the Secure by Design Standard.

The final design scheme will have an automatic dawn to dust PIR sensor external lighting at the main entrance door for each flat. A video door entry access system will be installed.

Bikes will be stored in a proprietary cycle store shed located within the rear garden. All cycle designed to 'secure by design' standards.

All gates to comply with secure by design in terms of robustness and locking systems.

New perimeter fencing to the rear boundary will be 1.8m close board timber fence with concrete post with an additional 300mm high timber trellis.

The north and south brick/concrete block walls be retained to a minimum of 2m height with supporting piers, and will be rendered white internally, and the existing red brick wall to the north by the access road will all be retained but reduced to 2.0m height.

## 6.0 TRANSPORT STATEMENT

The proposed parking layout and number of parking spaces complies with adopted parking standards to serve the new development. Electrical vehicle

charging points will be added in line with local policy, the location of which are to be determined by the system provider.

A total of two car parking spaces have been provided; one for each home, with electric vehicle charging bollards.

Highways Safety Requirements will also include:

The parking layout treatment will be of permeable block pavers, with coloured pavers to distinguish road, pedestrian and car parking zones.

Refuse and recycling bin strategy has been discussed in 2.3 above.

DDA car parking spaces have been discussed in “Movement” and “Access” above.

**END**