



107 Victoria Avenue
Hillingdon

PARKING STATEMENT

On behalf of
HDCL Group

ITR/5801/PS.1
September 2022

Experience and expertise working together



Document Control Sheet

Client: HDCL Group
30 Grantham Road
London
W4 2RS

Report Issue No.	Status	Date	Author	Authorised
ITR/5801/PS.1	Final	29 th September	ITR	Ian Roberts

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Introduction

Bellamy Roberts has been instructed by HDCL Group to carry out an on-street parking survey and advise on the parking matters arising from the planning application for a single dwelling at 107 Victoria Avenue, Hillingdon.

The applicant has received pre-application comments from the Local Planning Authority, which advises the applicant to review the maximum parking standards outlined in the London Plan Table 10.3.

It is suggested by the Local Planning Authority that the scheme would have a shortfall of 2 spaces.

Parking Standards

An extract from the Local Plan is attached at **Appendix 1** to this statement. The Residential Parking Policy T6.1 states that:-

- A. New residential development should not exceed the maximum parking standards set out in Table 10.3. These standards are a hierarchy with the more restrictive standard applying when a site falls into more than one category.*
- B. Parking spaces within communal car parking facilities (including basements) should be leased rather than sold.*
- C. All residential car parking spaces must provide infrastructure for electric or Ultra-Low Emission vehicles. At least 20 per cent of spaces should have active charging facilities, with passive provision for all remaining spaces.*
- D. Outside of the CAZ, and to cater for infrequent trips, car club spaces may be considered appropriate in lieu of private parking. Any car club spaces should have active charging facilities.*
- E. Large-scale purpose-built shared living, student accommodation and other sui generis residential uses should be car-free.*
- F. The provision of car parking should not be a reason for reducing the level of affordable housing in a proposed development.*
- G. Disabled persons parking should be provided for new residential developments. Residential development proposals delivering ten or more units must, as a minimum:*
 - 1. ensure that for three per cent of dwellings, at least one designated disabled persons parking bay per dwelling is available from the outset*

2. *demonstrate as part of the Parking Design and Management Plan, how an additional seven per cent of dwellings could be provided with one designated disabled persons parking space per dwelling in future upon request as soon as existing provision is insufficient. This should be secured at the planning stage.*
- H. *All disabled persons parking bays associated with residential development must:*
1. *be for residents' use only (whether M4(2) or M4(3) dwellings)*
 2. *not be allocated to specific dwellings, unless provided within the curtilage of the dwelling*
 3. *be funded by the payment of a commuted sum by the applicant, if provided on-street (this includes a requirement to fund provision of electric vehicle charging infrastructure)*
 4. *count towards the maximum parking provision for the development*
 5. *be designed in accordance with the design guidance in BS8300vol.1*
 6. *be located to minimise the distance between disabled persons parking bays and the dwelling or the relevant block entrance or lift core, and the route should be preferably level or where this is not possible, should be gently sloping (1:60-1:20) on a suitable firm round surface.*

Table 10.3 clearly identifies that the parking provision is set on a **maximum** and varies depending upon its location relative to London and its PTAL level. The standard also varies for the size of dwelling expressed by the number of bedrooms.

The PTAL calculations attached at **Appendix 2** identify the site to be within PTAL Level 2.

The scheme is for a 2-bed dwelling and in accordance with Table 10.3 of the London Plan, a 1 to 2 bed dwelling in outer-London with a PTAL level of 2 should provide a maximum parking provision of up to 0.75 spaces per dwelling.

As such, and in accordance with the current parking standards, the proposed dwelling should provide 0.75 space rounded up to one space.

The scheme accords with the adopted parking standards.

Notwithstanding this and in accordance with the instruction an on-street parking survey was undertaken on 13th and 14th July 2022. The survey revealed parked vehicles and spaces within a 200m radius of the site.

The proposal also requires the reconfiguration of the existing dwelling (3-bedrooms) and provide two car spaces for the property. Should further parking demand be generated, the on-street parking survey addresses the likely impact of such an occurrence.

The full results of the on-street parking survey are attached at **Appendix 3**. In summary, the findings were as follows:

Parking Stress Summary

Street Name	Parking Stress %
Regent Avenue	118%
Ryefield Avenue	46%-62%
Victoria Avenue	59%-64%

As identified by the parking survey, the most likely street that visitors and residents associated with the application site would use, is Victoria Avenue. This street has only 59%-64% occupancy during the peak times. There is therefore sufficient spaces on-street to accommodate any likely peak demand generated by the scheme's proposal.

Conclusion

Consideration of the LPA's residential parking standards established that the 2-bedroom dwelling proposed, shall provide a parking space of 0.75 cars, rounded to one car parking space. The proposal provides one car parking space in accordance with the London Plan.

An on-street parking survey undertaken within 200m of the site demonstrates that there are available safe parking spaces available should these proposals generate, at times a greater parking demand than one space.

The proposal accords with the standards and is safe, and there is no valid reason why the scheme should not be granted permission.

APPENDICES

APPENDIX 1

- 10.6.7 The general principles outlined in paragraphs 10.6.4 to 10.6.6 above apply to the parking standards set for residential, office (and Use Classes B2 and B8), retail, and hotel and leisure uses under Policy T6 .1 Residential parking to Policy T6 .5 Non-residential disabled persons parking. In relation to Policy T6 Car parking Part L, where industrial sites are redeveloped parking will be considered on a case by case basis as set out in paragraph 10.6.18.
- 10.6.8 Surface-level car parking should be **permeable** in accordance with Policy Policy SI 13 Sustainable drainage.

Policy T6.1 Residential parking

- A New residential development should not exceed the maximum parking standards set out in Table 10.3. These standards are a hierarchy with the more restrictive standard applying when a site falls into more than one category.
- B Parking spaces within communal car parking facilities (including basements) should be leased rather than sold.
- C All residential car parking spaces must provide infrastructure for electric or Ultra-Low Emission vehicles. At least 20 per cent of spaces should have active charging facilities, with passive provision for all remaining spaces.
- D Outside of the CAZ, and to cater for infrequent trips, car club spaces may be considered appropriate in lieu of private parking. Any car club spaces should have active charging facilities.
- E Large-scale purpose-built shared living, student accommodation and other sui generis residential uses should be car-free.
- F The provision of car parking should not be a reason for reducing the level of affordable housing in a proposed development.
- G Disabled persons parking should be provided for new residential developments. Residential development proposals delivering ten or more units must, as a minimum:
 - 1) ensure that for three per cent of dwellings, at least one designated disabled persons parking bay per dwelling is available from the outset
 - 2) demonstrate as part of the Parking Design and Management Plan, how an additional seven per cent of dwellings could be provided with one designated disabled persons parking space per dwelling in future upon

request as soon as existing provision is insufficient. This should be secured at the planning stage.

- H All disabled persons parking bays associated with residential development must:
- 1) be for residents' use only (whether M4(2) or M4(3) dwellings)
 - 2) not be allocated to specific dwellings, unless provided within the curtilage of the dwelling
 - 3) be funded by the payment of a commuted sum by the applicant, if provided on-street (this includes a requirement to fund provision of electric vehicle charging infrastructure)
 - 4) count towards the maximum parking provision for the development
 - 5) be designed in accordance with the design guidance in BS8300vol.1
 - 6) be located to minimise the distance between disabled persons parking bays and the dwelling or the relevant block entrance or lift core, and the route should be preferably level or where this is not possible, should be gently sloping (1:60-1:20) on a suitable firm ground surface.

Table 10.3 - Maximum residential parking standards

Location	Number of beds	Maximum parking provision*
Central Activities Zone Inner London Opportunity Areas Metropolitan and Major Town Centres All areas of PTAL 5 – 6 Inner London PTAL 4	All	Car free~
Inner London PTAL 3	All	Up to 0.25 spaces per dwelling
Inner London PTAL 2 Outer London Opportunity Areas	All	Up to 0.5 spaces per dwelling
Inner London PTAL 0 – 1	All	Up to 0.75 spaces per dwelling

Location	Number of beds	Maximum parking provision*
Outer London PTAL 4	1 – 2	Up to 0.5 - 0.75 spaces per dwelling+
Outer London PTAL 4	3+	Up to 0.5 - 0.75 spaces per dwelling+
Outer London PTAL 2 – 3	1 – 2	Up to 0.75 spaces per dwelling
Outer London PTAL 2 – 3	3+	Up to 1 space per dwelling
Outer London PTAL 0 – 1	1 – 2	Up to 1.5 space per dwelling
Outer London PTAL 0 – 1	3+	Up to 1.5 spaces per dwelling^

* Where Development Plans specify lower local maximum standards for general or operational parking, these should be followed

~ With the exception of disabled persons parking, see Part G [Policy T6 .1 Residential parking](#)

+ When considering development proposals that are higher density or in more accessible locations, the lower standard shown here should be applied as a maximum

^ Boroughs should consider standards that allow for higher levels of provision where there is clear evidence that this would support additional family housing

10.6.9 The Mayor's ambition is for London to be a city where it is easy for all disabled people to live and travel in London. Disabled people should have a genuine choice of housing that they can afford within a local environment that meets their needs. This means taking a holistic approach to creating streets, local services and a public transport network that caters for disabled people and people with long-term health conditions. It is recognised that some disabled people will rely on car travel more than others, whether as a passenger or a driver. This means that to ensure genuine housing choice, **disabled persons' parking** should be provided for new residential developments. In some circumstances this may include visitor parking for disabled residents who might have regular visitors such as carers. Any such parking should be marked out as such and restricted only for these users from the outset.

- 10.6.10 Where general parking is provided on-site, any disabled persons parking bays not provided at the outset should be identified on plan. For car-free development, how provision will be made, including whether bays are provided on-site or on-street, should be clearly set out and justified, in line with relevant guidance and local policies. All provision should be fully assessed and demonstrably consistent with the **inclusive design principles** of Policy D5 Inclusive design, and GG1 Building strong and inclusive communities; further information on how disabled persons parking should be approached and delivered will be set out in guidance.
- 10.6.11 Through **Parking Design and Management Plans**, applicants should provide details of how initial and future provision of disabled persons parking spaces will be made, managed and enforced. They should show where these spaces will be located and demonstrate how their availability will be made clear to residents prior to occupation to inform their housing decision. Where a bay is being marked up for a particular resident, this should be done prior to occupation. Details should also be provided of how existing or future residents would request a bay, how quickly it would be created and what, if any, provision of visitor parking for disabled residents is available. In car-free developments, at no time should any on-site space marked on plan for future disabled persons parking be used for general parking.
- 10.6.12 In implementing this policy, if three per cent of a scheme is less than one space, this should be rounded up to one.
- 10.6.13 Given the aims of this Plan and the Mayor's Transport Strategy in reducing car use and the priority given to affordable housing provision, to ensure the provision of parking does not impact on the level of affordable housing that is viable, the inclusion of parking provision (excluding disabled persons parking), even where consistent with the standards set out above, **should not result in a reduction to affordable housing**.
- 10.6.14 **Parking spaces should be leased rather than sold** to ensure the land they take up is used as efficiently as possible over the life of a development. This includes ensuring that disabled persons parking bays can be used by those who need them at any given time and ensuring enlarged bays are available to be converted to disabled persons parking bays as required. Leasing allows for spaces with active charging points to serve electric or other Ultra-Low Emission vehicles, and can more easily support passive provision becoming active. Leasing also supports parking provision to be adaptable to future re-purposing, such as following changes to transport technology or services. Leases should be short enough to allow for sufficient flexibility in parking allocation to reflect changing circumstances.

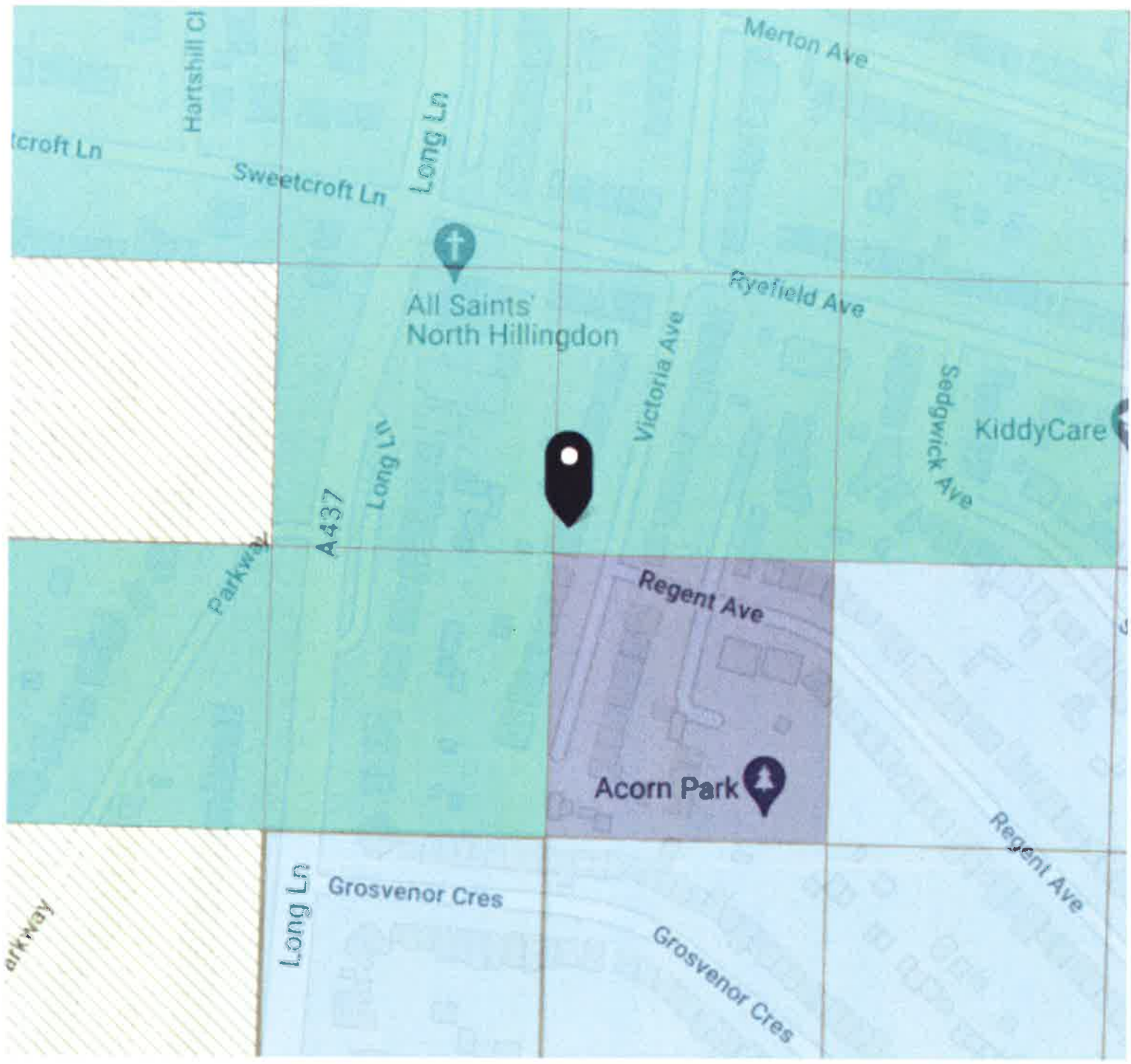
- 10.6.15 **Car clubs** count towards the maximum parking permitted because they share many of the negative impacts of privately-owned cars. However, in some areas, car club spaces can help support lower parking provision and car-lite lifestyles by enabling multiple households to make infrequent trips by car.

Policy T6.2 Office Parking

- A The maximum parking standards set out in [Table 10.4](#) should be applied to new office development.
- B In well-connected parts of outer London, including town centres, in close proximity to stations and in Opportunity Areas, office developments are encouraged to be car-free.
- C Car parking provision at Use Classes Order B2 (general industrial) and B8 (storage or distribution) employment uses should have regard to these office parking standards and take account of the significantly lower employment density in such developments. A degree of flexibility may also be applied to reflect different trip-generating characteristics. In these cases, appropriate provision for electric or other Ultra-Low Emission vehicles should be made.
- D Outer London boroughs wishing to adopt more generous standards are required to do so through an evidence-based policy in their Development Plan that identifies the parts of the borough in which the higher standards will be applied, and justifies those standards, including:
 - 1) the provision and operation of (existing and future) public transport, especially in relation to bus reliability
 - 2) the impact on the ability to deliver Healthy Streets, promote active travel and deliver mode shift
 - 3) the impact on congestion and air quality locally and on neighbouring boroughs and districts outside London as appropriate
 - 4) a commitment to increase or enhance publicly-available cycle parking
 - 5) a requirement (via Travel Plans) to reduce car parking provision over time and convert it to other uses.
- E Boroughs should not seek to adopt more generous standards borough-wide.
- F Operational parking requirements should be considered on a case-by-case basis. All operational parking must provide infrastructure for electric or other



APPENDIX 2



t for 2021 (Forecast)

02. Northing 184303

Map layers

- 1a
- 2
- 4
- 6a
- Change from base year

PTAL (cell size: 100m)

WebCAT PTAL Report

Site Details

Grid Cell: 100158

Easting: 507745

Northing: 184352

Report Date: 28/09/2022

Scenario: 2021 (Forecast)

Calculation Parameters

Day of Week: M-F

Time Period: AM Peak

Walk Speed: 4.8 kph

Bus Node Max Walk Access Time (mins): 8

Bus Reliability Factor: 2.0

LU Station Max Walk Access Time (mins): 12

LU Reliability Factor: 0.75

National Rail Station Max Walk Access Time (mins): 12

National Rail Reliability Factor: 0.75

Mode	Stop	Route	Distance (metres)	Frequency (vph)	Walk Time (mins)
SWT (mins)	TAT (mins)	EDF	Weight	AI	
Bus	LONG LANE	WESTERN AVENUE	U2	466.22 6.21	5.83 6.83
12.66	2.37	1	2.37		
LUL	Hillingdon	'UXBRDG-ALDGT SF	'	879.02 2	10.99 15.75
26.74	1.12	0.5	0.56		
LUL	Hillingdon	'ALDGT-UXBRDG S	'	879.02 6	10.99 5.75
16.74	1.79	1	1.79		
LUL	Hillingdon	'BKRST-UXBRDG S	'	879.02 6	10.99 5.75
16.74	1.79	0.5	0.9		
LUL	Hillingdon	'UXBRDG-BKRST SF	'	879.02 2	10.99 15.75
26.74	1.12	0.5	0.56		
LUL	Hillingdon	'Uxbridge-Cockfosters'		879.02 3.67	10.99 8.92
19.91	1.51	0.5	0.75		
LUL	Hillingdon	'ArnosGrove-Uxbridge	'	879.02 1	10.99 30.75
41.74	0.72	0.5	0.36		
LUL	Hillingdon	'Oakwood-Uxbridge	'	879.02 0.33	10.99 91.66
102.65	0.29	0.5	0.15		

Total Grid Cell AI: 7.44

PTAL: 2

APPENDIX 3



Client: **Bellamy Roberts**

Project Number: **TSP15105**

Project Name: **107 Victoria Avenue, Hillingdon**

Survey Type: **Parking Occupancy**

Survey Date: **13 & 14 July 2022**

Survey Time: **00:30-05:00**

Project Number: **TSP15105**
 Project Name: **107 Victoria Avenue, Hillingdon**
 Survey Type: **Parking Occupancy**



Waiting Restrictions											
Street Name	ACCESS	DOUBLE YELLOW	DOUBLE YELLOW/ DROPPED KERB	DOUBLE YELLOW/ PEDESTRIAN	DROPPED KERB	PERMIT HOLDER	PERMIT HOLDER/ DROPPED KERB	PERMIT HOLDER/ WHITE LINE	UNRESTRICTED BAY	UNRESTRICTED UNDESIRABLE	Total
Regent Avenue	6	16			15				17	6	60
Ryefield Avenue		4	1		12				26		43
Victoria Avenue		3	0	0	21	6	11	0	16		57
Total	6	23	1	0	48	6	11	0	59	6	160

Project Number: **TSP15105**
Project Name: **107 Victoria Avenue, Hillingdon**
Survey Type: **Parking Occupancy**



Occupancy					
Street Name / Regulation	Beat @ 13 Jul, Wed	Beat @ 14 Jul, Thu	Capacity	Beat @ 13 Jul, Wed	Beat @ 14 Jul, Thu
Regent Avenue					
Parking Occupancy	25	22	60	42%	37%
Parking Stress	20	20	17	118%	118%
ACCESS			6	0%	0%
DOUBLE YELLOW			16	0%	0%
DROPPED KERB	5	2	15	33%	13%
UNRESTRICTED BAY	20	20	17	118%	118%
UNRESTRICTED UNDESIRABLE			6	0%	0%
Ryefield Avenue					
Parking Occupancy	15	17	43	35%	40%
Parking Stress	12	16	26	46%	62%
DOUBLE YELLOW			4	0%	0%
DOUBLE YELLOW/ DROPPED KERB			1	0%	0%
DROPPED KERB	3	1	12	25%	8%
UNRESTRICTED BAY	12	16	26	46%	62%
Victoria Avenue					
Parking Occupancy	19	19	57	33%	33%
Parking Stress	13	14	22	59%	64%
DOUBLE YELLOW			3	0%	0%
DOUBLE YELLOW/ DROPPED KERB			0		
DOUBLE YELLOW/ PEDESTRIAN CROSSING			0		
DROPPED KERB	6	4	21	29%	19%
PERMIT HOLDER			6	0%	0%
PERMIT HOLDER/ DROPPED KERB		1	11	0%	9%
PERMIT HOLDER/ WHITE LINE			0		
UNRESTRICTED BAY	13	14	16	81%	88%
Area Summary					
Parking Occupancy	59	58	160	37%	36%
Parking Stress	45	50	65	69%	77%
ACCESS			6	0%	0%
DOUBLE YELLOW			23	0%	0%
DOUBLE YELLOW/ DROPPED KERB			1	0%	0%
DOUBLE YELLOW/ PEDESTRIAN CROSSING			0		
DROPPED KERB	14	7	48	29%	15%
PERMIT HOLDER			6	0%	0%
PERMIT HOLDER/ DROPPED KERB		1	11	0%	9%
PERMIT HOLDER/ WHITE LINE			0		
UNRESTRICTED BAY	45	50	59	76%	85%
UNRESTRICTED UNDESIRABLE			6	0%	0%

- DOUBLE YELLOW
- PERMIT HOLDER
- DROPPED KERB
- UNRESTRICTED BY PEDESTRIAN KERB



Project Number: TSP15105
Project Name: 107 Victoria Avenue, Hillinodon
Survey Type: Parking Occupancy



ID	STREET NAME	REGULATION	LENGTH	No of Bays	13 July, Wed	14 July, Thu
1	Victoria Avenue	DOUBLE YELLOW	4.2	0		
2	Victoria Avenue	PERMIT HOLDER/ DROPPED KERB	3.7	0		
3	Victoria Avenue	PERMIT HOLDER	5.8	1		
4	Victoria Avenue	PERMIT HOLDER/ DROPPED KERB	44.2	8		1
5	Victoria Avenue	DROPPED KERB	4.9	0		
6	Victoria Avenue	PERMIT HOLDER/ DROPPED KERB	5.4	1		
7	Victoria Avenue	DOUBLE YELLOW/ DROPPED KERB	3.5	0		
8	Victoria Avenue	DOUBLE YELLOW	3.7	0		
9	Victoria Avenue	DOUBLE YELLOW	6.3	1		
10	Victoria Avenue	PERMIT HOLDER	23.1	4		
11	Victoria Avenue	PERMIT HOLDER/ WHITE LINE	3.2	0		
12	Victoria Avenue	PERMIT HOLDER/ DROPPED KERB	5.6	1		
13	Victoria Avenue	PERMIT HOLDER/ DROPPED KERB	9.4	1		
14	Victoria Avenue	PERMIT HOLDER	9.1	1		
15	Victoria Avenue	DROPPED KERB	3.6	0		
16	Victoria Avenue	PERMIT HOLDER	1.3	0		
17	Victoria Avenue	DROPPED KERB	6.4	1		
18	Victoria Avenue	PERMIT HOLDER/ DROPPED KERB	0	0		
19	Victoria Avenue	PERMIT HOLDER/ DROPPED KERB	2.3	0		
20	Victoria Avenue	PERMIT HOLDER	4.0	0		
21	Victoria Avenue	PERMIT HOLDER/ WHITE LINE	1.3	0		
22	Ryefield Avenue	DOUBLE YELLOW	5.3	1		
23	Ryefield Avenue	UNRESTRICTED BAY	17.1	3		
24	Ryefield Avenue	DROPPED KERB	6.3	1		
25	Ryefield Avenue	UNRESTRICTED BAY	5.4	1		
26	Ryefield Avenue	DROPPED KERB	3.4	0		
27	Ryefield Avenue	UNRESTRICTED BAY	2.7	0		
28	Ryefield Avenue	DROPPED KERB	3.5	0		
29	Ryefield Avenue	UNRESTRICTED BAY	2.9	0		
30	Ryefield Avenue	DROPPED KERB	12.3	2		
31	Ryefield Avenue	UNRESTRICTED BAY	2.9	0		
32	Ryefield Avenue	DROPPED KERB	3.7	0		
33	Ryefield Avenue	UNRESTRICTED BAY	1.9	0		
34	Ryefield Avenue	DROPPED KERB	3.4	0		
35	Ryefield Avenue	UNRESTRICTED BAY	6.4	1		
36	Ryefield Avenue	DROPPED KERB	4.0	0		
37	Ryefield Avenue	UNRESTRICTED BAY	6.5	1		1
38	Ryefield Avenue	DOUBLE YELLOW	6.2	1		
39	Ryefield Avenue	UNRESTRICTED BAY	18.1	3		1
40	Ryefield Avenue	DROPPED KERB	11.6	2		1
41	Ryefield Avenue	UNRESTRICTED BAY	3.1	0		1
42	Ryefield Avenue	DROPPED KERB	19.4	3		1
43	Ryefield Avenue	UNRESTRICTED BAY	3.8	0		1
44	Ryefield Avenue	DROPPED KERB	3.4	0		
45	Ryefield Avenue	UNRESTRICTED BAY	14.5	2		2
46	Ryefield Avenue	DOUBLE YELLOW	6.9	1		
47	Victoria Avenue	DOUBLE YELLOW/ PEDESTRIAN CROSSING	3.7	0		
48	Victoria Avenue	DOUBLE YELLOW	2.2	0		
49	Victoria Avenue	DROPPED KERB	6.4	1		
50	Victoria Avenue	UNRESTRICTED BAY	4.5	0		1
51	Victoria Avenue	DROPPED KERB	3.0	0		
52	Victoria Avenue	UNRESTRICTED BAY	3.1	0		
53	Victoria Avenue	DROPPED KERB	4.3	0		
54	Victoria Avenue	UNRESTRICTED BAY	5.9	1		1
55	Victoria Avenue	DROPPED KERB	3.3	0		
56	Victoria Avenue	UNRESTRICTED BAY	7.3	1		1
57	Victoria Avenue	DROPPED KERB	2.7	0		
58	Victoria Avenue	UNRESTRICTED BAY	0.9	0		
59	Victoria Avenue	DROPPED KERB	3.8	0		
60	Victoria Avenue	UNRESTRICTED BAY	2.3	0		
61	Victoria Avenue	DROPPED KERB	9.4	1		1
62	Victoria Avenue	UNRESTRICTED BAY	6.4	1		1
63	Victoria Avenue	DROPPED KERB	4.5	0		
64	Victoria Avenue	UNRESTRICTED BAY	4.1	0		
65	Victoria Avenue	DROPPED KERB	6.5	1		
66	Victoria Avenue	UNRESTRICTED BAY	1.5	0		
67	Victoria Avenue	DROPPED KERB	3.4	0		1
68	Victoria Avenue	UNRESTRICTED BAY	5.8	1		1
69	Victoria Avenue	DROPPED KERB	3.7	0		1
70	Victoria Avenue	DOUBLE YELLOW	2.4	0		
71	Regent Avenue	DROPPED KERB	3.4	0		
72	Regent Avenue	DOUBLE YELLOW	6.9	1		
73	Regent Avenue	UNRESTRICTED BAY	22.9	4		3
74	Regent Avenue	DROPPED KERB	5.0	1		
75	Regent Avenue	UNRESTRICTED BAY	22.9	4		3
76	Regent Avenue	DROPPED KERB	4.4	0		
77	Regent Avenue	UNRESTRICTED BAY	3.6	0		
78	Regent Avenue	DROPPED KERB	9.4	1		1
79	Regent Avenue	UNRESTRICTED BAY	14.2	2		2
80	Regent Avenue	DROPPED KERB	5.9	1		
81	Regent Avenue	UNRESTRICTED BAY	8.8	1		1
82	Regent Avenue	DROPPED KERB	12.9	2		1
83	Regent Avenue	UNRESTRICTED BAY	1.8	0		
84	Regent Avenue	DROPPED KERB	14.2	2		1
84A	Regent Avenue	UNRESTRICTED BAY	4.3	0		1
84B	Regent Avenue	DROPPED KERB	10.2	2		1
85	Regent Avenue	UNRESTRICTED BAY	14.7	2		2
86	Regent Avenue	DROPPED KERB	2.5	0		
87	Regent Avenue	UNRESTRICTED BAY	1.8	0		
88	Regent Avenue	DROPPED KERB	4.4	0		
89	Regent Avenue	UNRESTRICTED BAY	2.4	0		
90	Regent Avenue	DROPPED KERB	7.1	1		
91	Regent Avenue	UNRESTRICTED BAY	2.1	0		
92	Regent Avenue	DROPPED KERB	6.5	1		1
93	Regent Avenue	UNRESTRICTED BAY	2.2	0		1
94	Regent Avenue	DROPPED KERB	5.5	1		
95	Regent Avenue	UNRESTRICTED BAY	3.5	0		1
96	Regent Avenue	DROPPED KERB	4.0	0		
97	Regent Avenue	UNRESTRICTED BAY	7.0	1		2
98	Regent Avenue	DROPPED KERB	12.1	2		1
99	Regent Avenue	UNRESTRICTED BAY	4.5	0		1
100	Regent Avenue	DROPPED KERB	4.5	0		
101	Regent Avenue	UNRESTRICTED BAY	3.4	0		1
102	Regent Avenue	DROPPED KERB	6.9	1		
103	Regent Avenue	UNRESTRICTED BAY	2.0	0		
104	Regent Avenue	DOUBLE YELLOW	8.6	1		
105	Regent Avenue	DOUBLE YELLOW	30.6	6		
106	Regent Avenue	DOUBLE YELLOW	30.6	6		
107	Regent Avenue	UNRESTRICTED UNDESIRABLE ACCESS	12.3	6		
108	Regent Avenue	ACCESS	32.3	6		
109	Regent Avenue	DOUBLE YELLOW	7.7	1		
110	Regent Avenue	UNRESTRICTED BAY	15.0	3		2
111	Regent Avenue	DOUBLE YELLOW	7.5	1		
112	Victoria Avenue	DOUBLE YELLOW	1.7	0		
113	Victoria Avenue	DROPPED KERB	7.0	1		1
114	Victoria Avenue	UNRESTRICTED BAY	2.0	0		1
115	Victoria Avenue	DROPPED KERB	3.0	0		1
116	Victoria Avenue	UNRESTRICTED BAY	8.0	1		1
117	Victoria Avenue	DROPPED KERB	5.0	1		1
118	Victoria Avenue	UNRESTRICTED BAY	1.3	0		
119	Victoria Avenue	DROPPED KERB	4.0	0		
120	Victoria Avenue	UNRESTRICTED BAY	2.0	0		
121	Victoria Avenue	DROPPED KERB	12.0	2		
122	Victoria Avenue	UNRESTRICTED BAY	3.0	0		1
123	Victoria Avenue	DROPPED KERB	3.0	0		
124	Victoria Avenue	UNRESTRICTED BAY	11.0	2		2
125	Victoria Avenue	DROPPED KERB	2.0	0		
126	Victoria Avenue	DROPPED KERB	5.0	1		
127	Victoria Avenue	UNRESTRICTED BAY	13.0	2		1
128	Victoria Avenue	DROPPED KERB	2.0	0		
129	Victoria Avenue	UNRESTRICTED BAY	7.1	1		
130	Victoria Avenue	DROPPED KERB	9.5	1		
131	Victoria Avenue	UNRESTRICTED BAY	5.9	1		
132	Victoria Avenue	DROPPED KERB	3.0	0		
133	Victoria Avenue	UNRESTRICTED BAY	6.7	1		
134	Victoria Avenue	DROPPED KERB	6.3	1		
135	Victoria Avenue	UNRESTRICTED BAY	5.7	1		1
136	Victoria Avenue	DROPPED KERB	3.0	0		1
137	Victoria Avenue	UNRESTRICTED BAY	1.0	0		
138	Victoria Avenue	DROPPED KERB	8.0	1		
139	Victoria Avenue	UNRESTRICTED BAY	2.0	0		
140	Victoria Avenue	DROPPED KERB	6.0	1		
141	Victoria Avenue	UNRESTRICTED BAY	2.0	0		
142	Victoria Avenue	DROPPED KERB	4.0	0		
143	Victoria Avenue	UNRESTRICTED BAY	2.0	0		
144	Victoria Avenue	DROPPED KERB	12.0	2		1
145	Victoria Avenue	UNRESTRICTED BAY	2.0	0		
146	Victoria Avenue	DROPPED KERB	12.0	2		
147	Victoria Avenue	UNRESTRICTED BAY	2.0	0		
148	Victoria Avenue	DROPPED KERB	12.0	2		
149	Victoria Avenue	UNRESTRICTED BAY	3.8	0		
150	Victoria Avenue	DROPPED KERB	10.0	2		
151	Victoria Avenue	UNRESTRICTED BAY	19.0	3		3
152	Victoria Avenue	DOUBLE YELLOW	11.0	2		
153	Ryefield Avenue	DOUBLE YELLOW/ DROPPED KERB	5.7	1		
154	Ryefield Avenue	UNRESTRICTED BAY	4.0	0		1
155	Ryefield Avenue	DROPPED KERB	12.7	2		
156	Ryefield Avenue	UNRESTRICTED BAY	21.9	4		3
157	Ryefield Avenue	DROPPED KERB	4.0	0		
158	Ryefield Avenue	UNRESTRICTED BAY	24.4	4		3
159	Ryefield Avenue	UNRESTRICTED BAY	27.1	5		
160	Ryefield Avenue	DROPPED KERB	7.4	1		
161	Ryefield Avenue	UNRESTRICTED BAY	12.8	2		1
162	Ryefield Avenue	DROPPED KERB	3.3	0		
163	Ryefield Avenue	UNRESTRICTED BAY	1.9	0		1
164	Ryefield Avenue	DROPPED KERB	7.8	1		
165	Ryefield Avenue	UNRESTRICTED BAY	3.7	0		1
166	Ryefield Avenue	DOUBLE YELLOW	6.4	1		



Clover House
Western Lane
Odiham
Hampshire
RG29 1TU
Tel: 01256 703355



Reg No OC303725. Registered Office: Clover House, Western Lane, Odiham, Hampshire RG29 1TU

Partner:
IT Roberts MCIHT
Associate Partner:
MJ Twinberrow BEng
Consultant:
GD Bellamy BSc CEng MICE

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