



DESIGN & ACCESS STATEMENT

27 RECTORY WAY, Ickenham, Uxbridge, Middlesex. UB10 8BS

THE EXISTING SITE AND NEIGHBOURHOOD CONTEXT

27 Rectory Way is a detached house in Ickenham, situated within the borough of Hillingdon. The property is plotted last at the end of a line of detached houses intersecting with Eleanor Grove. This means the site is gifted with one of the largest plots on the nearby roads as additional land at the end of the road is conveyed to the site (corner plot). The property has one neighbour to its left side and one perpendicular to the rear of the garden. No neighbour exists to the right side.

The property benefits from a front drive, which has access to the original attached small single garage. The garage roof is integrated with the roof over the front entrance, which projects slightly forward on the ground floor. Access to the garden is via the gate in the boundary fence and through the garage.

The property also has a generous rear garden within the fence line shown. Additional land outside of the fence is also owned and maintained by the property owner.



Boundary with #25



#27 Rectory Way, Front Elevation



#27 Rectory Way, Left Side Elevation,
within contained garden

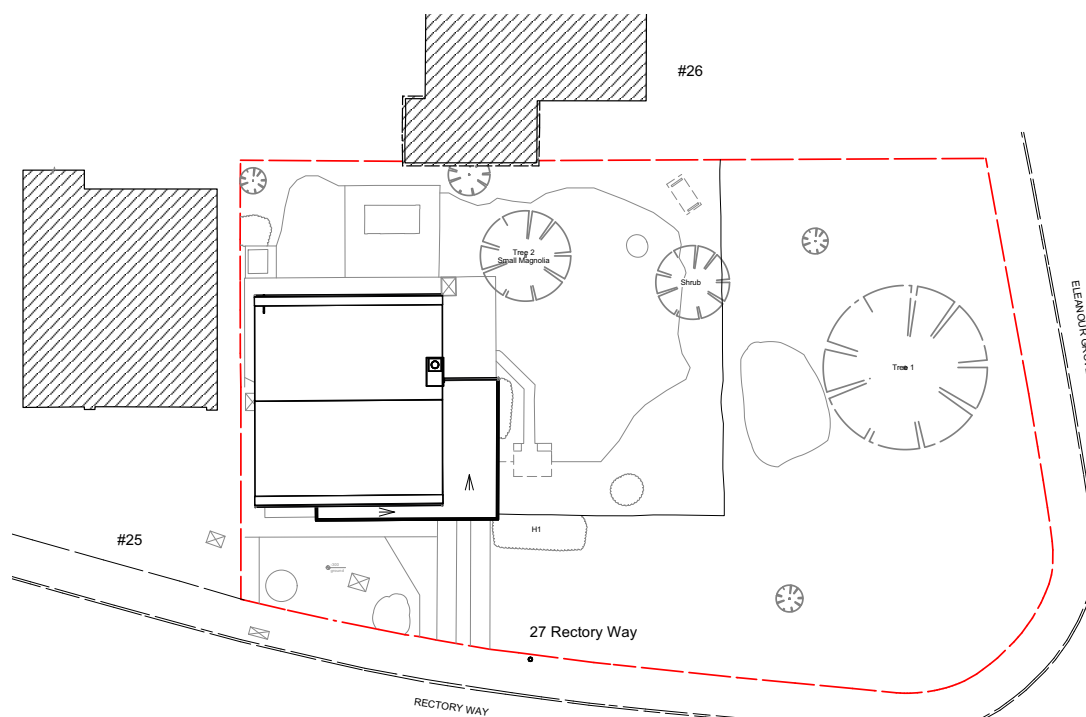


#27 Rectory Way intersecting with Eleanor Grove -
showing additional land outside of garden fence



EXISTING SITE

These drawings show the existing site conditions. The property is situated on an expansive site, with the curtilage extending to the kerb, beyond the enclosed garden fence. This is clearly evident in the site plan (outlined in red), emphasizing that the size of the site surpasses that of other properties in the area.





THE PROPOSAL

The proposed submission can be described as:

Extension of the existing original single storey garage to create a double garage.

Reason for application:

The current single garage was built alongside and at the same time as the house in the 1960's, in a time when cars were much smaller and households typically had max 1 vehicle. The current garage length and width is not practical for parking modern vehicles and can only be used for storage.

Furthermore, the current driveway arrangement only allows the homeowners to park 1 vehicle off the carriage way despite the large plot.

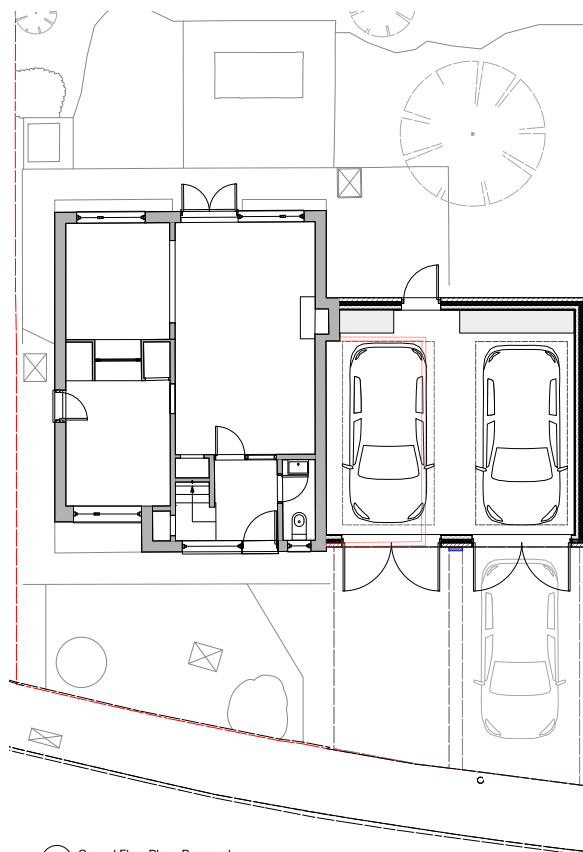
To improve this situation, the homeowner is looking to increase the size of the existing garage to safely park their cars within the garage.

Given the impending expansion of Ultra Low Emission Zone (ULEZ), the homeowner is also preparing for an update to electric vehicles & will be installing an electric vehicle charging point attached to the garage wall. The forecourt will therefore serve as a charging spot which will also assist by taking the family vehicles off the main road side.

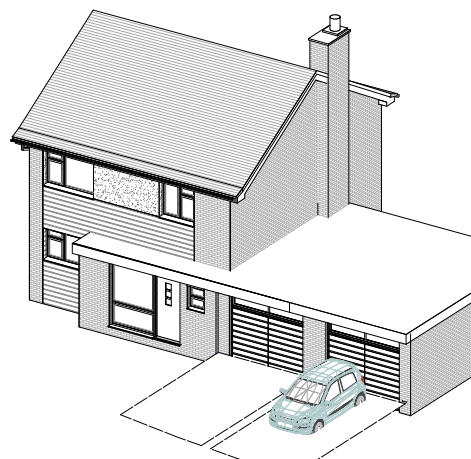
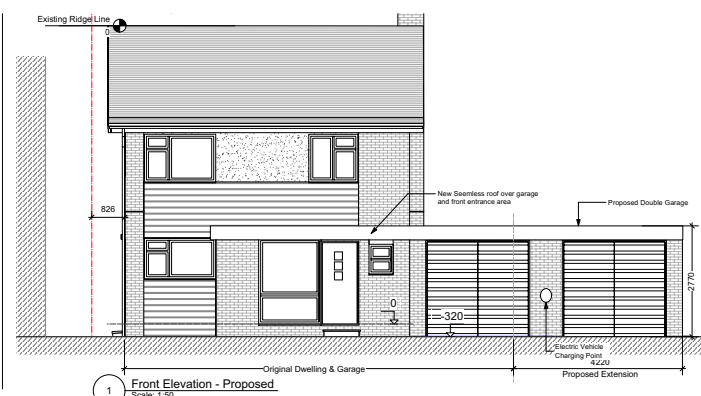
External materials and height

The Double garage is designed in the same architectural language as the existing garage, proposing bricks to match existing and a new flat roof over the garage and front of the house to match existing style.

The wall heights are proposed to remain the same but as a new roof construction over the garage is proposed with thicker joists, the overall thickness and overall height of the roof will increase slightly over the garage and front entrance / porch. The height of the flat roof will not exceed 3m from natural ground level. The increased thickness will also facilitate a greater degree of thermal efficiency in the roof installation used over the house front porch.



1 Ground Floor Plan - Proposed
Scale: 1:50





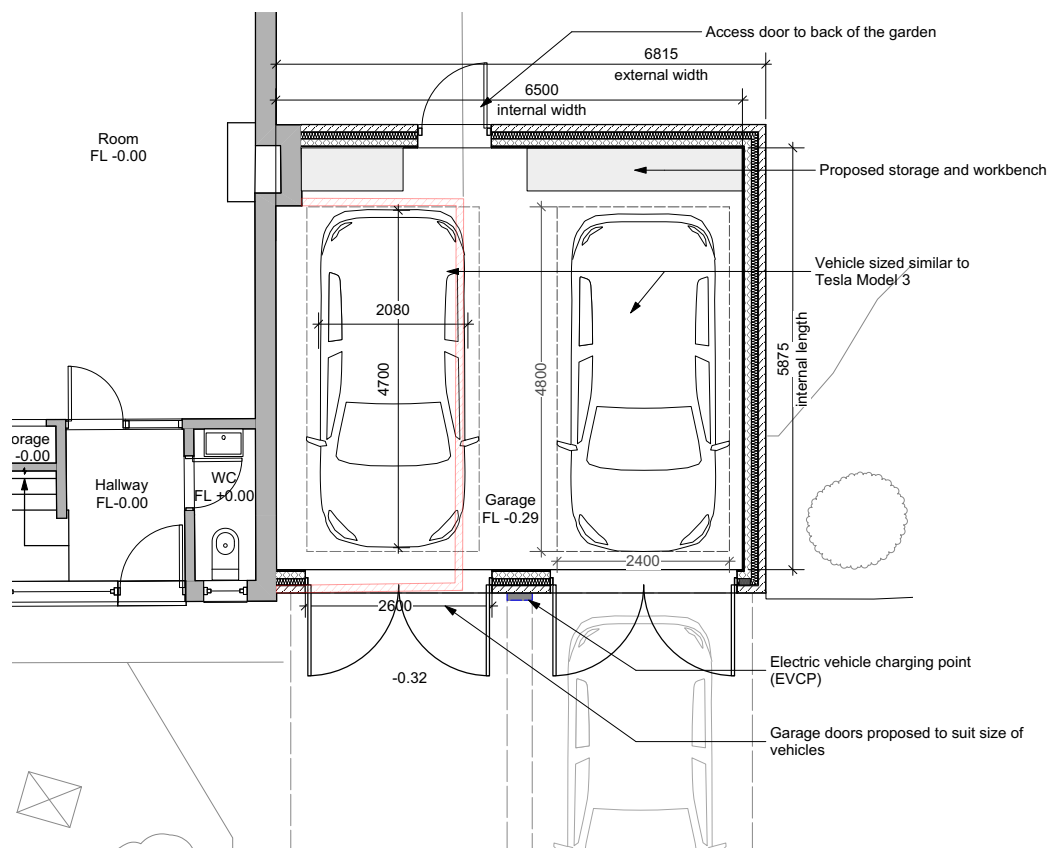
Garage footprint

The size of the garage extension has been determined from a functional point of view. Considering the growing size of modern vehicles, the design of the garage space aims to accommodate vehicles similar in size to the Tesla Model 3 (with dimensions of approximately 2.1m in width and 4.7m in length).

This garage expansion ensures convenient maintenance and charging facilities for the cars while also ensuring they are safely stored off the road. Within the allocated space, there will be provisions for a storage area and a workbench located at the rear of the garage. This setup will facilitate routine maintenance tasks.

Consequently, both the garage entrance doors have been sized to suit cars of this size & the small centralised wall between will mount the Electric vehicle Charging Point (EVCP).

The below extract of the garage floor plan demonstrates the points noted and that not much surplus space remains.



The Hillingdon Local plan section on detached garages recommends internal dimensions for double garages to be minimum 4.8m x 5.7m. Our proposed garage measures internally 5.875 x 6.5m which as you can see is not vastly greater, especially considering the recent increase and move towards SUV size vehicles. Our proposed double garage size falls within the range of sizes accepted for a double garage.

Hillingdon planning guidance states that an attached side extension and garage should be a minimum internal size of 3m x 6m (single garage) and should relate to the design of the building and area around whilst preserving and enhancing the local character. It should also not exceed half the width of the original house.



In relation to this guidance, It should be noted that the existing garage forms part of the original dwelling as it was first constructed (Circa 1960). The additional width now proposed does not exceed more than half the width of the original house if the original garage width is included in the calculation.

In terms of the overall site, the proposed garage extension is not located near any neighbouring properties and will therefore not impact any neighbours negatively. The openness of the corner will still be maintained due to the large expanse of the open undeveloped land on the site, outside of the garden fence but owned and maintained by the homeowners.

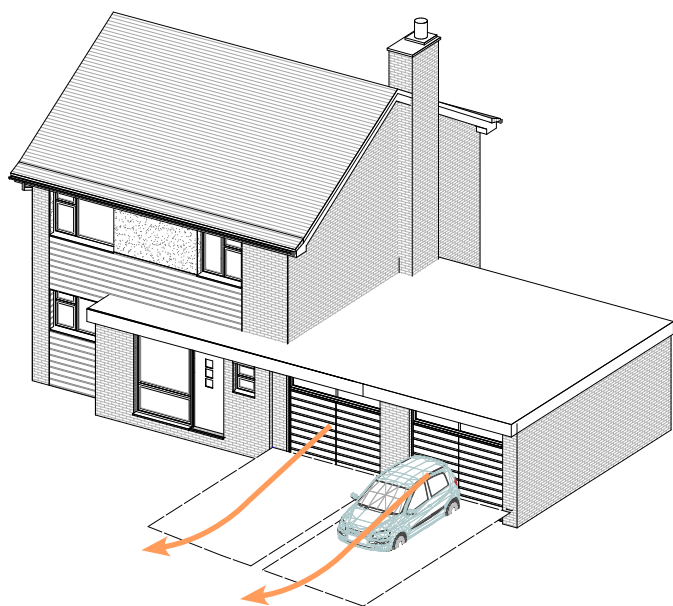
The overall height of the garage is below 3m in height and so should appear subservient but similar in design to the original house and garage.

Development on the plot

I would like to draw your attention to a vast majority of other properties on the street which are more or less full plot width detached properties except for on one boundary line where an access gate exists. The ratio of total development on these sites is much greater than the ratio of our site once the double garage is constructed. This point is to highlight that whilst it may appear the overall appearance of the garage as one unit will be wide, there are many other properties on the same road whose site area is smaller and total development on the land is greater than ours.

ACCESS

To ensure convenient accessibility, the existing access from the garage to the garden will be maintained through the rear pedestrian door, allowing for easy entry and exit. The current side gate located in the front fence should be preserved and maintained, positioned to the right of the front of the garage.



AMENITY SPACE

Adequate amenity space will still be available within the garden fence boundary of the dwelling following the extension of the existing garage. The rear garden alone is approx 180m².