



DESIGN AND ACCESS STATEMENT

**DEMOLITION OF EXISTING 3 BEDROOM DWELLINGHOUSE WITH ATTACHED GARAGE
CONSTRUCTION OF 2 NO. FOUR BEDROOM HOUSES**



at

**9 VINE LANE
HILLINGDON
MIDDLESEX
UB10 0AY**

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1.0 The Site

- 1.1 The Site is 9 Vine Lane, Hillingdon, Middlesex, UB10 0AY and is located on the west side of Vine Lane. At present the site comprises a two storey detached dwelling finished in brick, the property has a single storey attached garage and a garden to its front side and rear.



Fig. 1: Site Location Plan

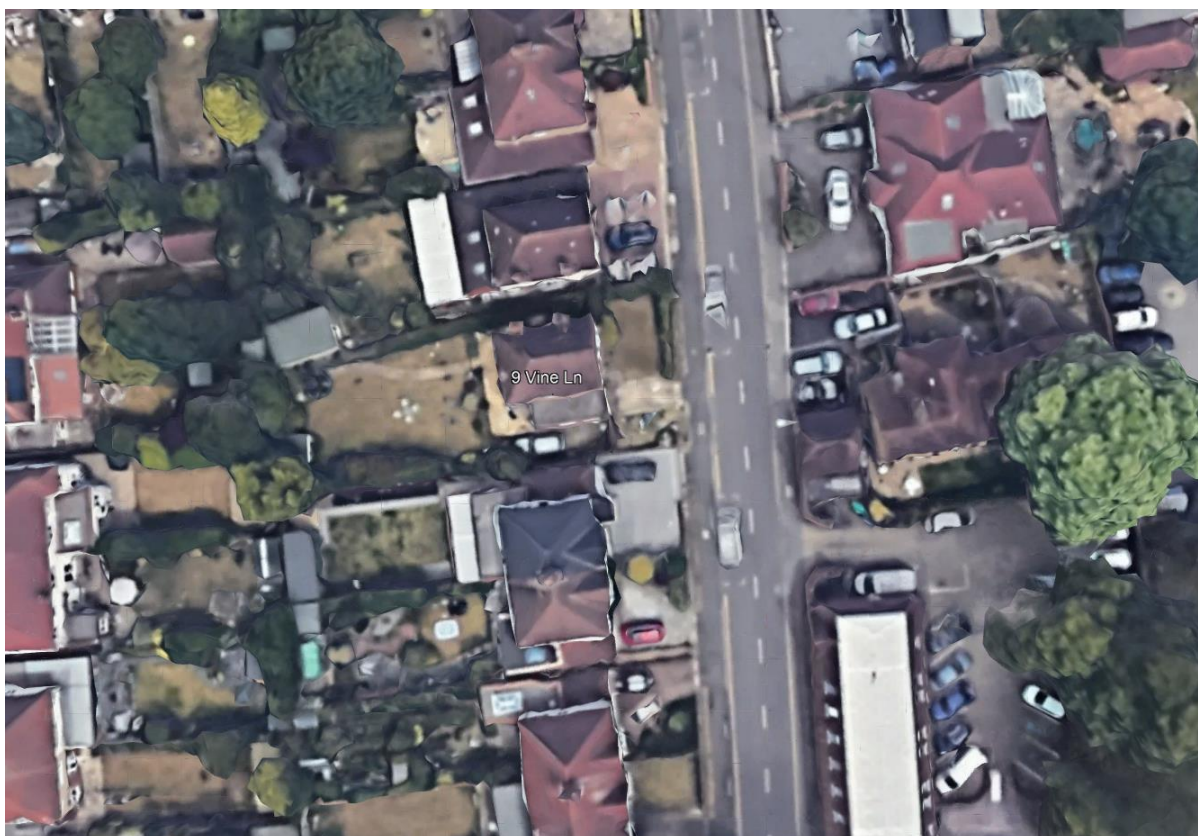


Fig. 2: Existing Aerial Image

- 1.2 The surrounding area is residential and properties are predominantly detached and semi-detached. In terms of height they are single and two storey. In terms of design, properties vary, however they have pitched roofs and finished in brick, render and mock tudor beaming.
- 1.3 The site is on the edge of, but is not within the Hillingdon Village Conservation Area.
- 1.4 The site is located upon potentially contaminated land and within a Air Quality Focus Area. It has a PTAL rating of 2.
- 1.5 Flood Risk

The site is located within Environment Agency Flood Zone 1, meaning flood risk is low.

Rivers and the sea: Very low risk

Very low risk means that this area has a chance of flooding of less than 0.1% each year.

Surface water: Very low risk

Very low risk means that this area has a chance of flooding of less than 0.1% each year. Flooding from surface water is difficult to predict as rainfall location and volume are difficult to forecast. In addition, local features can greatly affect the chance and severity of flooding.



Fig. 3: Street View Photograph

2.0 Site History

2.1 Pre-APP advice obtained on 21st August 2023

Case Officer: Haydon Richardson

Reference: 77852/PRC/2023/26

Comments:

The principle of development is considered to be acceptable

Provide amended parking layout showing 1 parking space per dwelling and no conflict with on street parking bay.

Provide cycle parking layout

Indicate Refuse Storage

Provide policy vehicular accesses.

Soft and hard landscaping plans

Additional info regarding the impact of the proposal on neighbouring side windows and what those windows serve.

3.0 The Proposed Development – Design, Scale and Layout

- 3.1 The Proposed Development seeks to demolish the existing three-bedroom detached dwelling house and garage and provide a pair of semi-detached dwellings. Semi-detached dwellings are common within the area and therefore the buildings would not be out of character. In terms of their design, they would be similar to 17 and 19 Vine Lane and would be in keeping with other dwellings in the area.



Fig. 4: Proposed elevation vs 17 and 19 Vine Lane

- 3.2 The properties would be set back from the road similar to other dwellings in the area. Front gardens within the area are fully hard surfaced or have a balance of hard and soft landscaping. The proposed front garden would therefore not be out of character with some grassed area and hard surfaces enough for a footpath and driveway.

- 3.3 The proposed dwellings would be set off of the shared boundaries by 1m at the narrowest point on each side and 1.4m at the widest point (south side) and 2m at the widest point (north side). This ensures the buildings cause no visual terracing and that they would retain one of the areas characteristics (space between plots). Gardens are located to the rear of the site like other dwellings in the area.
- 3.3 The Proposed Development will be symmetrical, externally and internally providing family living space. Externally there will be gable walls on each dwelling together with a feature wall to the front. The eight of the Proposed Development will be 7.790m compared to the existing height of 7.57m. As the streetscape is not uniform with varying heights and types of dwellings the Proposed Development's scale is appropriate. Several two storey dwelling houses have recently been approved to the eastern side of Vine Lane which are significantly higher..
- 3.4 The Proposed Development's external materials are in keeping with the existing material palette and will provide an improved street scene and contribute positively to the character of the area. We would assume that the external materials would be conditions for approval prior to the start of the development.

Material Schedule

AREA	LOCATION	TYPE
Walls	Front Elevation – low level Side Elevation Rear Elevation	Facing brick
	Front Elevation feature wall	White painted textured render
Roof	Roof Dormers	Concrete tiles
Dormer walls	Hanging roof tiles	
Windows	General	UPVC

- 3.5 The internal layout of the Proposed Development will provide an efficient use of space for family living. The internal spaces are all in accordance with the National Minimum Space Standards.
- 3.6 The Proposed Development will provide private rear garden space, the existing mature landscaping will be retained. All areas to the front noted as soft landscaping will be landscaped accordingly. There will also be a front driveway and the boundary treatments will be close boarded timber fencing.

Amenity

3.7 7 Vine Lane

The proposed dwellings would not dissect lines drawn at 45 degrees, refer to drawings. Fig. 5 shows the layout for no. 7 Vine Lane (source: Rightmove) which confirms the window positions as indicated on the drawings. No windows to this property will be affected by the proposal.

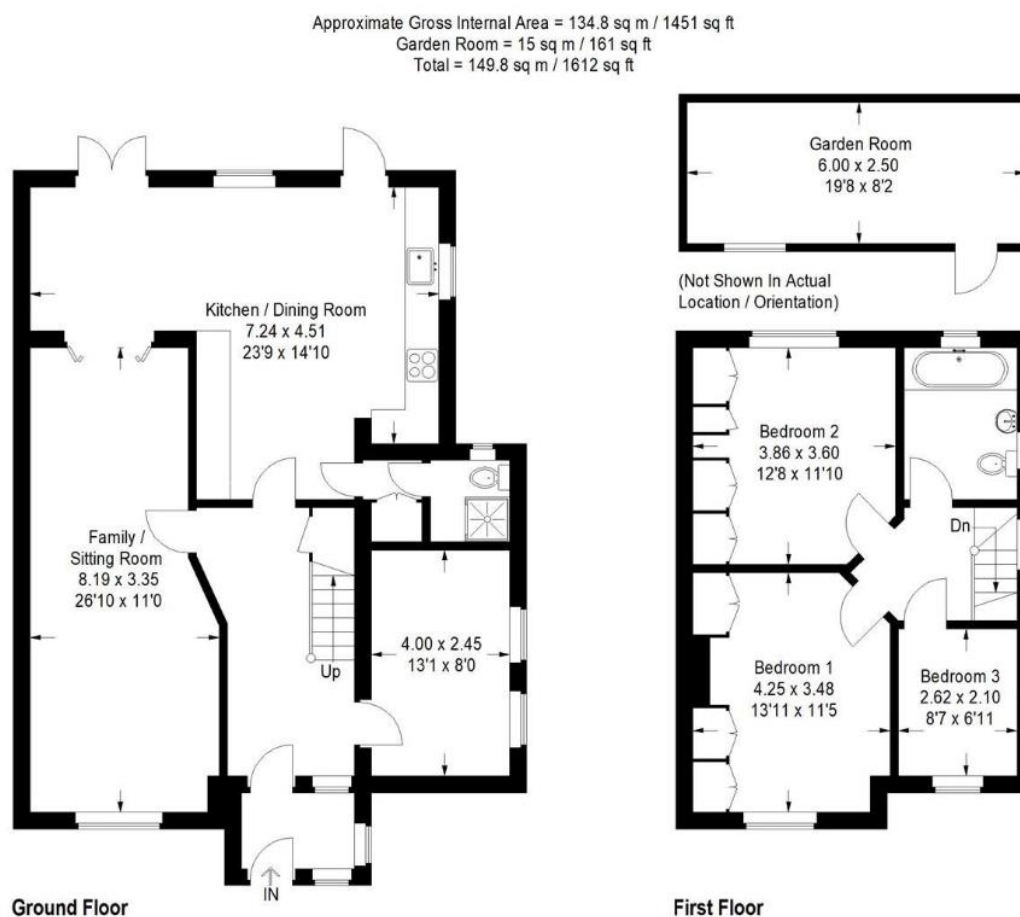


Fig. 5: Floor plans for 7 Vine Lane (Source: Rightmove)

3.8 9 Vine Lane

The proposed dwellings would not dissect lines drawn at 45 degrees, refer to drawings. Fig. 6 shows the layout and elevations (source: Hillingdon planning search) for no. 9 Vine Lane which confirms the window positions as indicated on the drawings. No windows to this property will be affected by the proposal.

3.9 Side elevations exist at both neighbouring properties, the windows are obscure or serve non habitable rooms. The proposal will therefore not cause any harmful loss of light or outlook from these windows.

3.10 The front and rear windows of the properties would provide similar views to the existing dwelling and therefore will cause no harmful loss of privacy to the neighbouring properties.

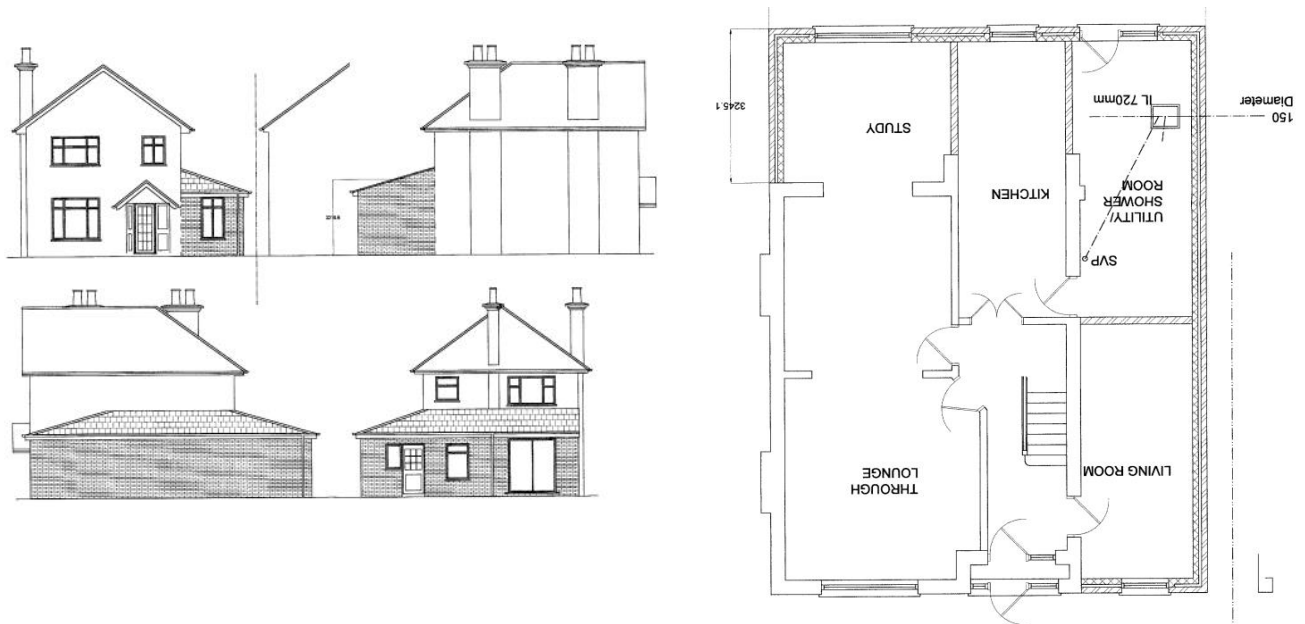


Fig. 6: Ground Floor Plan (Source: Hillingdon Planning Search)



Fig. 7: Photograph of 9 and 11 Vine Lane

Amenity Space

- 3.10 The existing property has a garden depth of circa 21m -24.5m. The new dwelling houses will have gardens with a length of 17m, total area: 100-112sqm.

Internal Layout

- 3.12 The modest size house would provide a kitchen/diner at ground floor level together with a living room, family room, WC and utility room with two double bedrooms with an ensuite bathroom and family bathroom on the first floor along with a study.
- 3.13 Two car parking spaces for each property are provided to the front of the dwellings along with a rear garden comprising a patio area with raised planters with lawn beyond.

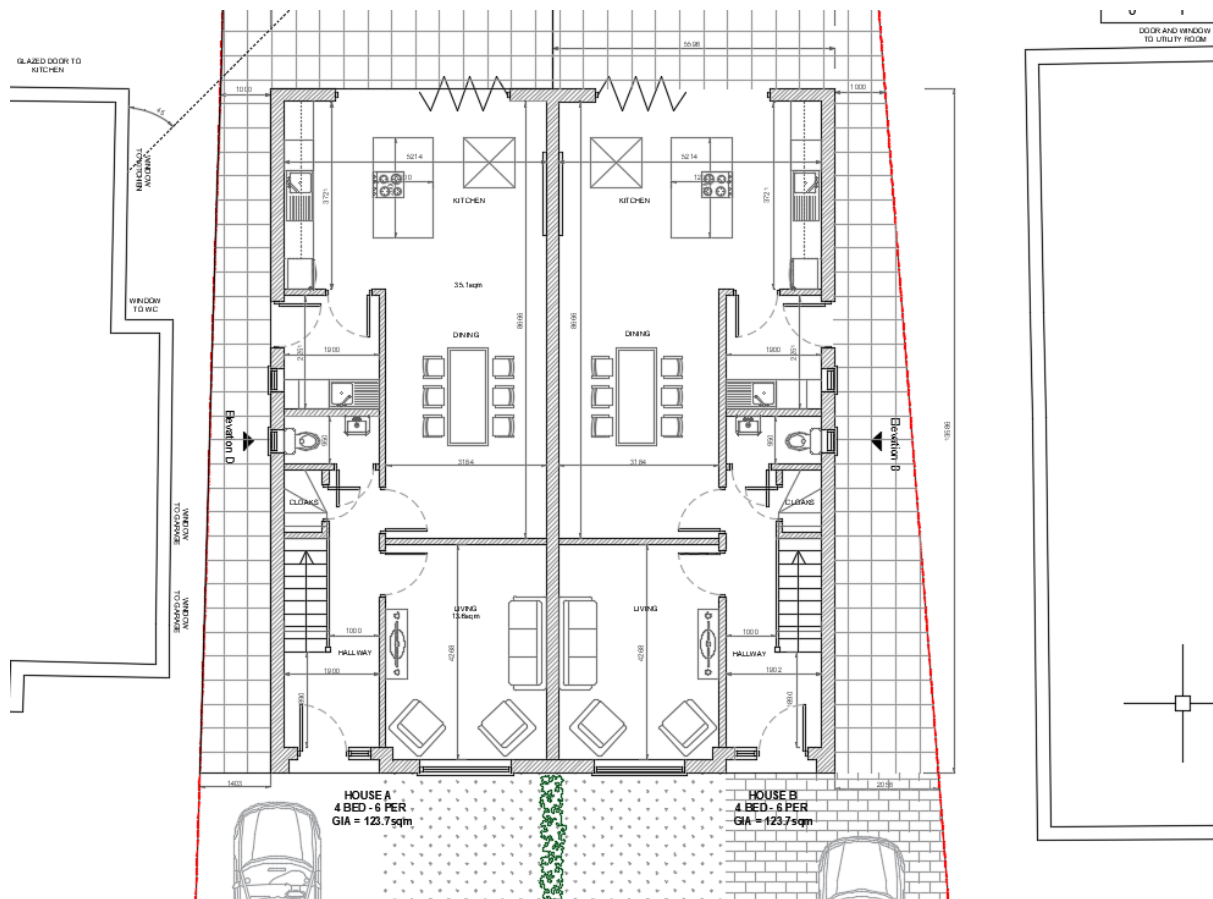


Fig. 8: Proposed Ground Floor Plan



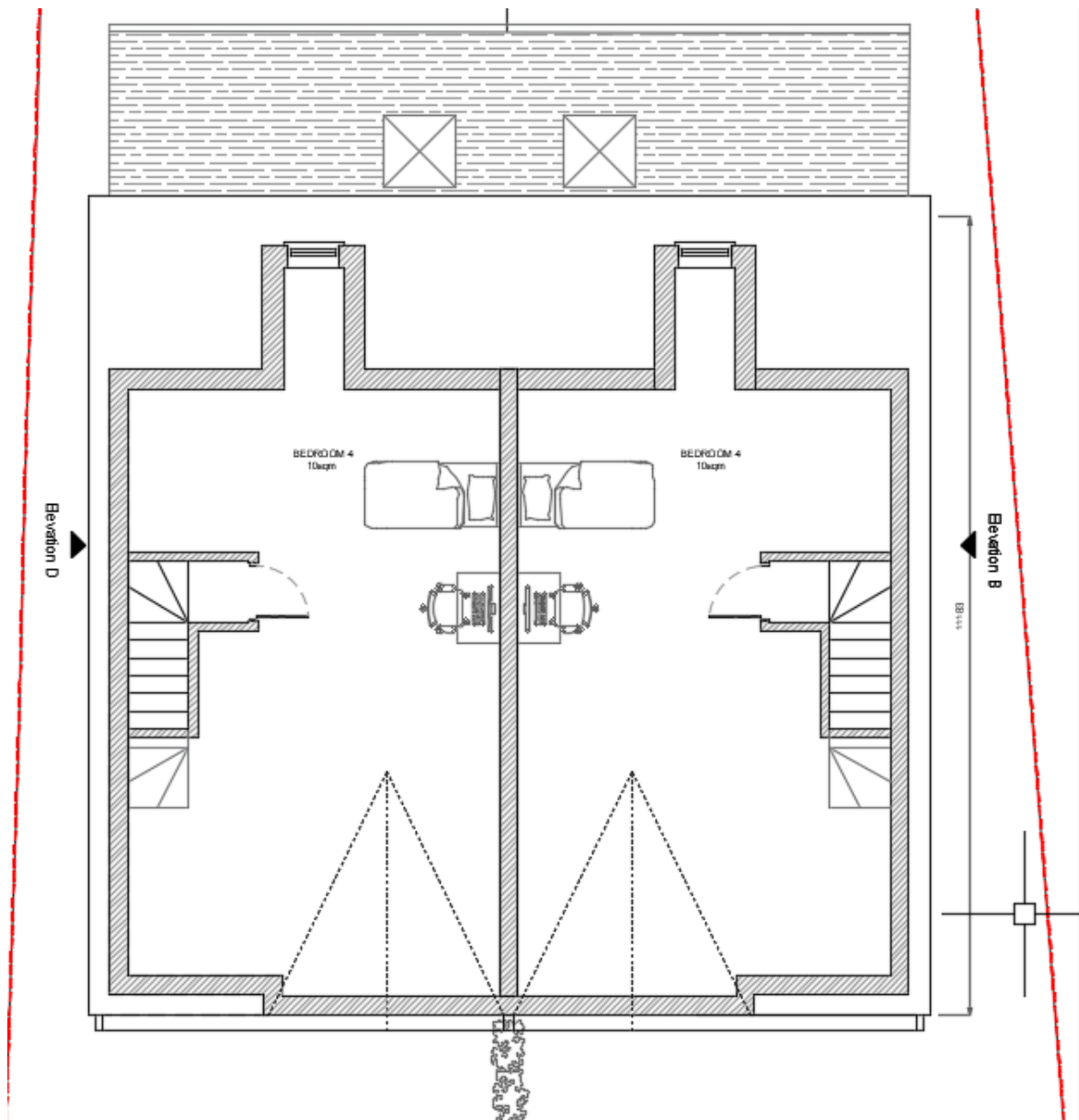


Fig. 10: Proposed Loft Floor Plan

Highways

- 3.14 The development site is located within PTAL 2. The 2 x four bedroom properties the proposal would therefore require a maximum of 2 car parking spaces to comply with the standards set out in the London Plan (2021).
- 3.15 Pre-app advice was therefore to retain only 1 space per dwelling house with soft landscaping. No changes would need to be made to the parking space (CPZ) on the road to accommodate a new crossover for the second property.
- 3.16 An electric vehicle charging point will be provided for each dwelling.

- 3.17 2 no. cycle spaces will be provided in the rear garden of each property. Each property will have a timber shed sized to house 2 cycles.
- 3.18 Refuse space will be provided to the front of each property behind the timber gate out of sight. These can be wheeled out to the roadside on bin collection day.

Noise and Vibration

- 3.19 As the Site is already in residential use, the issue of noise and vibration will be dealt with via the installation of triple glazing and appropriate Building Regulations measures.

Accessibility Strategy

- 3.20 Level access will be provided to approach and gain access to the dwelling. The approach route will be gently sloping up towards the private entrance to each dwelling house. All external parts of the approach route will have a suitable ground surface i.e. block paving. The approach route will be min. 900mm wide with a maximum cross fall 1 in 40 outside the area of a parked car.
- 3.21 The principal door will have a min. clear opening width of 775mm and will not impede wheelchairs.
- 3.21 Vehicle parking is provided within the curtilage. Access to the dwelling will be from the point of exiting from a car.
- 3.22 A disabled person who is able to walk is able to visit the dwelling house – the ground floor is fully accessible.
- 3.23 Visitors can access and use the habitable rooms and a WC within the entrance storey of the dwelling. There are no changes in level within the entrance storey, all doors are compliant with Approved Document M Part 1.
- 3.24 Wall mounted switches and socket outlets in habitable rooms will be reasonably accessible to people who have reduced reach.

4.0 Conclusion

The pre-app advice stated that the principle of development is considered to be acceptable and that the following should be provided:

Provide amended parking layout showing 1 parking space per dwelling and no conflict with on street parking bay.

Refer to ground floor plan showing parking space and crossovers.

Provide cycle parking layout

Indicate Refuse Storage

Provide policy vehicular accesses.

Soft and hard landscaping plans

Additional info regarding the impact of the proposal on neighbouring side windows and what those windows serve.