

2.4 CONSENTED ILLUSTRATIVE SCALE AND MASSING



NOTE: ALL IMAGES ON THIS PAGE ARE EXTRACTS FROM THE CONSENTED DESIGN AND ACCESS STATEMENT FOR THE HAYES TOWN CENTRE ESTATE HYBRID PLANNING APPLICATION (PLANNING REF. 76550/APP/2021/4499)

The massing of the consented scheme varies across the site from two and twelve storeys.

Consented Massing Strategy for Canalside Urban Block

The Canalside urban block ranges between six storeys and twelve storeys, with the tallest blocks fronting the Grand Union Canal, responding to the taller context of the new and emerging developments along the canal.

The twelve storey block is intended to create a legible figure which signposts the connection between the Canal and Silverdale Road.

All buildings within the scheme are designed to fit within the Aviation Exclusion Zone across Hayes with the exception of the 12 storey building. Heathrow Airport and NATS have agreed that in principle a 12 storey building would be acceptable in this location if not taller than Skeffington Court. Skeffington Court is 15 storey however considered to be equivalent in height to the proposed 12 storey height.

Consented Massing Strategy for New Street

The three identical urban blocks between Crown Close and Austin Road are mid-rise blocks between six and eight storeys. The heights of these tight urban blocks have been determined by daylight sunlight to achieve good levels within the shared central gardens.

Consented Massing Strategy for Houses

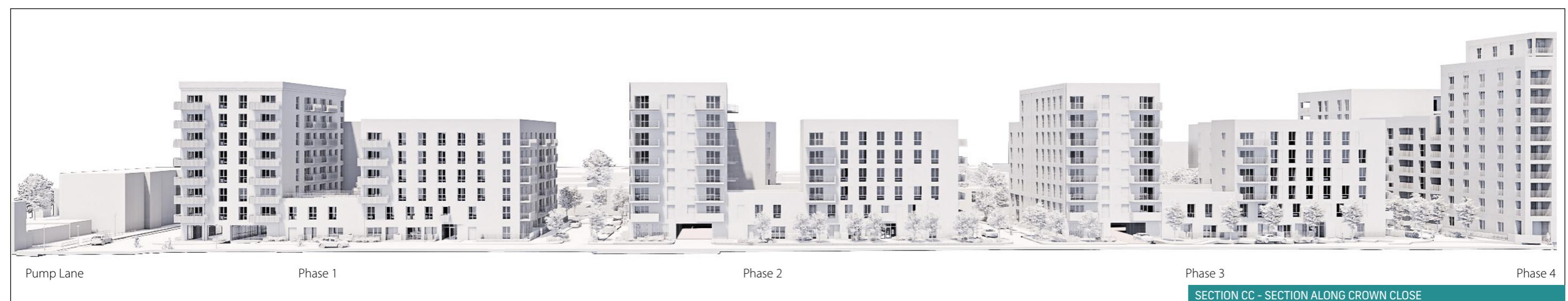
The consented scheme proposes two storey houses along the narrow strip fronting Austin road, backing onto the rear gardens of the adjacent houses responding sensitively to the surrounding context.



CONSENTED ILLUSTRATIVE SCALE AND MASSING



NOTE: ALL IMAGES ON THIS PAGE ARE EXTRACTS FROM THE CONSENTED DESIGN AND ACCESS STATEMENT FOR THE HAYES TOWN CENTRE ESTATE HYBRID PLANNING APPLICATION (PLANNING REF. 76550/APP/2021/4499)



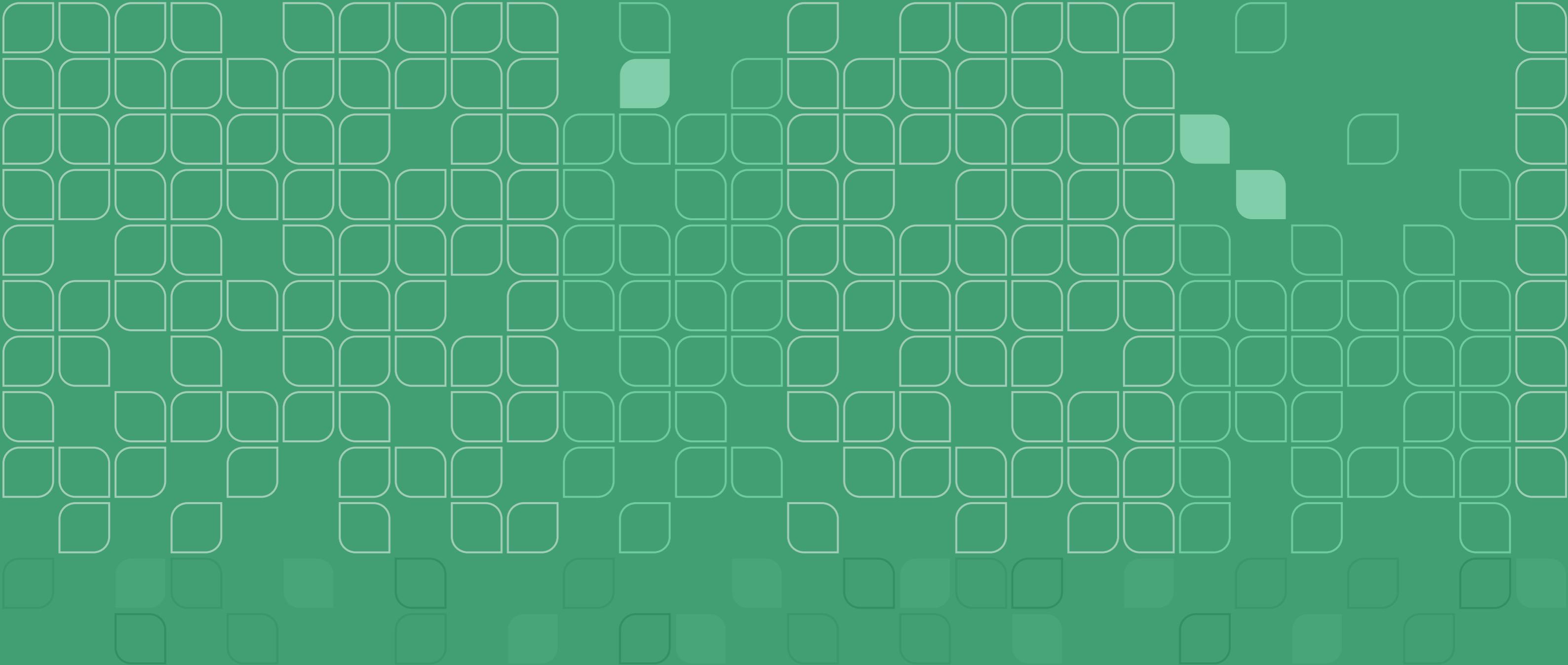
CONSENTED ILLUSTRATIVE SCALE AND MASSING



NOTE: ALL IMAGES ON THIS PAGE ARE EXTRACTS FROM THE CONSENTED DESIGN AND ACCESS STATEMENT FOR THE HAYES TOWN CENTRE ESTATE HYBRID PLANNING APPLICATION (PLANNING REF. 76550/APP/2021/4499)



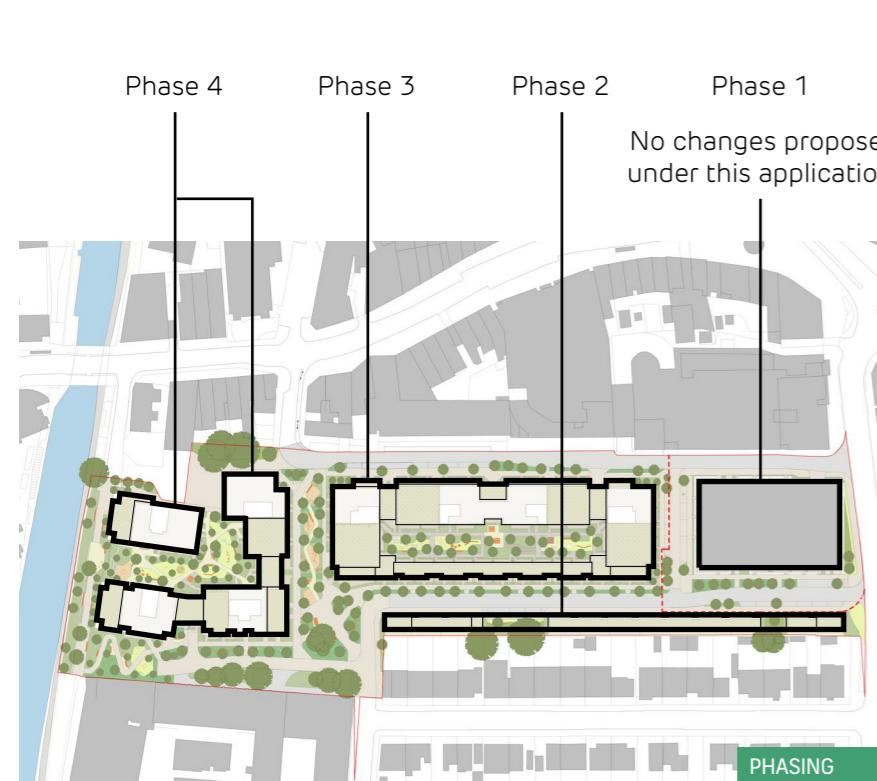
CONSENTED ILLUSTRATION DEMONSTRATING POTENTIAL FOR VARIETY ALONG AUSTIN ROAD WITHIN PARAMETER BUILDING HEIGHTS



3. DESIGN EVOLUTION & STAKEHOLDER ENGAGEMENT

3.1 VISION

The hybrid permission seeks to regenerate the existing 1960's housing estate, create a new neighbourhood which is safe, more secure and a better place to live for existing and future residents. Working closely with LB Hillingdon, local residents and the wider consultant team, the application team seek to retain the principle of creating a better place to live whilst utilising opportunities to make positive improvements to the consented vision.



INCREASED QUANTUM

More overall homes, more affordable homes, larger family-sized homes



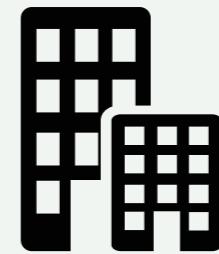
PEDESTRIANISATION PRIORITISATION

Pedestrian prioritisation in the public realm including level access, by using a robust servicing strategy



SIMPLIFIED PHASING

Revisions to the phasing strategy are proposed, for simplicity and to create a safer and more coherent street pattern. The consented phases 2/3 would merge into the proposed phase 3, and the consented phase 5 renamed to proposed phase 2. Please refer to proposed Site Phasing Plan for details.



GOOD PLACEMAKING AND WAYFINDING

Redistributed heights and refined massing strategy to emphasise good wayfinding and placemaking principles



INCREASED PLAYSPACE

More communal playspace for residents and public play-on-the-way spaces integrated into high quality public realm.



ACTIVE STREET FRONTAGES

Improvements to active frontages and street engagement to provide natural surveillance and increased safety in the community



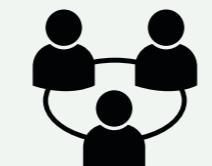
RETAINED TREES

An uplift in the number of retained trees on the site integrated into high quality public realm.



HIGH-QUALITY DESIGN

High-quality residential homes designed to provide good day lighting and maximised views



COMMUNITY CENTRE

A new community centre providing an uplift on area and design quality compared to the existing provision. This will be located at the heart of the new development.



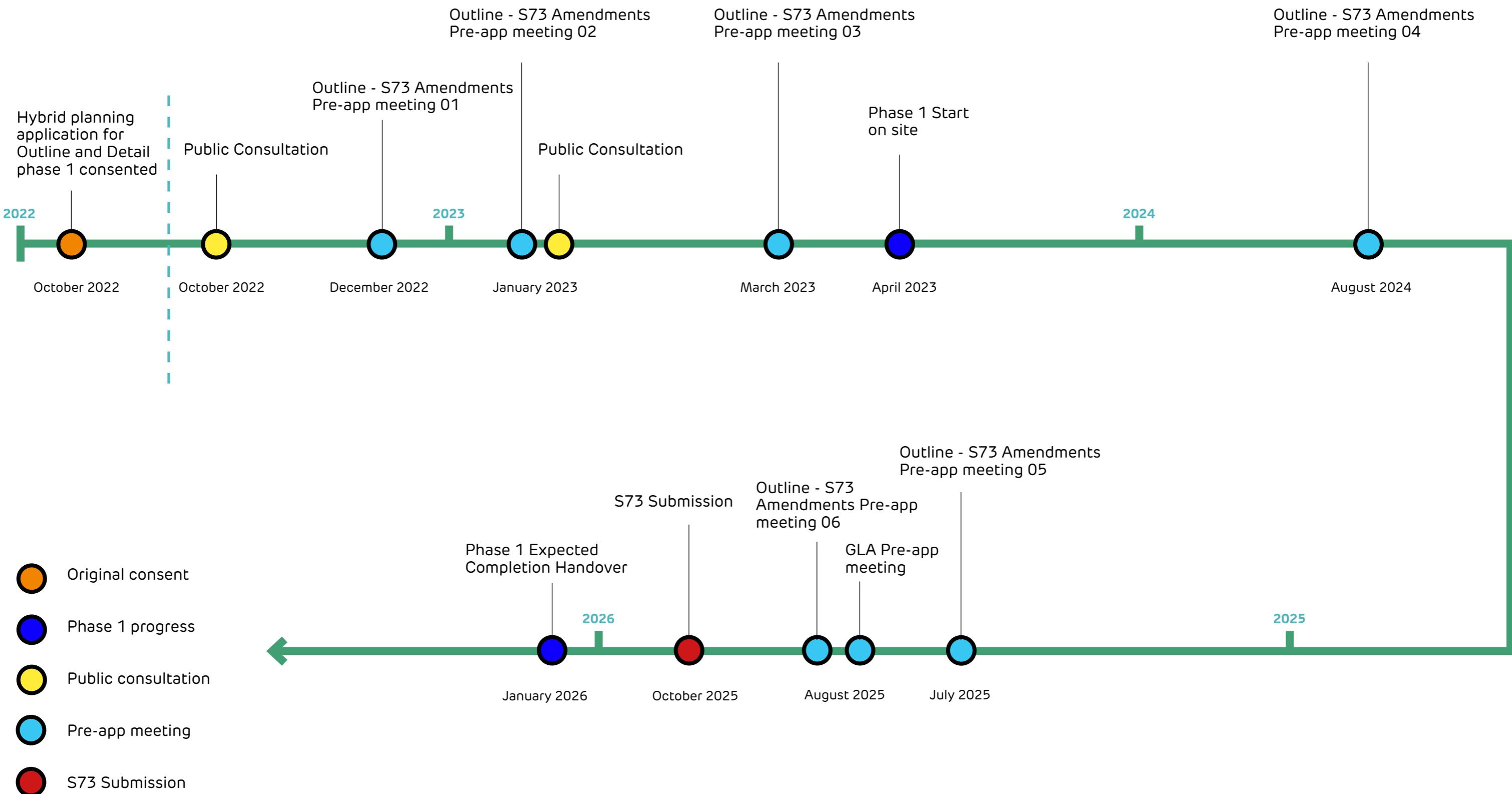
IMPROVED CANAL PATH

Improvements to the canal path with better accessible access from the proposed development

3.2 CONSULTATION TIMELINE



Extensive consultation with the LPA, GLA and the public has taken place in preparation for this Section 73 Application, dating back to late 2022. This is illustrated in the timeline below.



3.3 CONSULTATION

3.3.1 London Borough of Hillingdon - Pre-app 1 - 15th December 2022



At the first Pre-Application meeting the consultant team presented the proposed scheme, highlighting the differences compared with the consented. The key differences discussed were the changes to the Phasing; Community centre relocation; and a differing approach to the key massing strategy.

The council raised a number of queries and concerns which were addressed and formed the basis for the design evolution and Pre-Application meeting 2.

SUMMARY OF FEEDBACK

Separation Distance

- Austin Road (relationship to Little Road)
- Crown Close (Overhang & Residential Lobby)
- Distance to locally listed building on Silverdale Road
- Canal side
- Community Centre visibility

Public realm

- Canal-side walk
- Connections to/from canal-side walk

Overshadowing

- Transient shadow study for the community square

Dual aspect

- Consented scheme dual aspect ratio estimate
- Proposed dual aspect ratio

Vehicle access and parking

- Silverdale Road and Austin Road connection
- Phase 2-3 podium parking access
- On-street parking along Crown Close
- Overall parking ratio
- Cycle store

General

- Mix & Area comparison
- Like for like comparison on Parameter Plans
- Phasing



CONSULTATION

3.3.2 London Borough of Hillingdon - Pre-app 2 - 26th January 2023



KEY CHANGES SINCE PRE-APP 1

- Building separation distances increased in strategic locations around the site to give more breathing space between the proposal and the existing context
- Building heights updated based on D/S test
- Adjustments to the proposed frontage of the community centre, thereby maximising its visibility from different angles
- Improvements to the inclusivity of the proposed public access along the canalside

to line both sides with trees to increase urban greening and SUDS.

- Request for Phasing Plan for play provision setting out clearly the different user groups. Allowance should be made that small provision on Austin Road that may be well used due to good daylight.
- Request exploration of localised cutting through canal-bank for level access
- Further details requested on space around retained trees between crown close and canal side
- Explore heritage with instrument or music making in public realm design

SUMMARY OF FEEDBACK

Transport

- Confirmation of pedestrian path to east of Austin Road and request for it to be a shared surface
- Request for Delivery Vehicles to have allocated bays.
- Confirmation of 3 point turn on Silverdale road tracking in relation to landscape strategy.
- Confirm cycle strategy, particular concern with contraflow on Crown Close - will speed restriction need altering?
- Allowance for Santander Cycles docking station
- How will scheme connect to wider national and London cycle network?
- Clarification of connection between upper canal side and western boundary.

Density

- Concerns over canalside and overshadowing of public realm from 2pm onwards.
- Connections to/from canal-side walk and Crown Close need clarifying
- Proposal to increase NE corner of phase 4 from 8-10 storeys is not supported. Applicant asked to explore redistribution to improve afternoon sunlight provision.
- Officers happy with balcony strategy
- Request that upper canal path widened to 7m to allow align with consented area.
- 8 sketch views requested

Community Centre

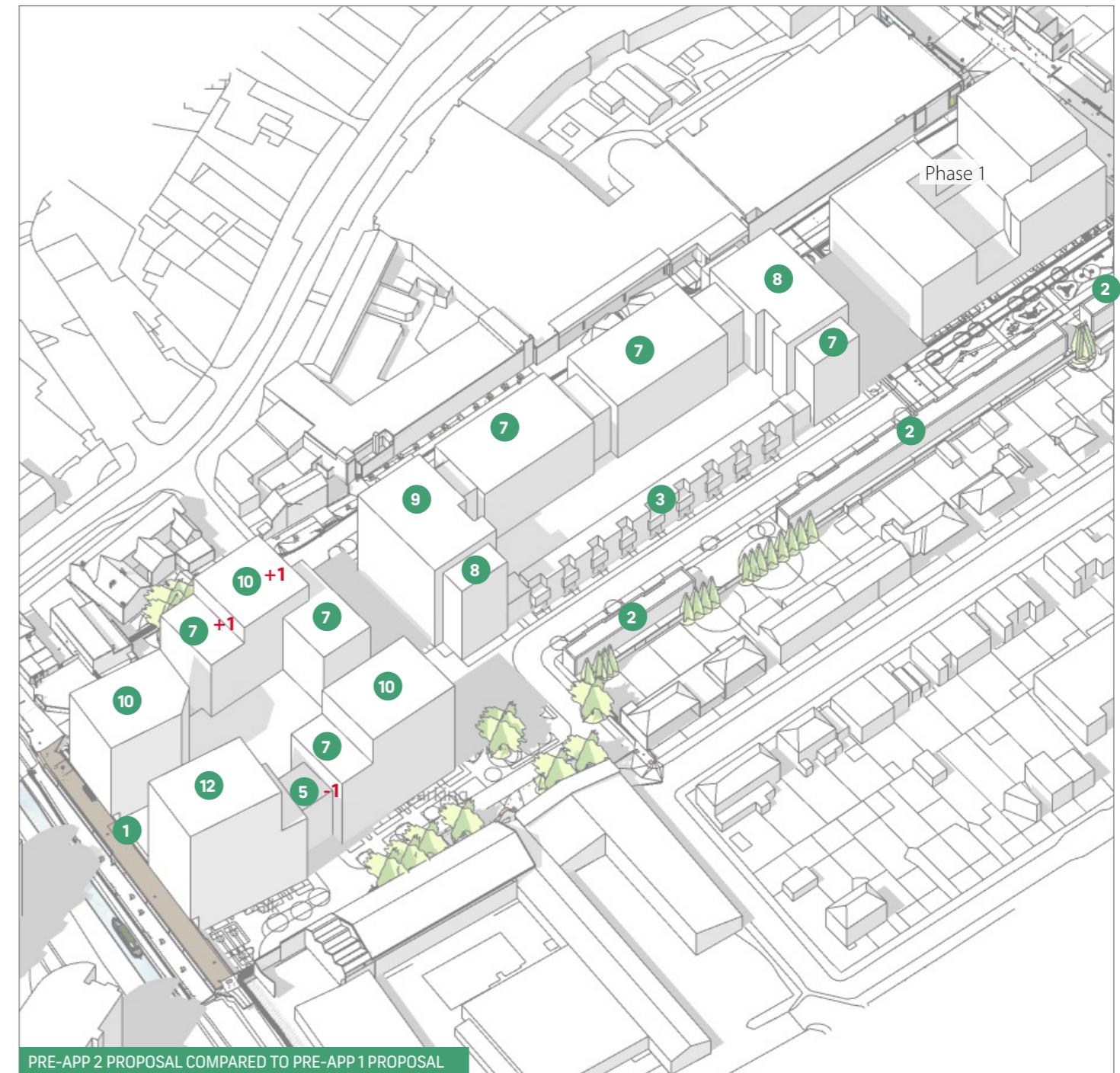
- Removal of two trees in front of Community Centre and entrance brought forward to improve legibility

Residential

- Breakdown numbers of Duplexes, houses and Flats.
- Request for majority of units to be dual aspect - suggest 5 units to be lost to increase dual aspect by 15.

Materials

- Inspiration for Building Identity could be taken by Peter Hounsell's book 'Bricks of Victorian London' who contains an illustrative chapter about brick making along the Grand Junction Canal in West Middlesex.
- Applicant should have a strategy to re-use some of the existing materials in line with LPG Whole Life-Cycle Carbon Assessment.



Landscape and Public Realm

- Request for combined roof and ground landscape plan.
- Suggestion to increase urban greening along canal.
- Confirmation of tree strategy on Austin Road and suggestion

CONSULTATION

3.3.3 London Borough of Hillingdon - Pre-app 3 - 24th March 2023



KEY CHANGES SINCE PRE-APP 2

- Phase 4 building heights reduced due to the comments received from pre-app 02: "P4 height to be reduced. Depart from 3no 10 storey approach and create height variation. Explore redistribution of massing particular on northeastern corner of Phase 4 to address overshadowing of new square"
- Added 2nd stair to the 12th building in phase 4
- Building line set back from the canal

The changes resulted in a considerable loss of dwellings and this was discussed during the meeting. It was agreed additional height may be possible in certain locations, but shoulders should be introduced to the canalside and the community square and that this lowered edge should be reflected in the parameter plans as well as the illustrative masterplan.

SUMMARY OF FEEDBACK

Massing

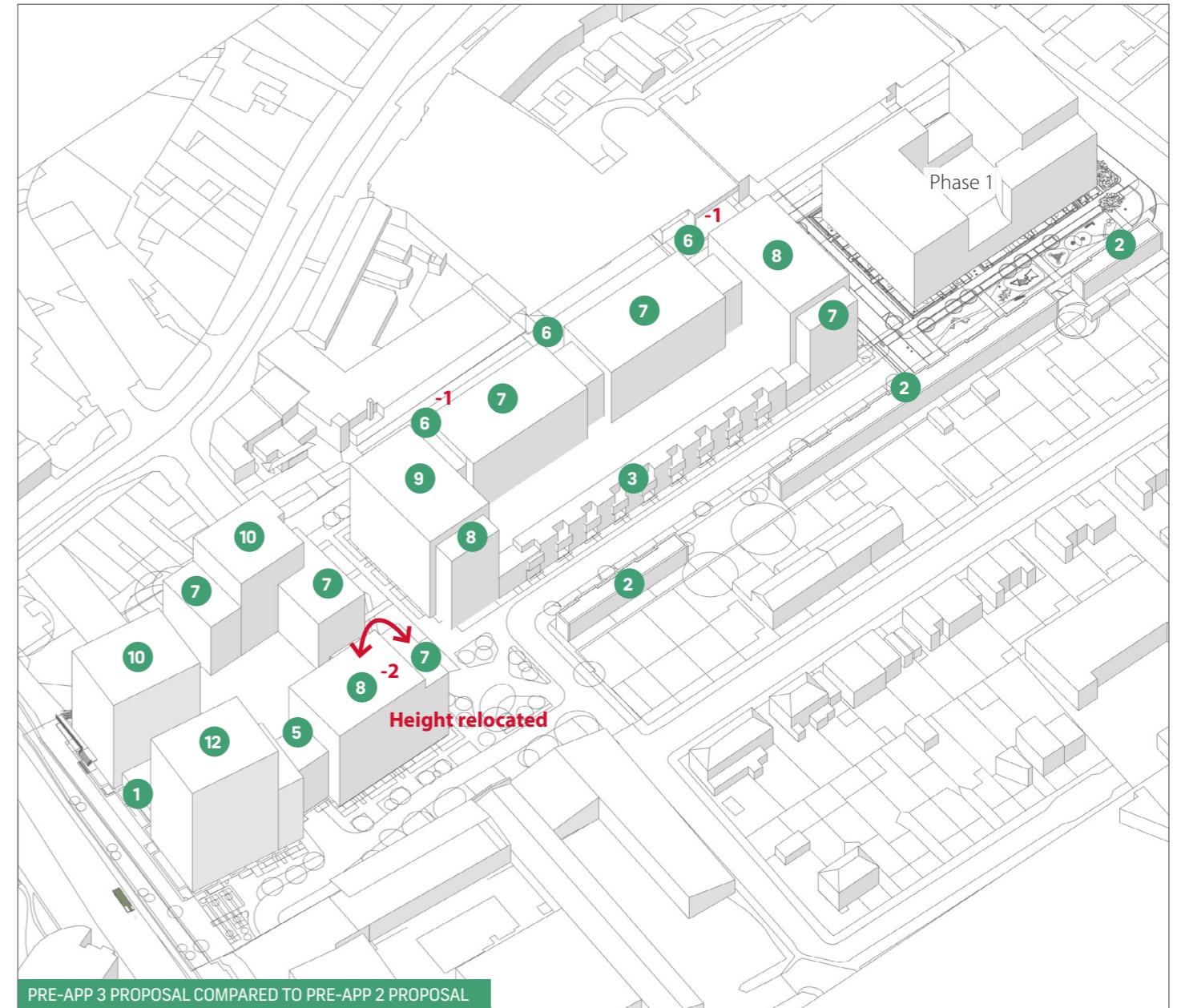
- Agreed there is scope to increase density and massing for the Austin Road proposal
- Phase 4 next to canal has the greatest potential to increase heights
- 9-storey (possibly with 7-storey shoulders) to 15/16 storeys building in phase 4 could probably be accommodated within the amended scheme, and justified in the planning balance.
- Make sure the buildings next to canal are parallel to the canal.
- One additional floor setback from the edge along Crown Close in Phase 2-3 have fairly limited impact.

Community Centre

- A standard residential floor-to-ceiling height for community centre would not be appropriate.

Landscape and Public Realm

- Improve the access between development and the towpath



CONSULTATION

3.3.5 London Borough of Hillingdon - Pre-app 4 - 19th August 2024



KEY CHANGES SINCE PRE-APP 3

- Additional height added in certain locations
- Added 2nd stair to the 12th building in phase 4
- Shoulders introduced to the canalside /community square
- Second stair added to all buildings over 18 metres
- Tenure change

- The proposed duplex units towards the south side of the site are acceptable. The quality of the proposed residential accommodation for these units should be tested further
- The proposed elevations should be reviewed in detail. The principle of a defined base, middle element and top floor elements should be considered to reduce the sense of massing.

SUMMARY OF FEEDBACK

Design

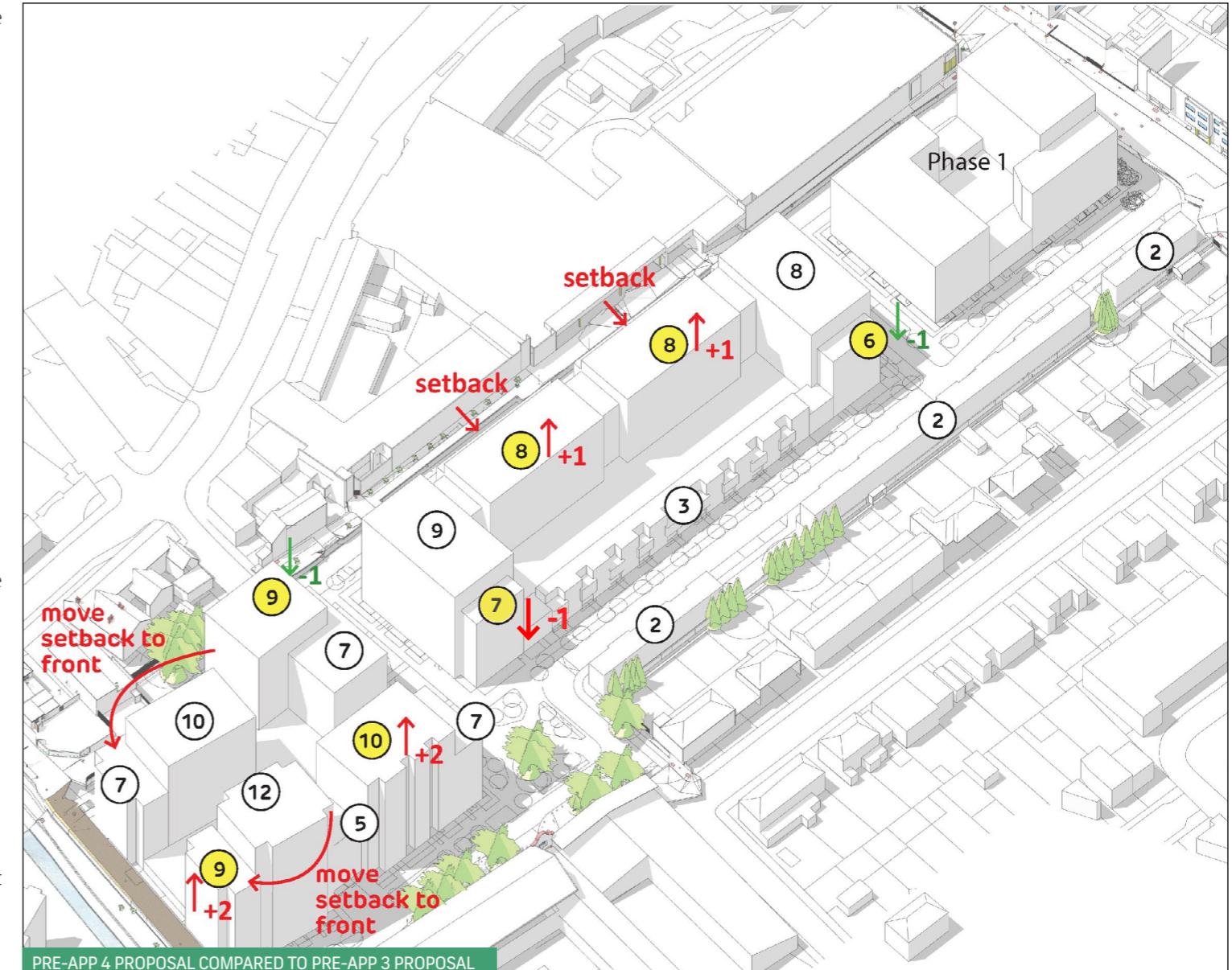
- Further details and scaled drawings that show elevational treatments, refuse collection points, servicing arrangements and emergency routes should be provided
- A microclimate assessment should be reviewed in due course, such as wind and overshadowing.
- The proposal should therefore resolve the changes in ground level through design and a comprehensive landscaping strategy, that secures pedestrian movements and permeability, particularly along Silverdale Road
- The approved 'community square' should be reinforced as a way to open movements towards the canal from Austin Road.
- The location of the 'community centre' is therefore supported
- A landscape-led design approach that softens any perception of building height should also be reinforced, particularly at Crown Close and Silverdale Road
- The option to re-locate the proposed entrance fronting the pedestrian link between Blocks 2 and 3 should be considered, to ensure that all the entrances towards the north side of the site are consistently distributed along Crown Close with a clear perception of street hierarchy.
- Overall massing along Silverdale Road requires further review, as this change should also acknowledge forthcoming developments towards the south side of the site and any perception of a narrow and uninviting street should be avoided
- Whilst the proposed public square is accepted, concerns over the quality of this communal amenity space should be noted, particularly by reason of overshadowing and limited floor area.

Quality of Residential Accommodation

- A sunlight and daylight assessment would be required for all dwellings, albeit specific concern with the above is noted
- The quality of the residential accommodation for the proposed duplex units along both sides of Austin Road should be reviewed.
- The separation distance between habitable windows would appear acceptable
- The units at first floor level overlooking the podiums should contain defensible spaces, considering that these windows would be directly overlooked by the communal amenity areas
- Details of wheelchair accessible units should be provided

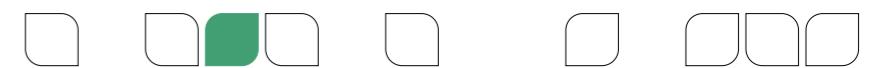
Highway

- Car Parking Management Scheme and Transport Assessment would be required for further review
- No details of cycle parking stores for the different building elements have been presented at this stage but these should be conceived to meet regional standards
- In terms of refuse, waste collection distances between a designated bin store and a refuse vehicle should not exceed 10m to comply with waste collection standards (with carrying distances from each residential unit to a storage area not exceeding a distance of 30 metres)



CONSULTATION

3.3.6 London Borough of Hillingdon - Pre-app 5 - 23rd July 2025



KEY CHANGES SINCE PRE-APP 4

Masterplan improvement

- Car-free approach is proposed for phase 4. The podium parking has been removed. Landscaped ramp and seating steps are proposed to improve the canal side connections
- Podium parking access along Silverdale Road has been changed into servicing and emergency access only to create a better pedestrian environment.
- 7 Accessible on-street parking spaces are proposed here to leave the court yard completely car free for soft landscape/planting/play space

Compliant with the latest fire regulations

- All the buildings are designed to BS9991:2024

Tenure mix improvement

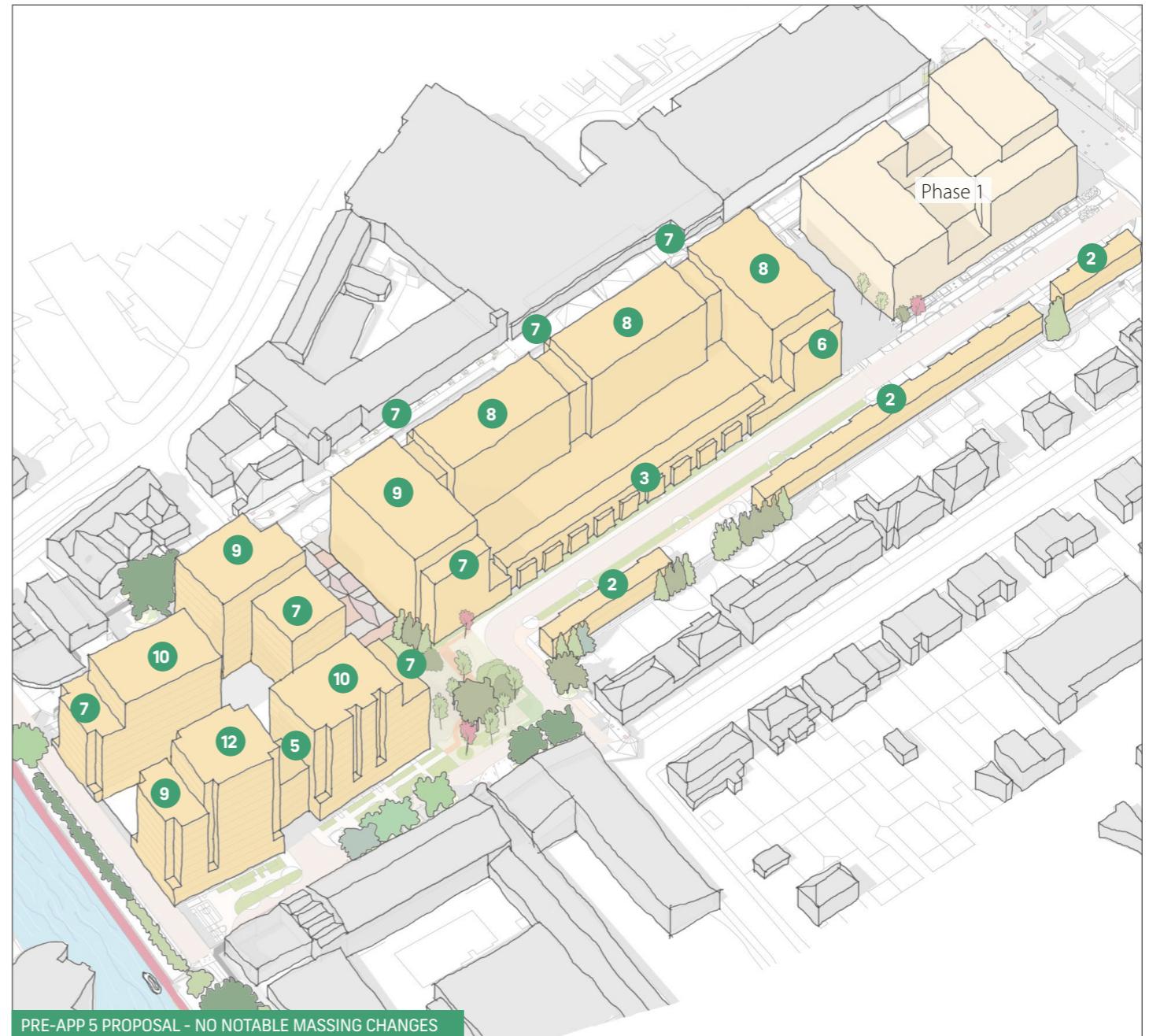
- More balanced tenure mix
- The majority of the duplexes and town houses are social rent tenure

SUMMARY OF FEEDBACK

- Update on Phase 4 buildings facing the canal is a positive change and reduces the overall bulk of the buildings, where viewed from the Canal and Hayes Town Centre. The trade off in massing has been achieved by transferring the floor space to the central Phase 3 block, and whilst this has added height to the building, the additional floor has been recessed and does not appear harmful in design terms.
- The Phase 2 houses are supported in principle and are an efficient use of space – further detail needed.
- Whilst the preference would be to provide a higher percentage of dual aspect units, it is noted that the dual aspect percentage has increased compared with the consented scheme and could be accepted on this basis.
- Works within the Canalside area will need to consider proposals currently in development for enhancing this space. These works may progress in advance of Phase 4 of the development.
- The access ramp arrangements are convoluted and overly engineered, to be reviewed to provide a more practical and straightforward route.
- Planning application 71374/APP/2016/4027 on the adjoining site to the east was granted approval in July 2019 and has expired. Whilst the approved plans showed access being provided into this site via Silverdale Road through the development site, this would be a suboptimal arrangement

with access from the north being more suitable. The consent on the Austin Road site is extant and is a material consideration in terms of considering negotiations around access into the adjoining site and any vehicular access into this adjoining land would cut across the public open space compromising its quality and usability. Taking this into consideration, provision of vehicular access would not need to be included in the scope of any application.

- The extent of public open space will need to be clarified in terms of sqm area. There is an increase in residents compared with the consented scheme so a comparison is needed in terms of the delivery of open space, ideally an uplift in the quantity of public open space would be expected.
- It is positive that the UGF target of 0.4 is exceeded, although this is marginal and a further increase in greening is encouraged to ensure that this can be more comfortably achieved.
- Play provision calculation needs to be clearer and must not include more general areas of public realm, private amenity, or access paths/roads.
- The boundaries between Phases 3 and 4 could be more coherent and the delivery of a larger part of the public open space and children's play area, which sits between Phases 3 and 4 on the Phasing Plan could be implemented at Phase 3 to allow the residents and the public access to these spaces.
- Clarification is required on affordable housing provision across the site as a percentage by habitable room.
- Car-free approach to phase 4 was well-received.



CONSULTATION

3.3.7 GLA pre-app - 13th August 2025



SAME SCHEME PRESENTED TO THE GLA AS AT LBH PRE-APP 5

SUMMARY OF FEEDBACK

Development layout and Public realm

- The joining of the two central perimeter blocks is the key change from the consented scheme, reducing permeability and creating a long continuous block.
- The central space is larger than consented; design should be informed by sunlight/daylight assessments to locate play and relaxation zones optimally.
- The large central block's building line has been pushed east, compressing space for the road and mews terraces; the mews is supported in principle, but consider moving the central block back or creating spaces between mews homes to avoid a continuous terrace.
- Ensure the calculated play space is distinct from general public and communal amenity space.
- Pump Lane frontages requires further review; entrances to affordable housing blocks should improve street presence.
- Public realm should follow the Mayor's Healthy Street guidance and reflect Good Growth by Design; consider how spaces feel at different times of the day to ensure they are attractive and perceived as safe at all times.

Tall buildings, Scale and Massing

- The revised arrangement allows better central sunlight; address massing with elevational and roofline variety and articulation to break down long elevations.
- Daylight studies indicating internal daylight and impact on neighbours should be provided with any future application.

Residential quality

- The layout includes north-facing single aspect homes, including 2B homes; this should be addressed; reduce reliance on double-loaded corridors and demonstrate how good passive ventilation is achievable if pursued.

Delivery and servicing

- It has been indicated that part of Silverdale Road could become pedestrianised. As presented at the GLA pre-application meeting, Silverdale Road also part of the site's delivery and servicing strategy; this approach, as indicated by the applicant, is subject to further review. Should Silverdale Road be used, appropriate design and management measures should be implemented to ensure that any potential conflict between different modes is minimised



CONSULTATION

3.3.8 London Borough of Hillingdon - Pre-app 6 - 29th August 2025



KEY CHANGES SINCE PRE-APP 5

- Improved connection between canal bank and Silverdale Road - fully accessible
- Improved connection between canal bank and phase 4 courtyard - visual connection with no direct access.
- Centralised lobby in phase 3 to improve entrance experience
- Internal entrance/access provided to most Ground Floor dwellings
- Vehicle access along Silverdale road reduced
- Substation access provided
- Slight reduction in parking spaces
- Cycle Layout update
- An increased number of larger, family homes and less 1 bedroom units

SUMMARY OF FEEDBACK

- Massing and storey heights are supported.
- Calculation of play space accepted in principle.
- Proposed parking ratio accepted.
- Improved connections between canal bank and Silverdale Road was well-received.
- Centralised lobby to phase 3 supported in principle.
- Changes to accommodation mix well-received.
- Detailed layouts to houses in phase 2 requested at RMA stage along with successful precedents.

