



ARCHITECTURE



# 25 - 49 VICTORIA ROAD, RUISLIP MANOR.

C4163 DS01 DESIGN AND ACCESS STATEMENT

December 2023



placemaking for life.

# 25 - 49 VICTORIA ROAD, RUISLIP MANOR | Design and Access Statement

## CONTENTS

1.0	Introduction	03
2.0	Contextual Analysis	04
3.0	Site Analysis	10
4.0	Planning History	17
5.0	Pre-application Advice (Feedback)	24
6.0	Design Proposal	26
7.0	Summary	36

## 1.0 Introduction | Purpose of document

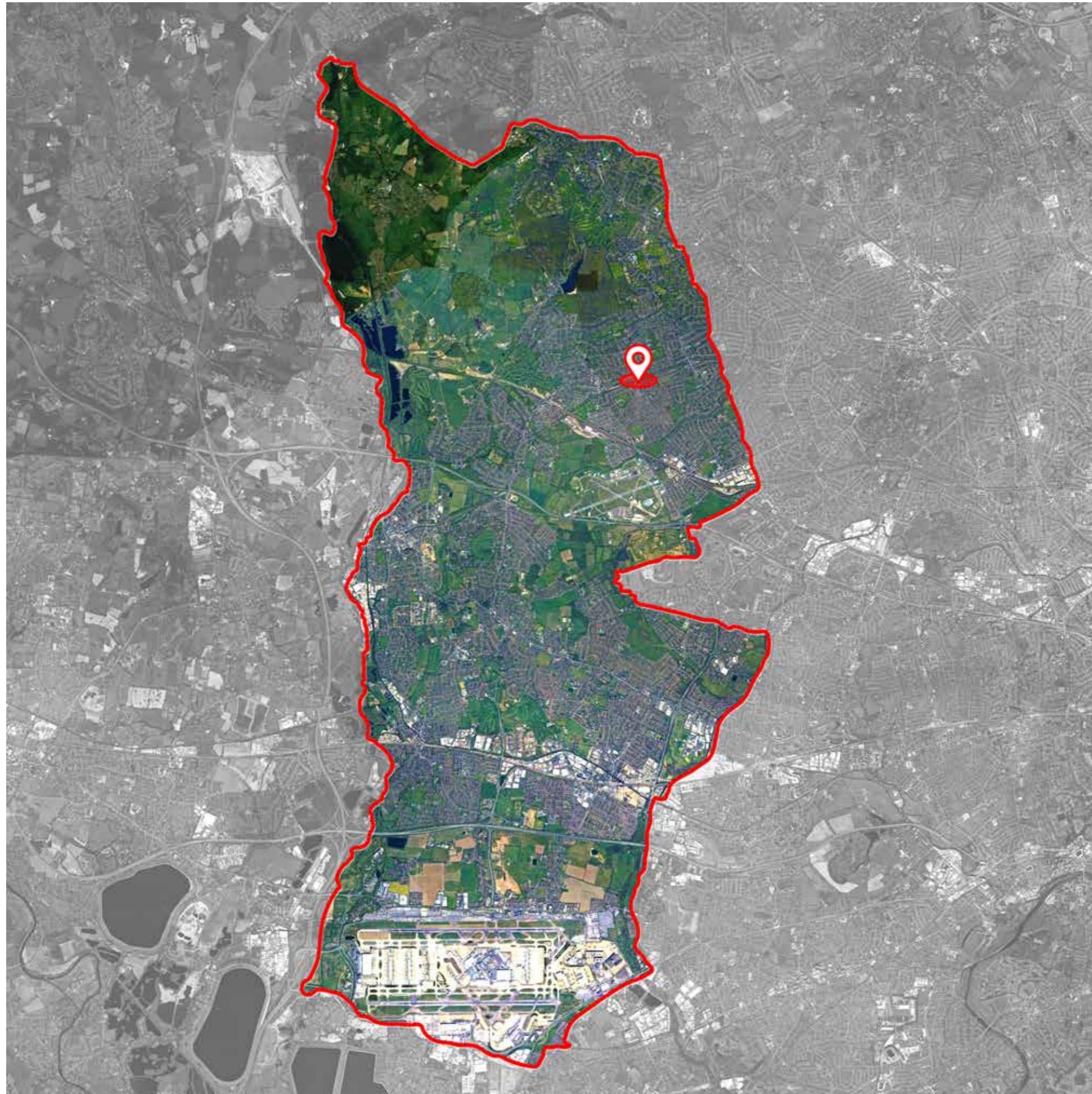
- 1.1.1 This application seeks approval for the extension of the existing buildings along 25-49 Victoria Road, providing an additional 9no. residential dwellings (use class C3) and 26sqm of new commercial floorspace (use class E).
- 1.1.2 This document forms the Design and Access Statement for the Full Planning Application, and has been prepared to comply with the Town and Country Planning (Development Management Procedure) (England) Order 2015. This document addresses the use, accommodation, layout, scale, appearance and access of the proposal.
- 1.1.3 This document has been prepared by IDP on behalf of the client, Urbanhold Ltd. It has been developed through collaborative work with the client, our strong team of consultants, and Hillingdon Council to ensure a coherent and well-realised design proposal.
- 1.1.4 The application site measures approximately 0.25ha and is comprised of a series of 2/3 storey properties with ground floor commercial premises and residential uses at the upper levels.
- 1.1.5 The document should be read in conjunction with the documentation submitted in support of this application, listed below:
  - Daylight & Sunlight Assessment | T16 Design
  - Energy & Sustainability Statement | T16 Design
  - Healthy Streets Transport Statement | PJA
  - Refuse Strategy | PJA
  - Noise Impact Assessment | Quantum Acoustics Ltd.
  - Fire Statement | Accendo Fire Safety
  - Flood Risk & SuDS Drainage Strategy | EAS
  - Air Quality Assessment | Air quality Assessments Ltd.

## 2.0 Contextual Analysis

- 2.1 Site Location - Hillingdon
- 2.2 Site Location - Ruislip Manor
- 2.3 Photographic Survey
- 2.4 Policy and Background
- 2.5 Policy and Background



## 2.1 Contextual Analysis | Site Location - Hillingdon

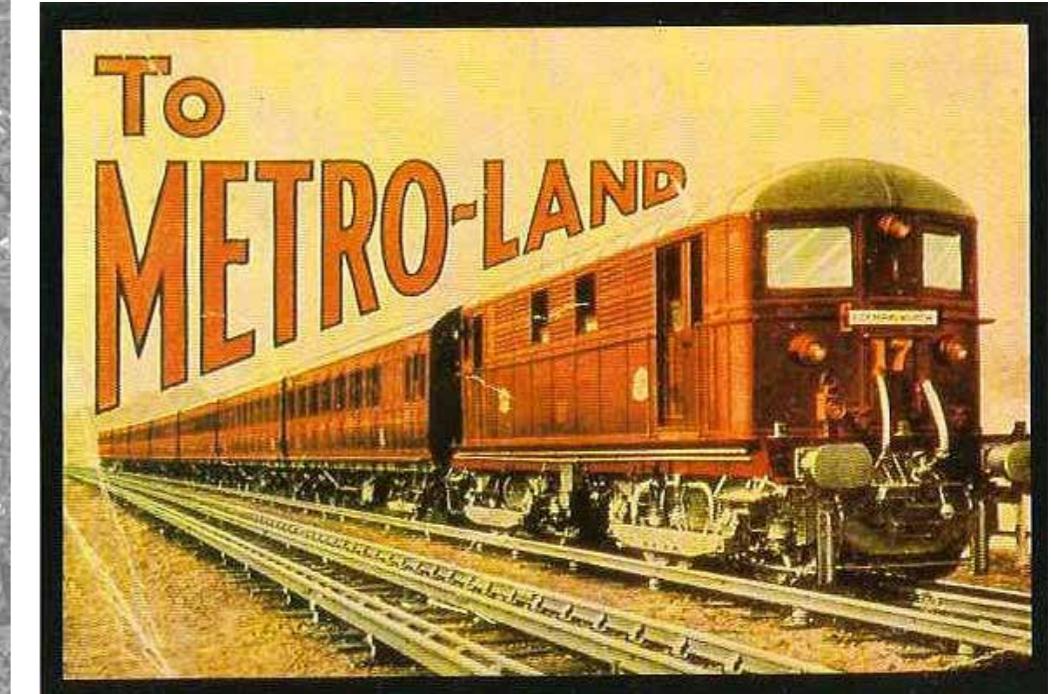


### Site Location

- 2.1.1 The site is located within the metropolitan neighbourhood of Ruislip Manor, sited within the north eastern portion of the borough of Hillingdon. Part of the Metroland development, Ruislip Manor sits centrally within this neighbourhood, an area centred along the spine of Victoria Road.
- 2.1.2 The arrival of the Uxbridge Metropolitan line in 1914 transformed the area, and the character of the area adjacent to the station is largely a result of the estate development which occurred between 1933 and 1939.

Left:  
Aerial photograph of the Hillingdon Borough

Below:  
Metroland, the term coined to describe the countryside to the North-West of London. Promoted as an idyllic area to live in, with fast connection to central London via railway service.



## 2.2 Contextual Analysis | Site Location - Ruislip Manor

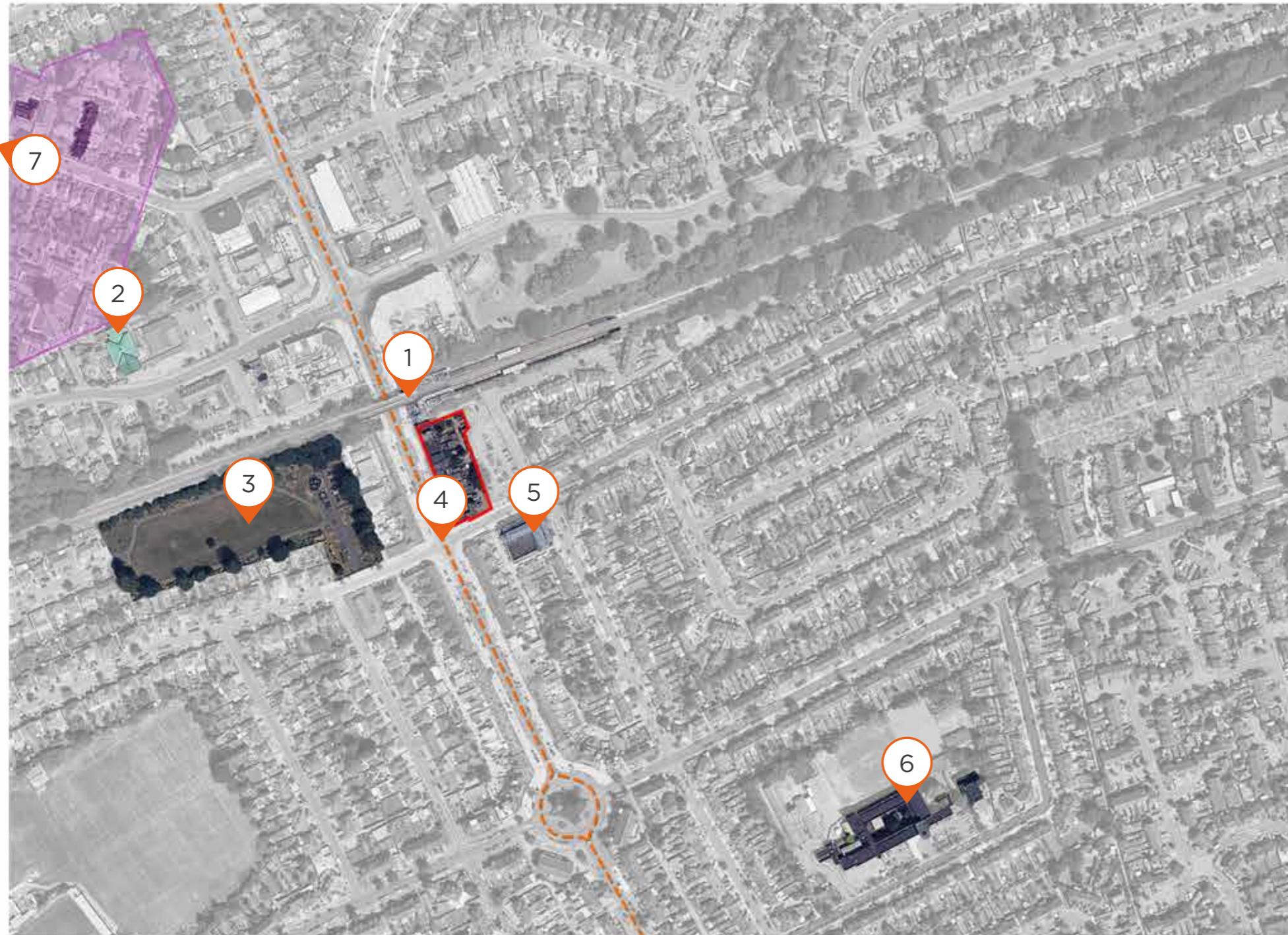


Fig. 1 | Ruislip Manor Station (0.11km)

Fig. 2 | Church Of The Most Sacred Heart (0.34km)

Fig. 3 | Shenley Park (0.1km)

Fig. 4 | Ruislip Manor Town Centre (0.04km)

Fig. 5 | Ruislip Manor Library (0.02km)

Fig. 6 | Lady Banks School (0.53km)

Fig. 7 | Ruislip Town Centre (600m via Brickwall Lane)

— Primary Access Route

Ruislip Manor Way Conservation Area



Fig. 1



Fig. 2



Fig. 3



Fig. 4



Fig. 5



Fig. 6

## 2.3 Contextual Analysis | Photographic Survey



Fig. 1 | View from Ruislip Manor Station (Looking South).

Fig. 2 | Victoria Road (Looking South).

Fig. 3 | Linden Avenue (Looking East).

Fig. 4 | Site Entrance. Looking from London Avenue.

Fig. 5 | Linden Avenue (Looking West).

Fig. 6 | Linden Avenue Car Park.



Fig. 1



Fig. 2

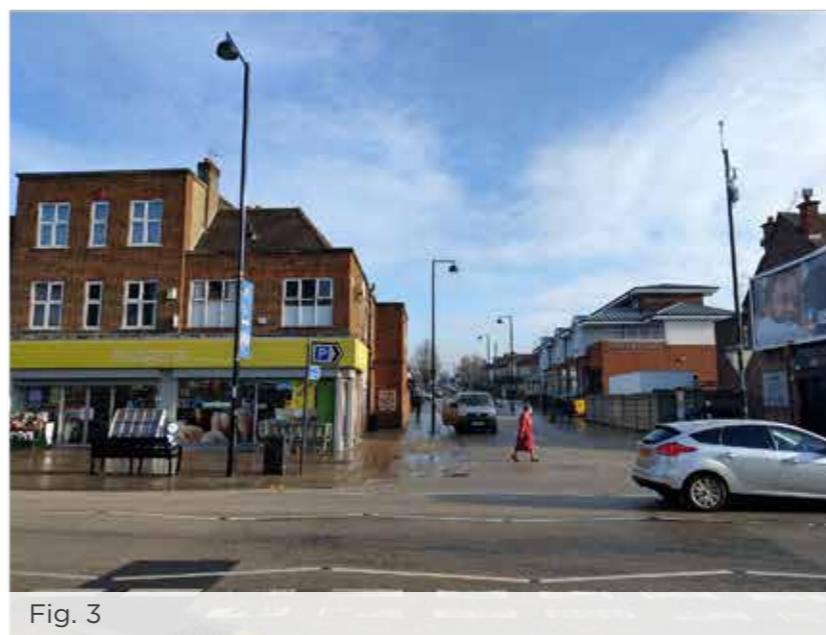


Fig. 3



Fig. 4

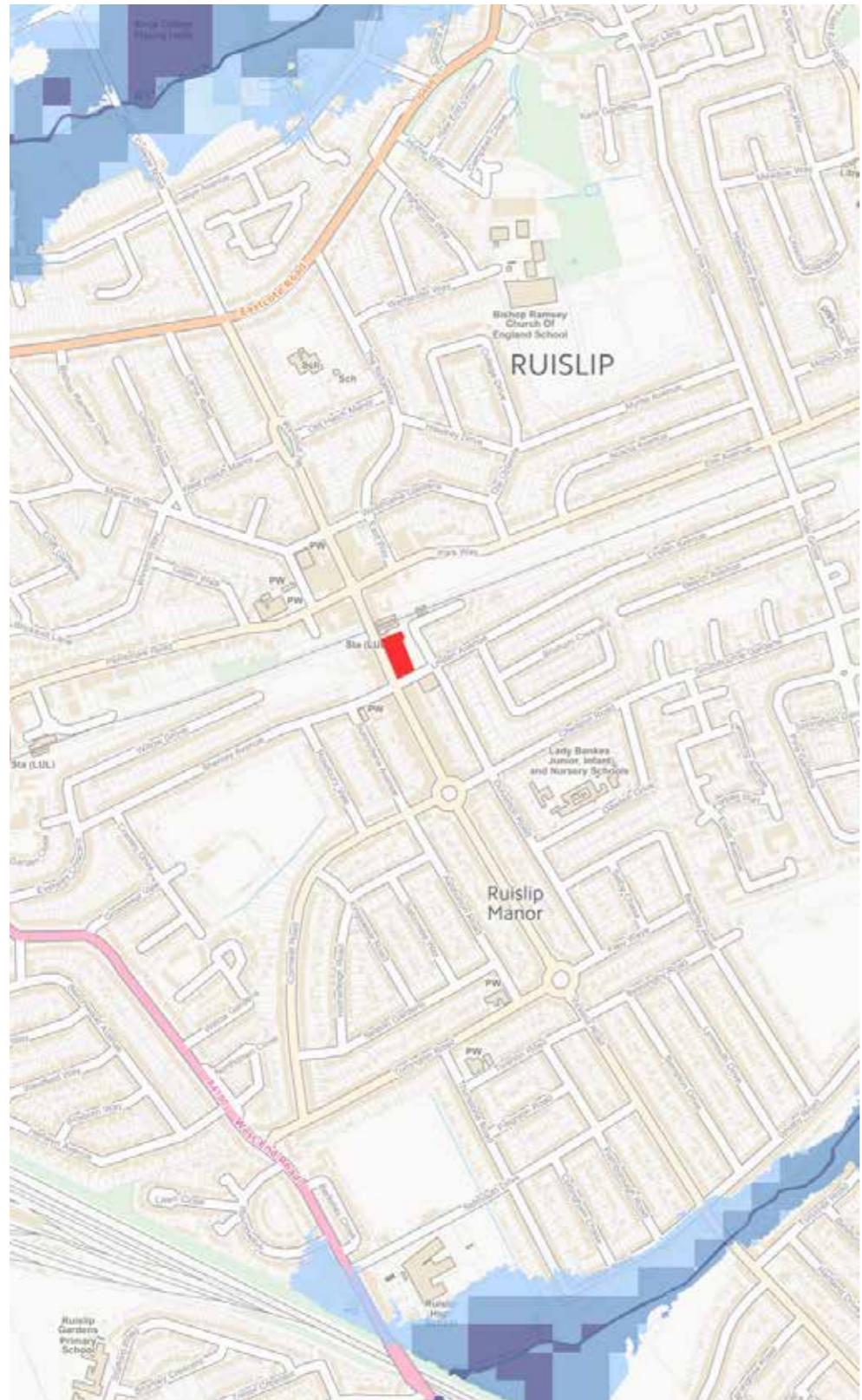


Fig. 5



Fig. 6

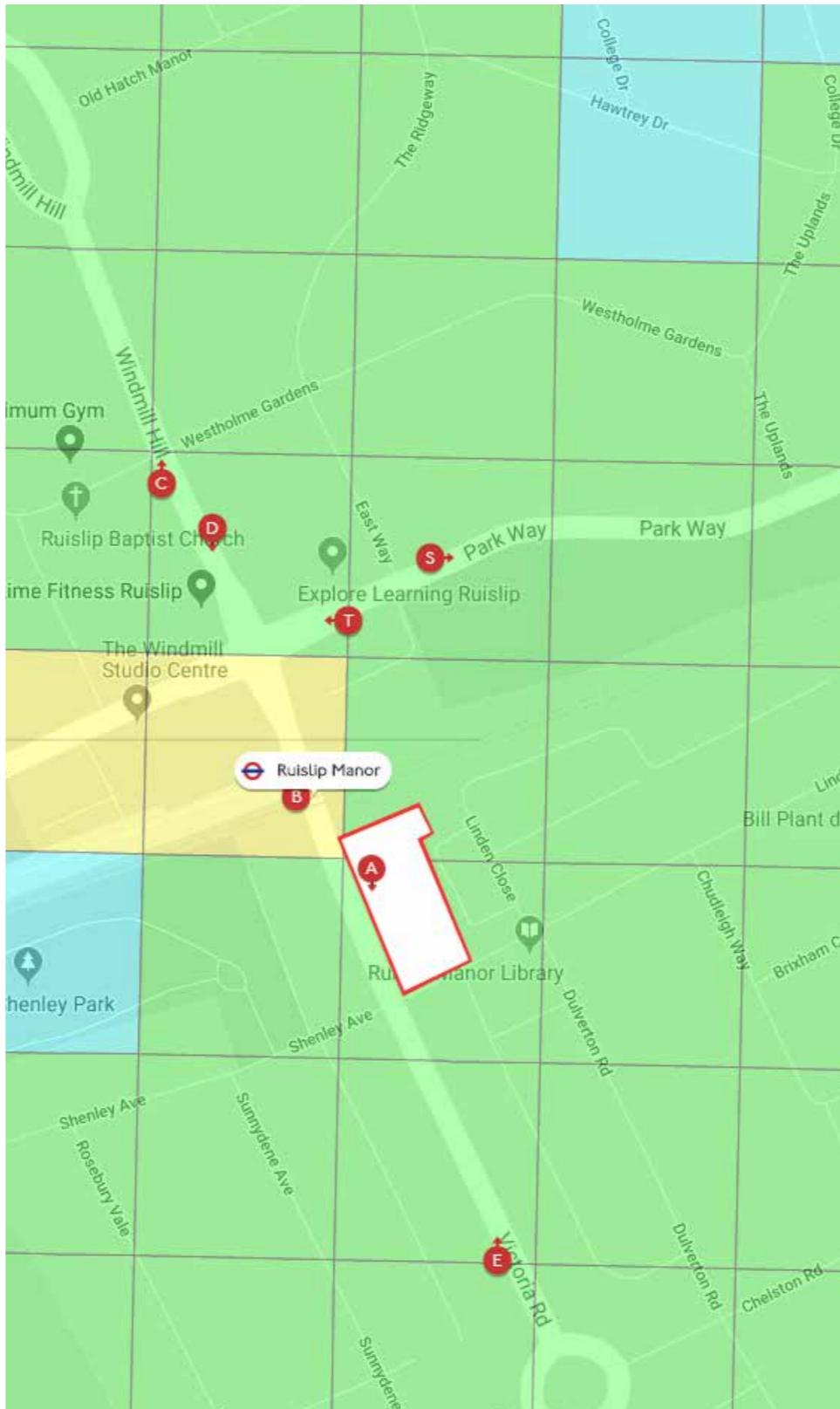
## 2.4 Contextual Analysis | Policy and Background



Flooding

Risk Level: Very Low Risk

The site has a chance of flooding of less than 0.1%  
(Includes risk from rivers and surface water).



Public Transport Links

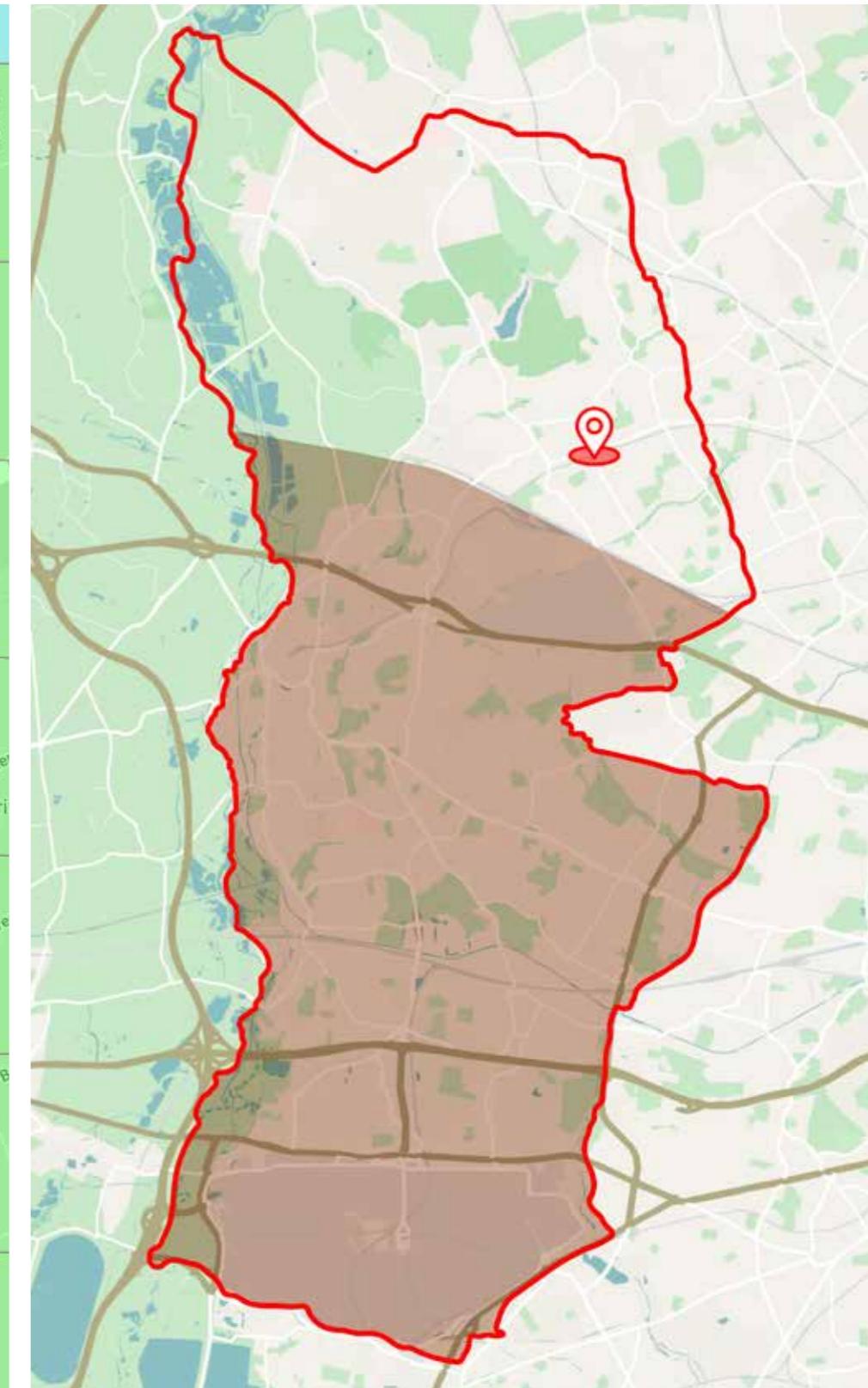
PTAL Level: 3

The site is immediately adjacent to a PTAL Level 4 designated area, with the site having good connections to public transport services; with short walks to the train station and bus stops.

PTAL Level: 2

PTAL Level: 3

PTAL Level: 4

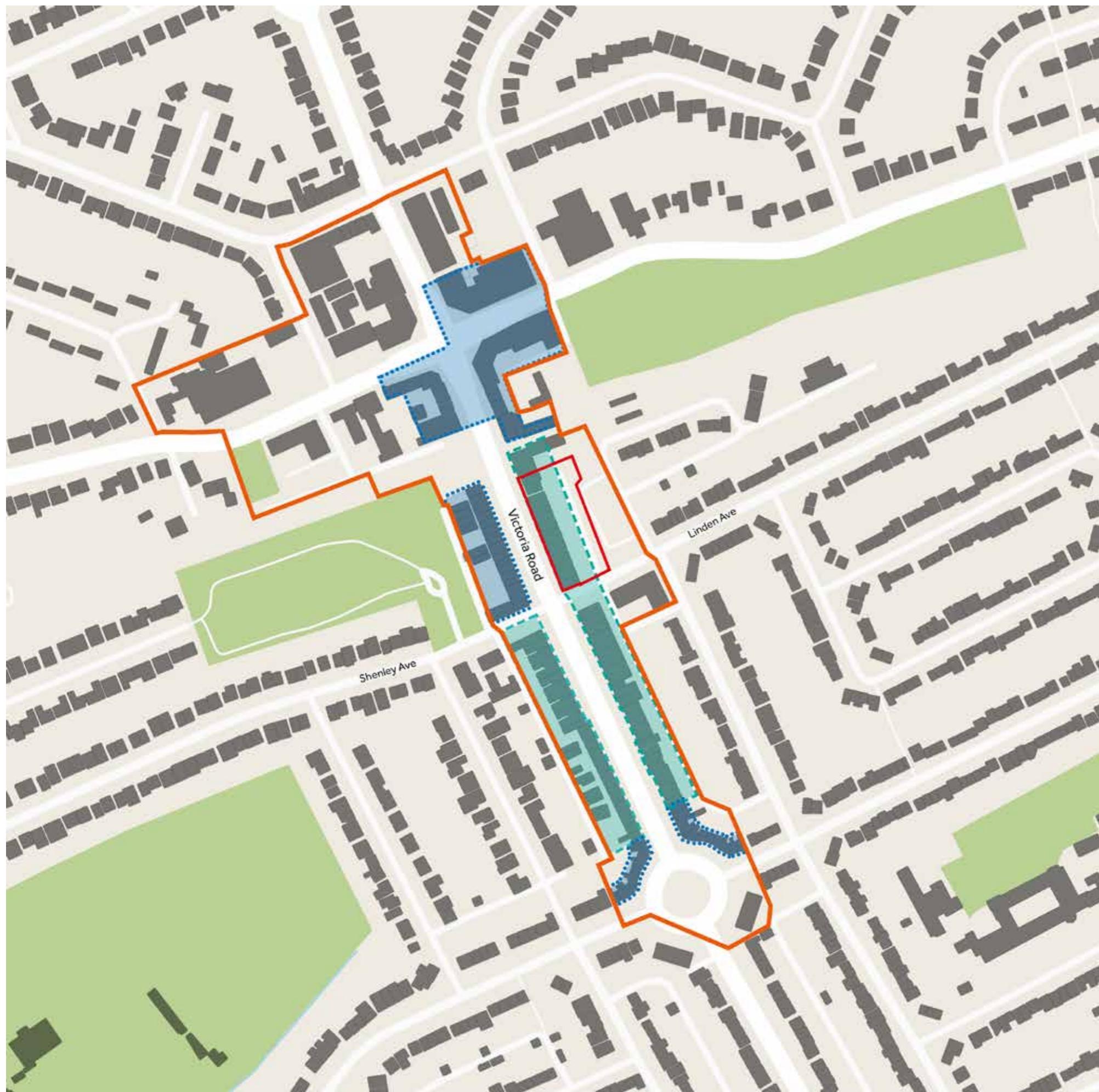


Air Quality Management

The London Borough of Hillingdon Air Quality Management Area Order 2003 denotes the area (shown above) as the Air Quality management area.

The site sits comfortably outside of the identified area.

## 2.5 Contextual Analysis | Policy and Background



### Development Management Policy - Minor Town Centre

- Site Boundary
- Town Centre Boundary
- Primary Shopping Area
- Secondary Shopping Area

### Residential Density Matrix - Policy 5.67 ( London Borough of Hillingdon - Local Plan Part 2 )

Location	Other Town Centres				
	PTAL	2-3			
Setting	Suburban				
Dwelling Type					
Detached and linked houses	Ave. 3.5 hr/unit	140-200 hr/ha	35-65 u/ha		
Terraced houses and flats	Ave. 3.1 hr/unit	155-248 hr/ha	50-80 u/ha		
Mostly flats	Ave. 3 hr/unit	200-250 hr/ha	80-100 u/ha		

## 3.0 Site Analysis

- 3.1 Site Photos
- 3.2 Site Photos
- 3.3 Refuse collection
- 3.4 Refuse collection
- 3.5 Site Assessment
- 3.6 Surrounding Building Heights



### 3.1 Site Analysis | Site Photos



Fig. 1 | Site Entrance. A clear boundary defined by the level change between the adjacent car park and the site.

Fig. 2 | Access to maisonette. Individual sets of stairs lead to a small private patio and front door, a recurring arrangement.

Fig. 3 | Views across the site from the private patio of a maisonette.

Fig. 4 | View across raised patios, towards Ruislip Manor Train Station.

Fig. 5 | The site boundary expanding towards the north.

Fig. 6 | View towards Linden Close.



Fig. 1



Fig. 3



Fig. 5



Fig. 2

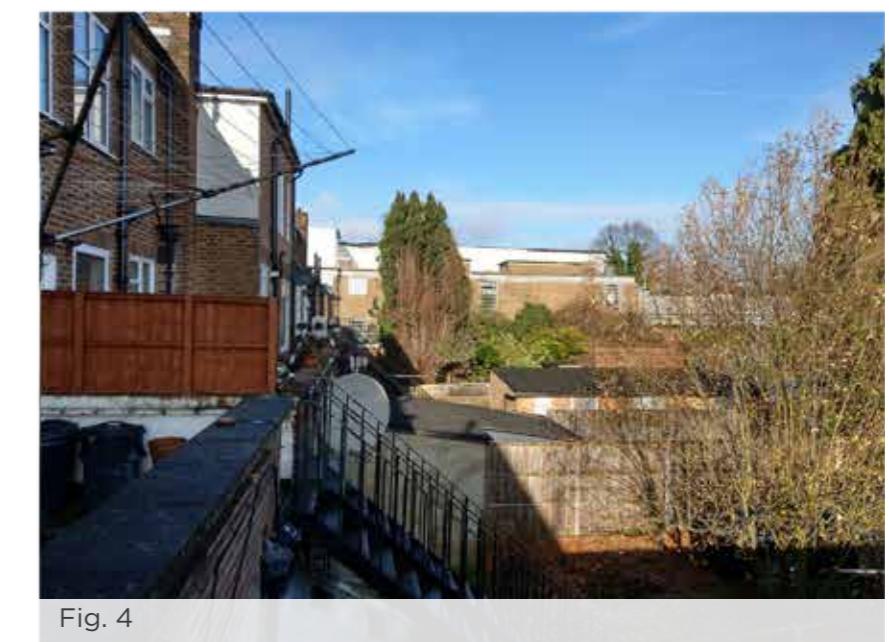


Fig. 4



Fig. 6

## 3.2 Site Analysis | Site Photos



Fig. 1 | Site threshold, back of house and uninviting.

Fig. 2 | Shared site access for cars & vehicles. Poor quality surfacing, and unenforced parking strategy.

Fig. 3 | Access via perimeter of private amenity, resulting in discomfort to inhabit.

Fig. 4 | Existing residential terraces, lack of usability due to depth.

Fig. 5 | Access route to residences, indiscernible pride of place

Fig. 6 | Established trees and outbuildings.



Fig. 1

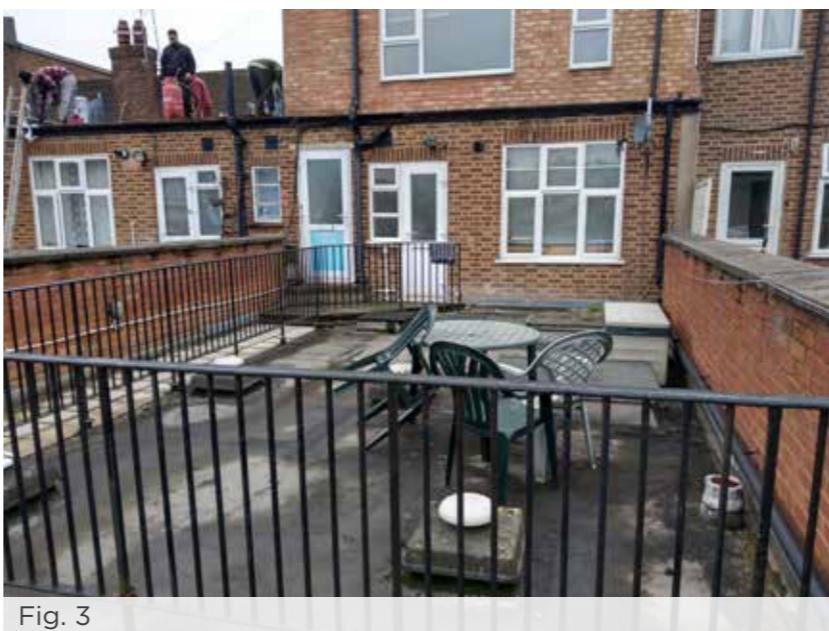


Fig. 3



Fig. 5



Fig. 2

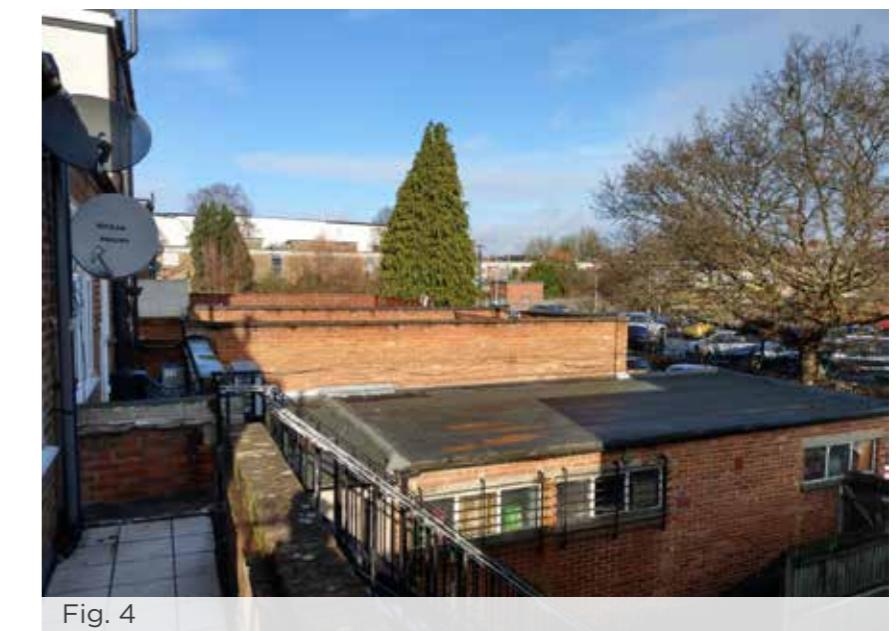


Fig. 4

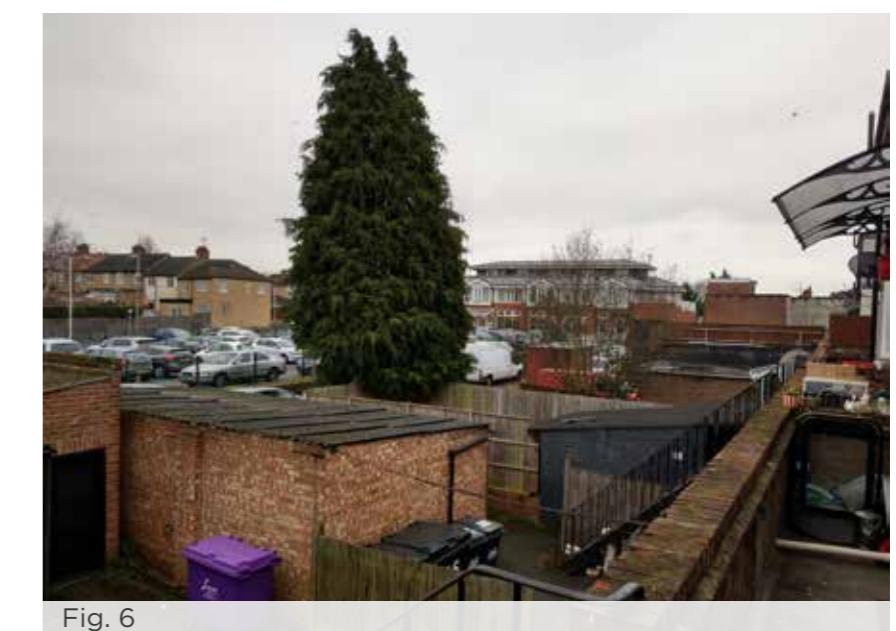
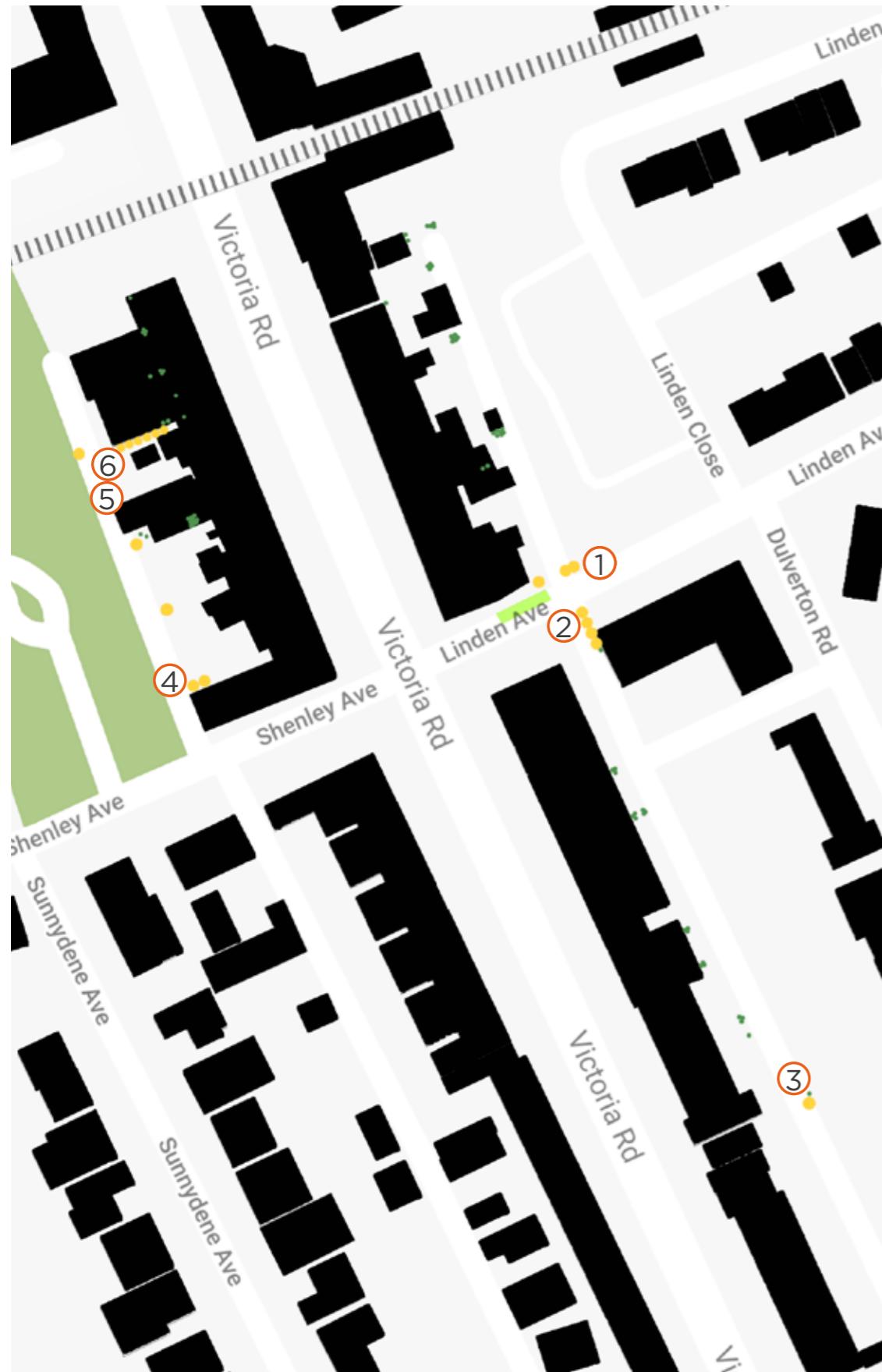


Fig. 6

### 3.3 Site Analysis | Refuse Collection



Refuse Collection Point

Commercial

Residential



Fig. 1 | Site threshold, eurobins moved to boundary for collection



Fig. 3 | Refuse collections located along service road for Victoria Road no.s 51+



Fig. 5 | Towards Northern end of access road off Shenley avenue, approx 65m from kerbside



Fig. 2 | Eurobins moves to edge of service road for Victoria Road no.s 51+



Fig. 4 | Refuse collections located along service road for Victoria Road, accessed via Shenley Avenue



Fig. 6 | Futher refuse points, adjacent to figure 5 location

### 3.4 Site Analysis | Refuse Collection



Refuse Collection Point

Commercial

Residential



Fig. 7 | Loose refuse left for collection, blocking retail access from service road



Fig. 9 | Residential refuse bins kept on flat roof of single storey retail extension, access via metal staircase



Fig. 11 | Refuse along edge of shared first floor walkway serving residential units to rear of Victoria road, no.s 36 B / D



Fig. 8 | Loose refuse left for collection by residents at edge of service road



Fig. 10 | Refuse left both at ground level and on elevated access balcony, no.s 59a and 61a

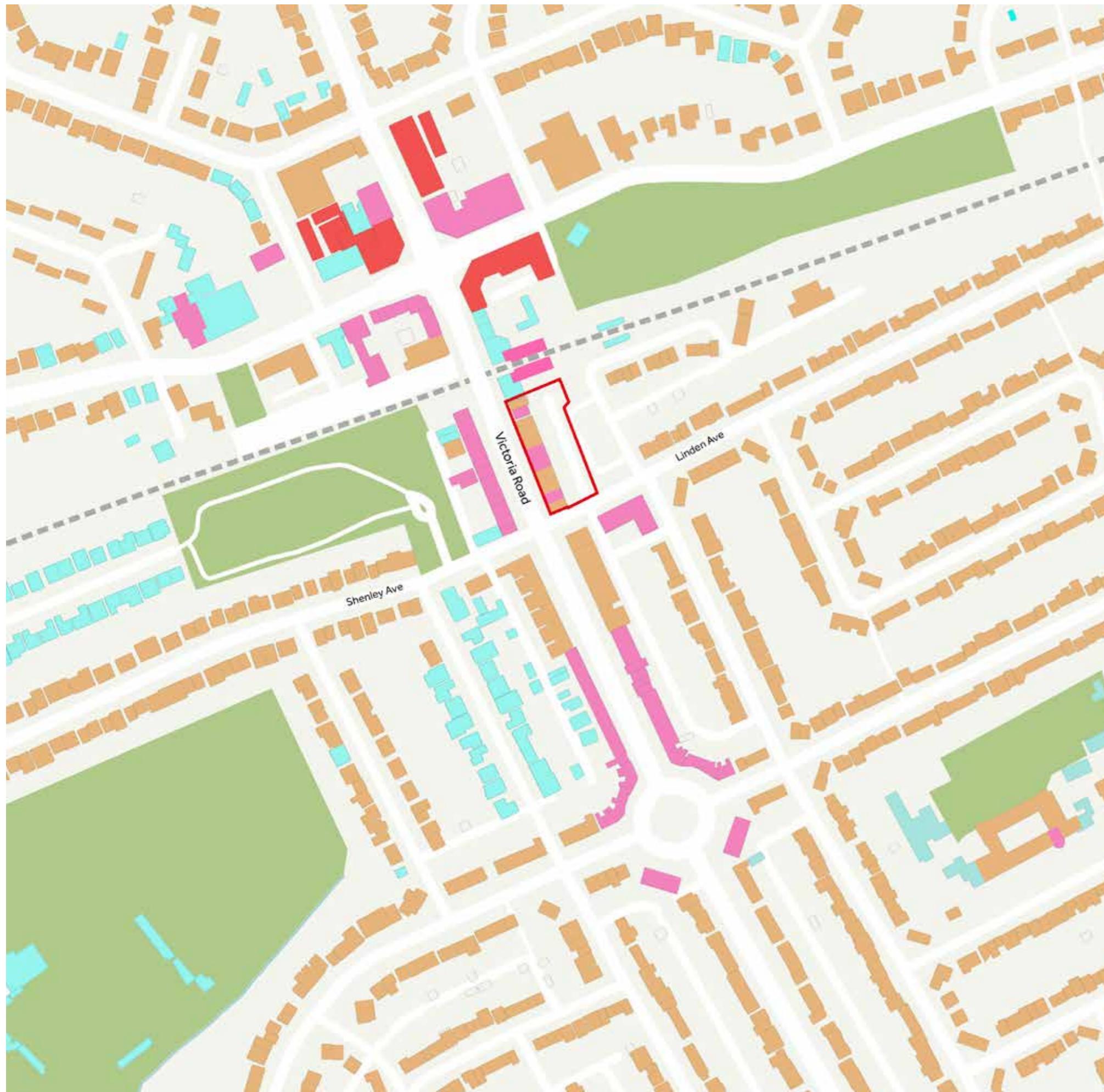


Fig. 12 | Refuse along edge of shared first floor walkway serving residential units to rear of Victoria road, no.s 24-28a

## 3.5 Site Analysis | Site Assessment



### 3.6 Site Analysis | Surrounding Building Heights



#### Key

- One Storey
- Two Storey
- Three Storey
- Four Storey
- Railway Line
- Site Boundary

3.6.1 The adjacent drawing demonstrates the distinct effect of town centre proximity on property massing. Within the town centre predominantly three to four storey buildings are present, with retail and services premises at ground floor, and residential uses at upper levels.

3.6.2 Moving away from Victoria road areas are largely residential, and of a two storey massing. It is notable however that a large number of dormer, side, and rear extensions have been incorporated, lending additional massing to these areas.

3.6.3 Additionally, where within the town centre location, and within proximity to the local transport hubs four storey developments have been permitted and / or built. This reflects the housing needs and intensification of town centre density. Ruislip Manor Town centre is now 'characterised by modest-scale, three and four storey buildings' <sup>11</sup>.

Ref 1: North Planning Committee (2007). 'Item 3.37: Building design and impacts on the streetscene'. *Minutes of North Planning Committee 20th February 2007*, Committee Room 5, Civic Centre High Street.

## 4.0 Planning History

- 4.1 Nearby Applications
- 4.2 Windmill P.h
- 4.3 69-83 Park Way & 1-11 Victoria Road
- 4.4 Rear of 26-28 Victoria Road
- 4.5 Rear of 34-36 Victoria Road
- 4.6 Summary & Conclusion



## 4.1 Planning History | Nearby Applications

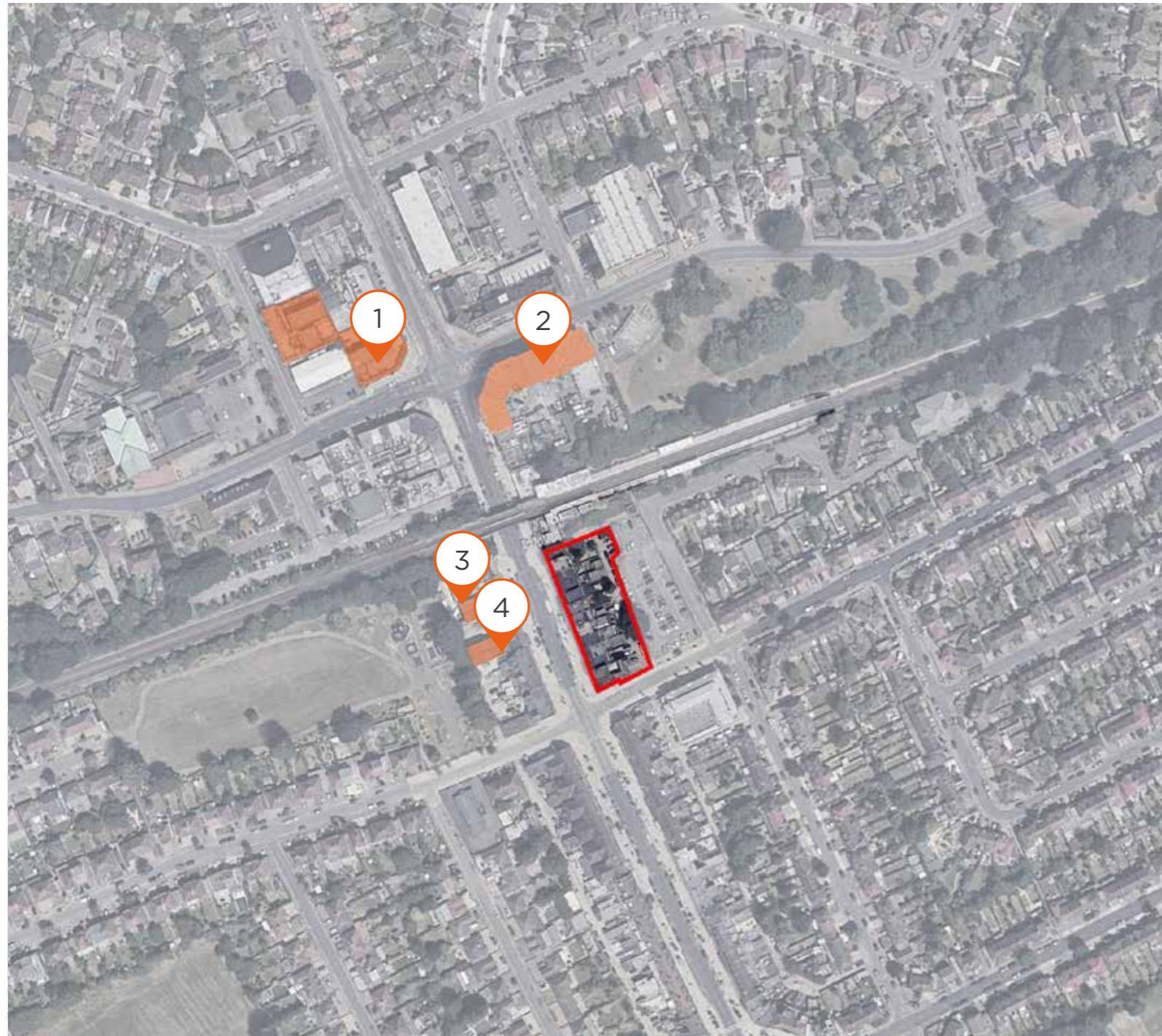


Fig. 1. Windmill P.H



Fig. 2. 69-83 Park Way & 1-11 Victoria Road



Fig. 3. 26-28 Victoria Road



Fig. 4. 34-36 Victoria Road

**Fig.1.** Ref no. | 11924/APP/2006/2632  
Location: Windmill P.H, Windmill Hill, Ruislip  
Status: Approved 20th July 2007

Description - Redevelopment of the site with a mixed use comprising thirty nine residential units in two blocks with ground and lower ground parking, amenity space and access from west way and retail unit on ground floor facing Pembroke road/windmill hill (involving demolition of public house).

The development of 39 flats comprising 22 1B|2P and 17 2B|4P flats, result in a density of 201 u/ha with an average of 490 hr/ ha.

**Fig.2.** Ref no. | 72040/APP/2016/2531  
Location: 69-83 Park Way & 1-11 Victoria Road, Ruislip  
Status: Approved 5th May 2017

Description - Enlargement of the roof, including raising of the ridge and eaves height to provide new recessed third floor, erection of a third floor extension to the rear of Nos. 69-81 Park Way, including the installation of rooflights, rebuilding of chimneys and new emergency external stairs to the rear of No.9 Victoria Road, to allow for the provision of 9 new flats.

The development creates a 4 storey mixed use Commercial/ Residential Property. The approved scheme results in a density of 147 u/ha 295.6 hr/ha.

**Fig.3.** Ref no. | 71956/APP/2016/2130  
Location: Rear of 26-28 Victoria Road, Ruislip  
Status: Approved 4th August 2016

Description - Raising of roof to create a first floor to form 2 x 1-bed flats.

**Fig.3.1** Ref no. | 71956/APP/2018/4058  
Location: Rear of 26-28 Victoria Road, Ruislip  
Status: Approved 18th January 2019

Description - Raising of roofspace to create habitable roofspace for use as 2x studio flats.

**Fig.4.** Ref no. | 20094/APP/2018/794  
Location: 34-36 Victoria Road, Ruislip  
Status: Approved 2nd May 2018

Second floor extension to create 1 x 3-bed self-contained flat

## 4.2 Planning History | Windmill P.H

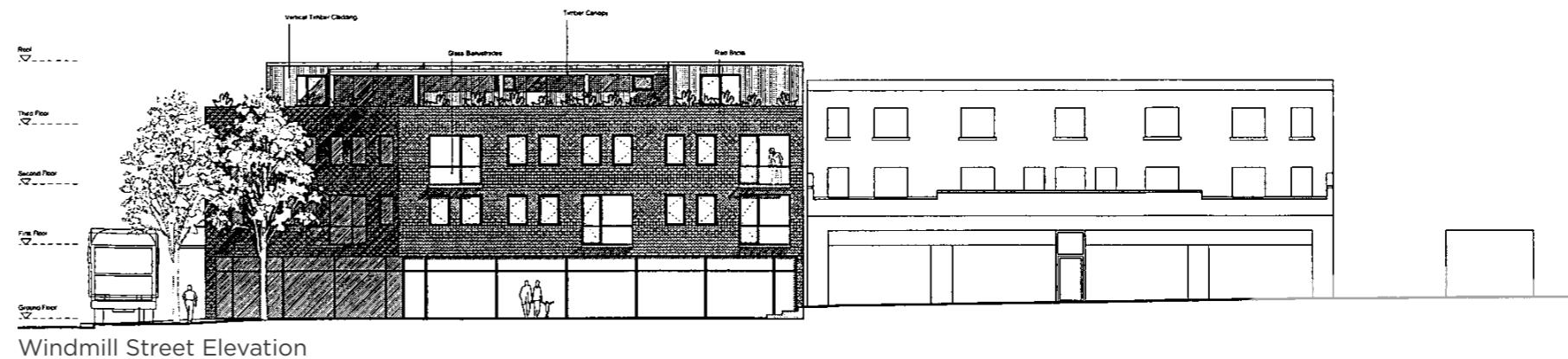
Ref no. | 11924/APP/2006/2632

Location: Windmill P.H, Windmill Hill, Ruislip

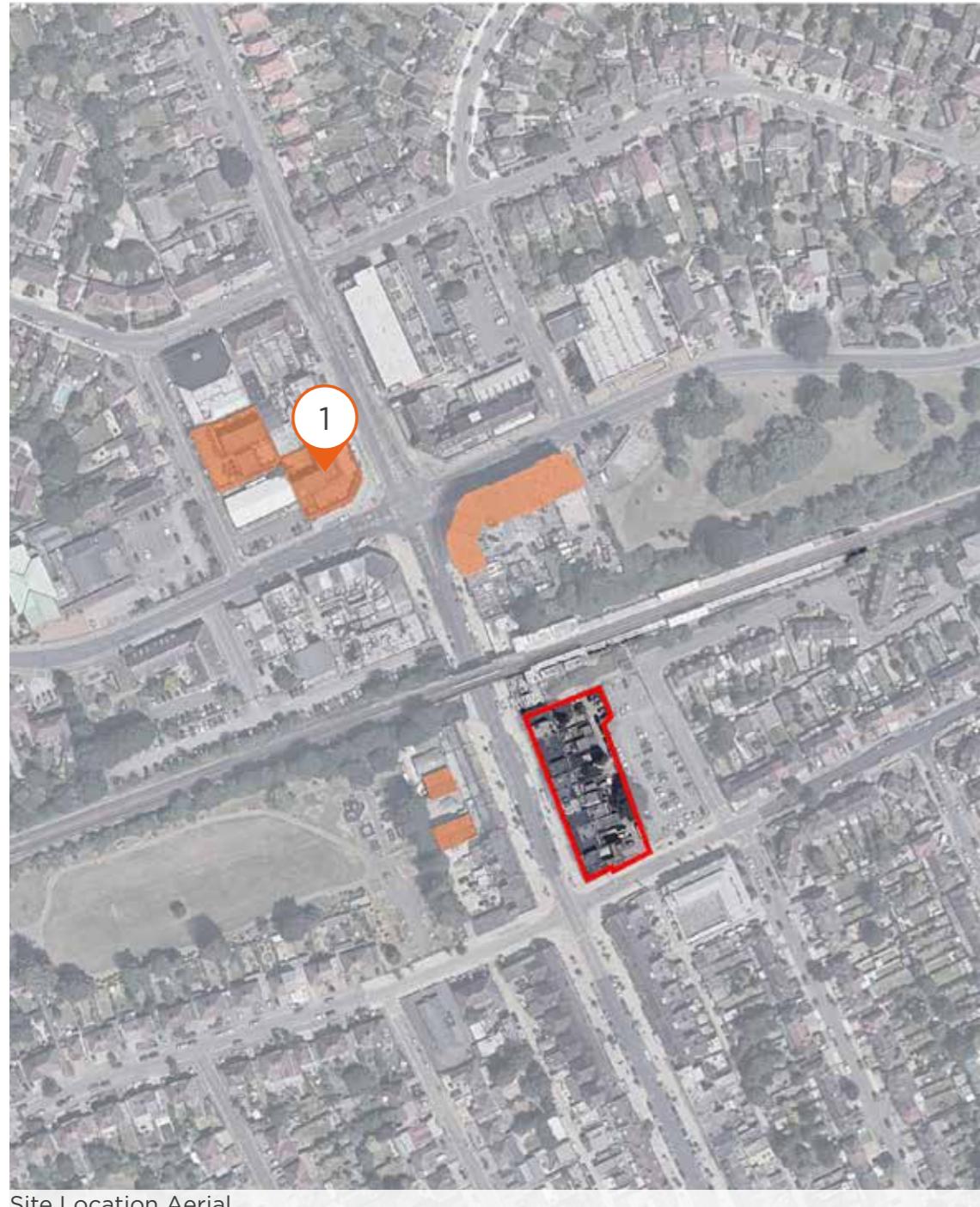
Status: Approved 20th July 2007

Description - Redevelopment of the site with a mixed use comprising thirty nine residential units in two blocks with ground and lower ground parking, amenity space and access from west way and retail unit on ground floor facing Pembroke road/windmill hill (involving demolition of public house).

The development of 39 flats comprising 22 1B|2P and 17 2B|4P flats, result in a density of 201 u/ha with an average of 490 hr/ ha.



Windmill Street Elevation



Site Location Aerial



View from crossroads



View from Pembroke Road (View east)

Situated approximately 150m North East of the proposed site, and bounded by Pembroke Road (South) and Windmill Hill (East) the largely residential mixed use development is a four storey development, providing 39 flats, with an average density of 201 u/ha.

Key comments from officers report:

'Whilst the residential density proposed is high, it is considered to be acceptable having regard to the urban context of the town centre, the proximity of the site to local services and public transport, and the high quality of the proposed building.'

'8.4 (i) of the UDP states that housing is an acceptable use within town centres. Indeed the development would allow for the more efficient use of existing infrastructure and services, adding to the vitality of the town centre.'

'The site is located within the Ruislip Manor Town Centre which is characterised by modest-scale, three and four storey buildings.'

'The retail unit would result in 3-4 deliveries per day. The assessment considers that this would not significantly impact on local road conditions, subject to a servicing management plan.'

## 4.3 Planning History | 69-83 Park Way & 1-11 Victoria Road

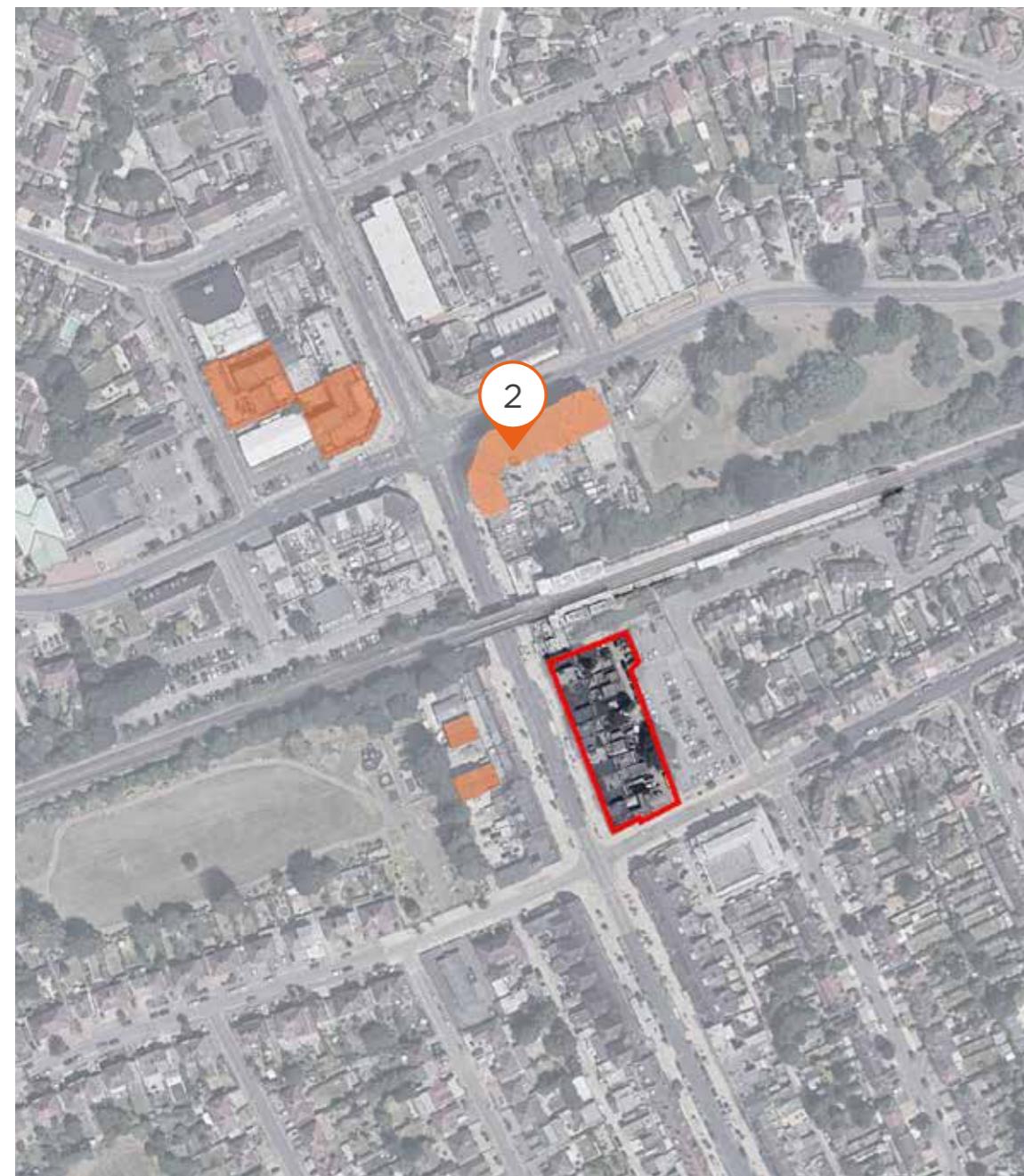
Ref no. | 72040/APP/2016/2531

Location: 69-83 Park Way & 1-11 Victoria Road, Ruislip

Status: Approved 5th May 2017

Description - Enlargement of the roof, including raising of the ridge and eaves height to provide new recessed third floor, erection of a third floor extension to the rear of Nos. 69-81 Park Way, including the installation of rooflights, rebuilding of chimneys and new emergency external stairs to the rear of No.9 Victoria Road, to allow for the provision of 9 new flats.

The development creates a 4 storey mixed use Commercial/ Residential Property. The approved scheme results in a density of 147 u/ha 295.6 hr/ha.



Situated approximately 75m North East of the proposed site, and bounded by Victoria Road (West) and Park Way (North) the approved application sought the raising of the existing roof to adapt a three storey property into a four storey premises. The site addresses Victoria Road, and at the southern most point of the site creates a three storey disparity between the proposal and its immediate neighbour (see fig 'Proposed Elevation - Victoria Road' above.)

Key comments from officers report:

The new roof maintains similar proportions, roof slope and detail [...]. The height of the building would increase by less than 2m, with the new main eaves height (as measured from the middle of the Park Way frontage) would increase from 9.1m to 10.9m and the ridge height from 12.1 to 13.9m.

Density is not considered particularly useful for small scale schemes, such as the proposal where the standard of residential accommodation provided and the impact of the extension on the existing building and surrounding area are more important.

## 4.4 Planning History | Rear of 26-28 Victoria Road

Ref no. | 71956/APP/2016/2130

Location: Rear of 26-28 Victoria Road, Ruislip

Status: Approved 4th August 2016

Description - Raising of roof to create a first floor to form 2 x 1-bed flats.

Situated approximately 55m West of the proposed site the approved, and subsequently built, application to raise the roof and create 2 x 1 bed flats is sited along a servicing road to the rear of 26-28 Victoria Terrace. Access to residences of 26-28 Victoria Road and the approved application are via a shared external staircase and elevated walkway (see figures 3 & 4).

Subsequent approval was granted (Ref no. | 71956/APP/2018/2376) for the addition of a third storey, creating two studio flats at the third floor (see dotted orange outlines to figures 1, 5 & 6).

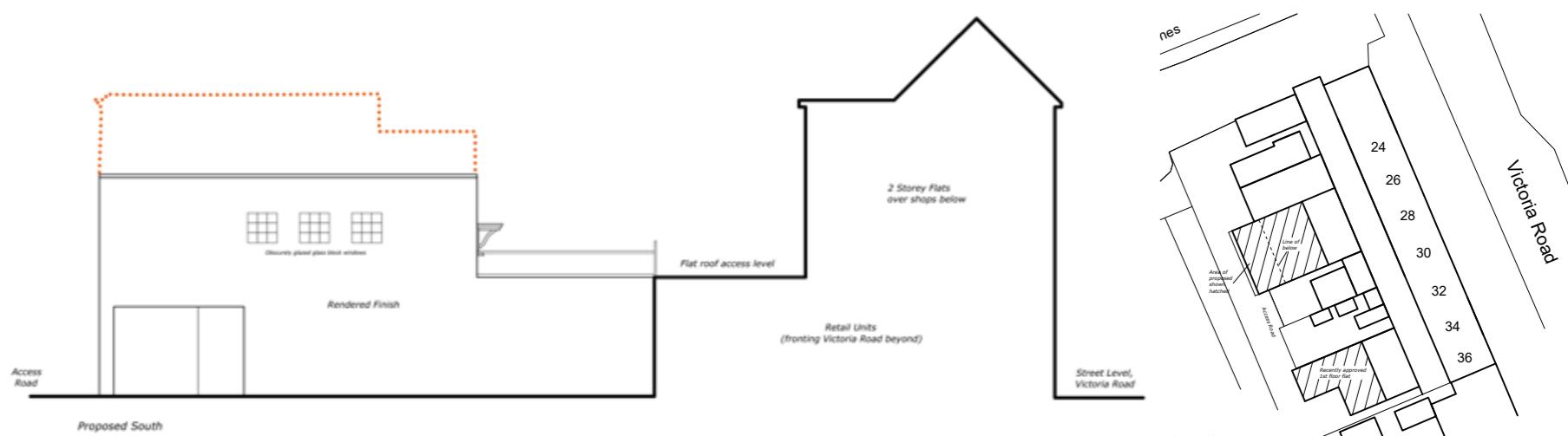
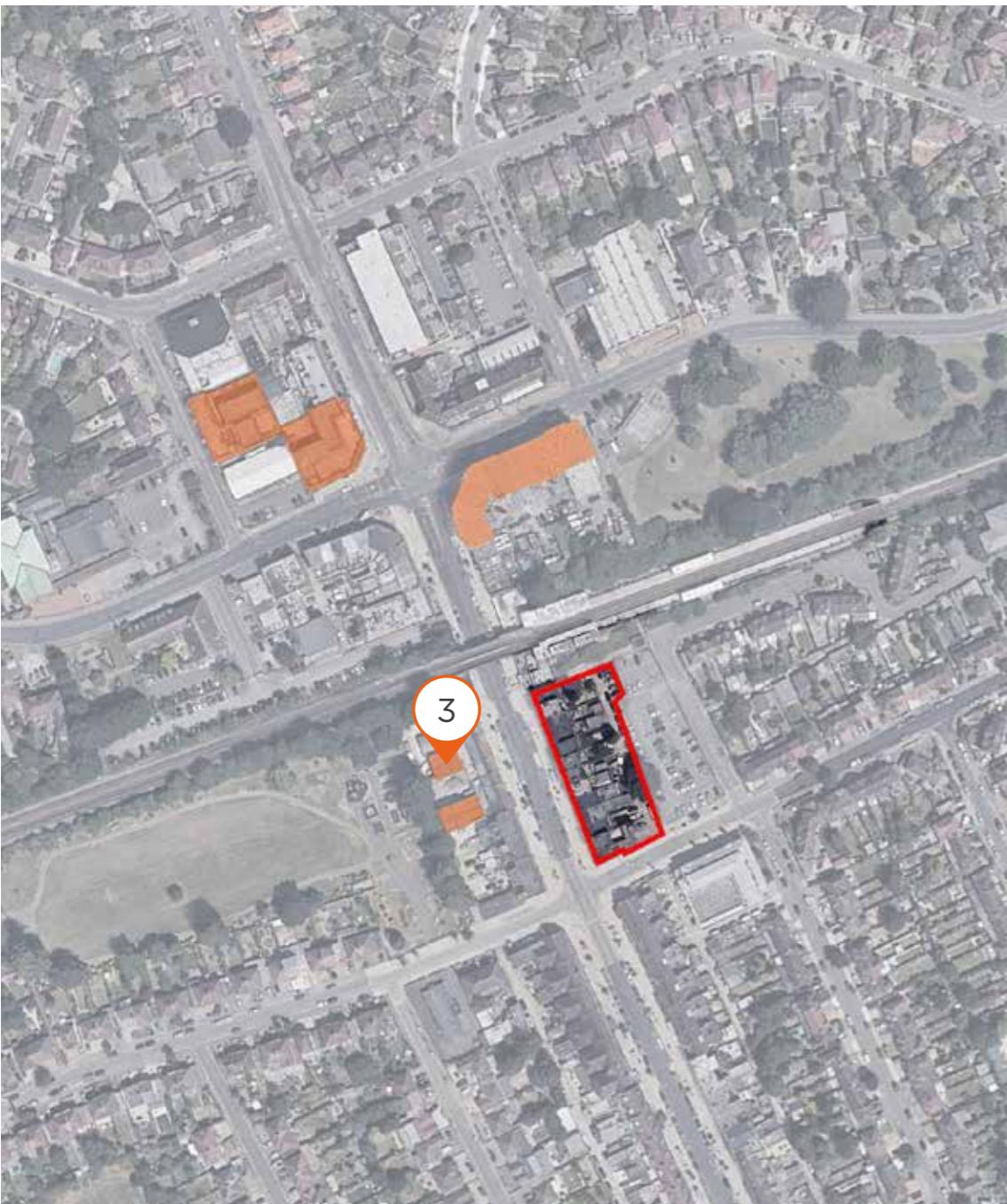


Figure 1 | Approved south elevation



Figure 3 | As built, shared access to flats along service road

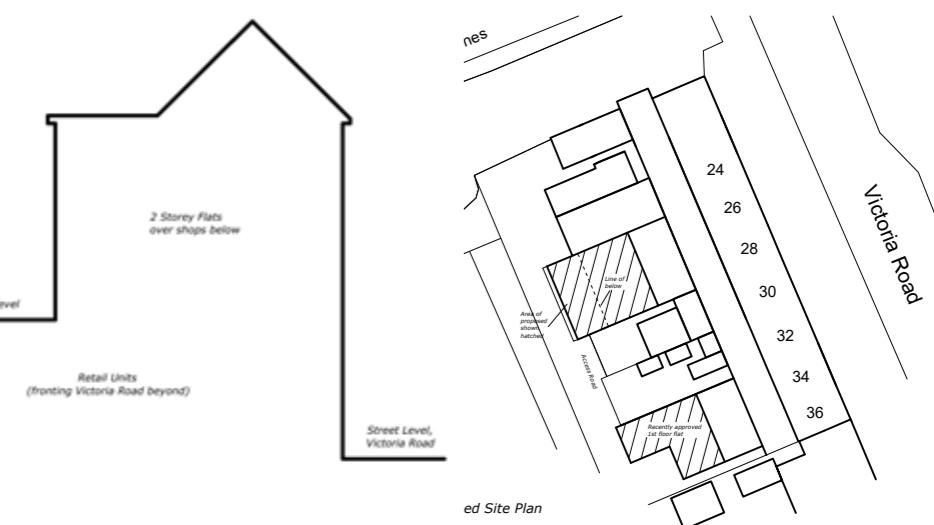


Figure 2 | Approved site plan



Figure 4 | As built approach towards approved flats past amenity

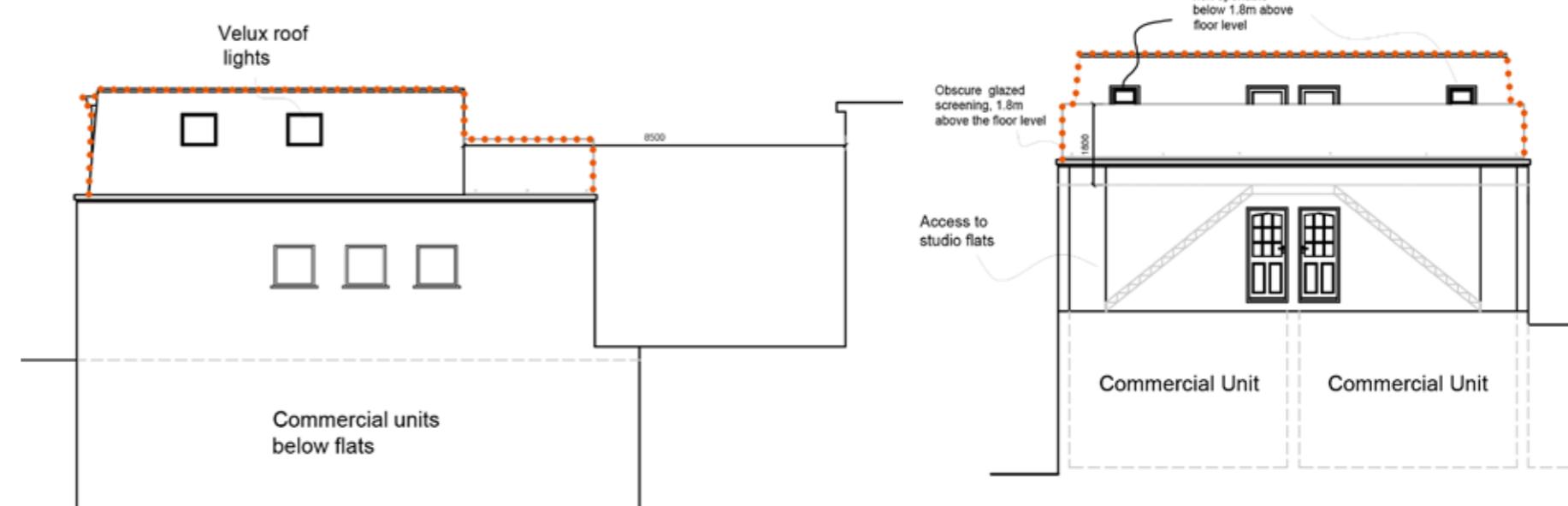


Figure 5 | APP/2018/2376, approved additional storey, creating 2 studio flats. Figure 6 | Approved elevation, facing onto Victoria Road.

## 4.5 Planning History | Rear of 34-36 Victoria Road

Ref no. | 20094/APP/2018/794

Location: 34-36 Victoria Road, Ruislip

Status: Approved 2nd May 2018

Second floor extension to create 1 x 3-bed self-contained flat

Situated approximately 55m West of the proposed site the approved, and subsequently built, the application sought to raise the roof and create 1 x 3 bedroom flat. The approved design creates a three storey series of dwelling houses along the servicing road to the rear of 34-36 Victoria Road, accessed via a shared external staircase and elevated walkway.

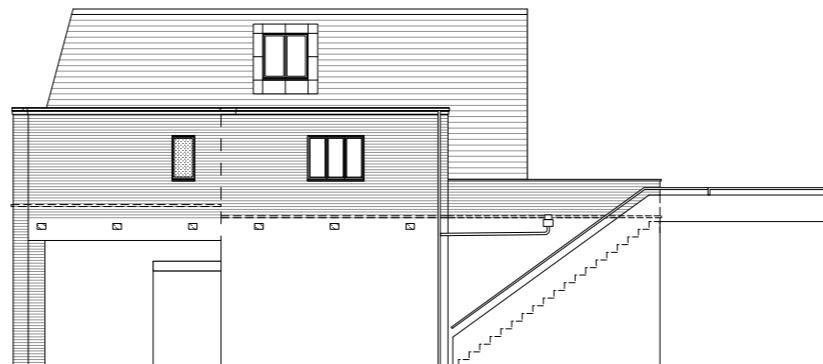
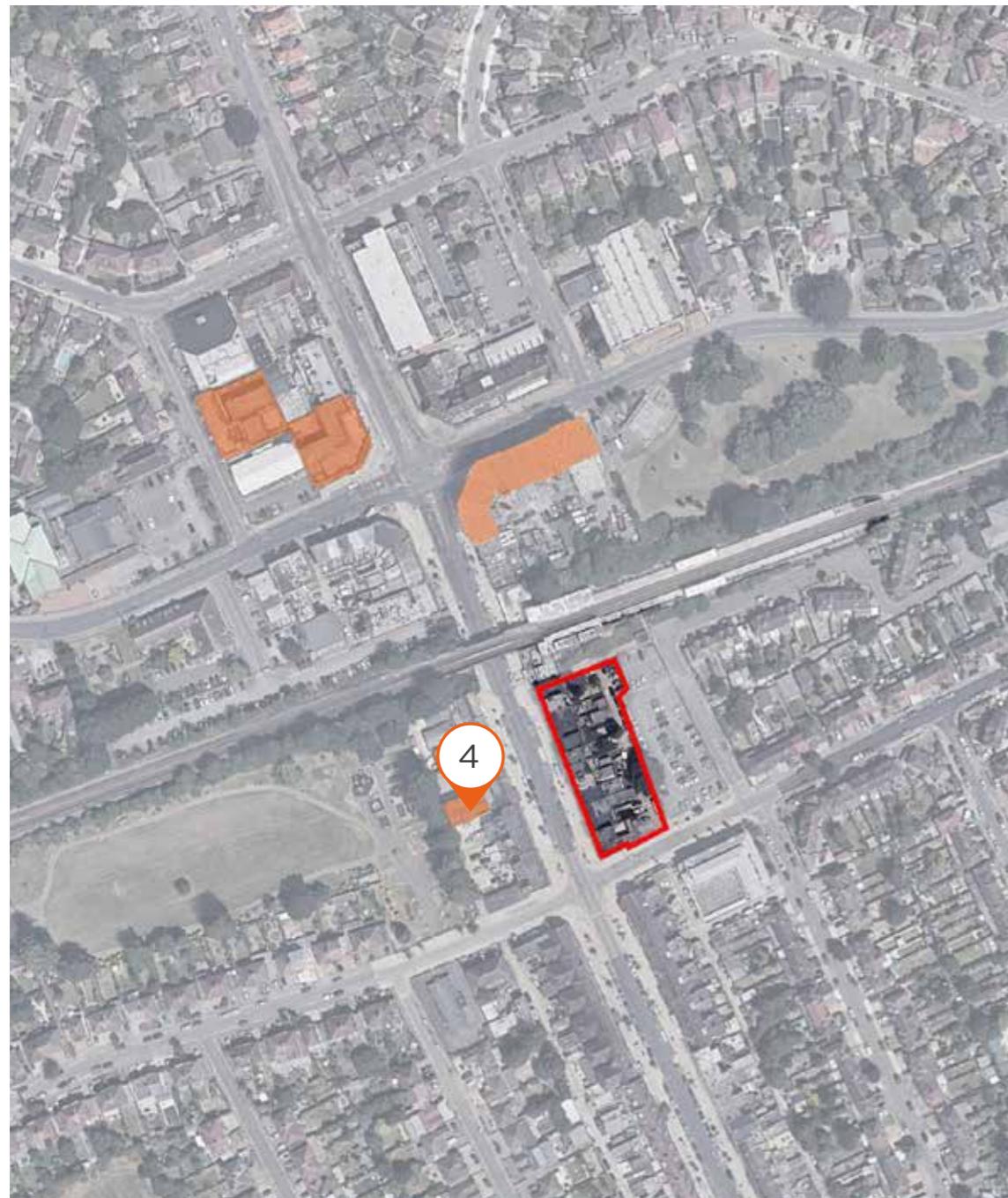


Figure 1 | Approved additional third storey (South Elevation)

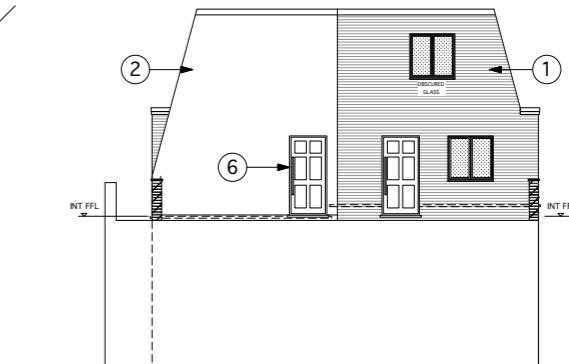


Figure 2 | Approved third storey (East elevation)

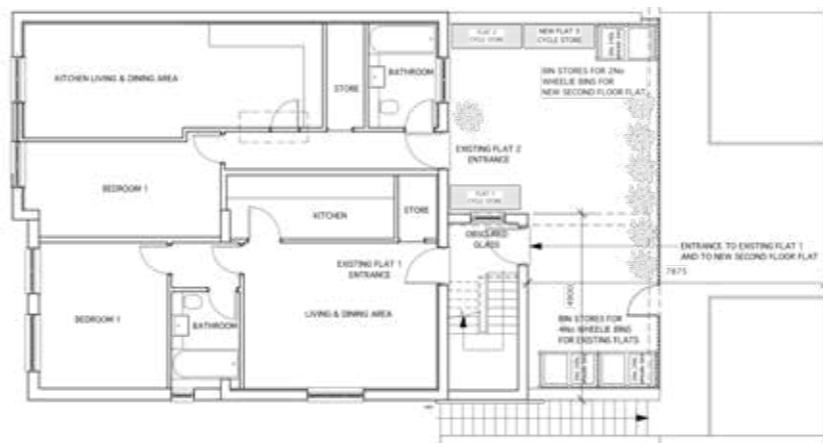


Figure 3 | Approved plan - Access and amenity via shared walkway



Figure 5 | As built, south elevation.



Figure 4 | Opposite, resident entrance and amenity



Figure 6 | As built, north elevation.

Key comments from officers report:

'Consideration of the proposal at appeal the inspector advised that the arrangement of the mansard roof would result in the appearance of a parapet roof above the existing two storey projection' and that 'the proposed extension would provide added visual interest to the building'

'The extension would be constructed 7.87m from the rear of the original three storey property and would raise the roof of the existing extension [...] bringing it to a total height of 9.47m and 5.5m above the terrace. Whilst occupiers of the flats within the upper floors of the three storey terrace would be able to see the raised section of the extension, the impact of the proposal on these residential properties is not considered to be of such a degree as to warrant a recommendation for refusal of the application.'

'An additional window is proposed in the side elevation facing towards the extension at the rear of 26-28. This would look over the roof of the first floor flat which has 3 side windows facing windows approximately 11m away, however these windows are obscure glazed and not opening.'

'in view of the good connectivity and access to services the future occupiers would not necessarily be reliant on the private car and the lack of parking would be consistent with the objectives of the NPPF [...] It would therefore be unreasonable to refuse this scheme on the basis of the lack of car parking provision.'

## 4.6 Planning History | Summary & Conclusion

### Item 1 | PTAL & Car Free Development



*'In view of the good connectivity and access to services the future occupiers would not necessarily be reliant on the private car and the lack of parking would be consistent with the objectives of the NPPF'*

Ref no. | 20094 /APP/2018/794



The proposal benefits from similar siting to the approved scheme. Existing site access is in effect mirrored along Victoria road, benefitting from the same connections and services access.

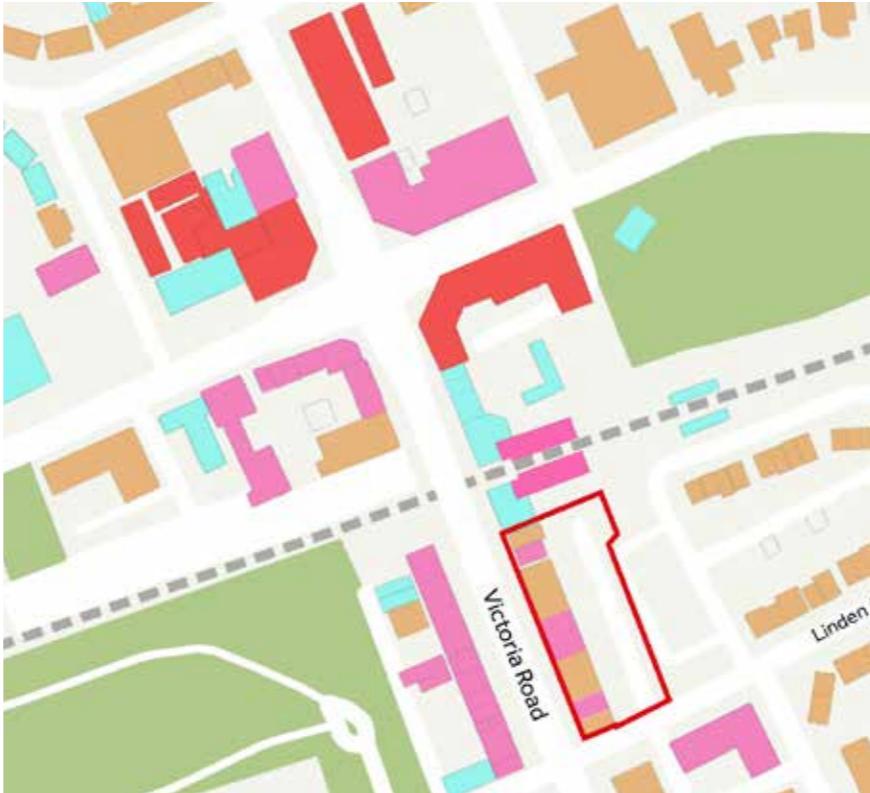
— Rail connection from 34-36 Victoria Road (Approx 210m)

— Rail connection from proposal site (Approx 160m)

### Item 2 | Massing and Scale

*'The site is located within the Ruislip Manor Town Centre which is characterised by modest-scale, three and four storey buildings'.*

Ref no. | 11924/APP/2006/2632



Located within the Town Centre the proposal will respond to massing characteristics of its surroundings which are primarily three storey buildings, elevating to four storeys at Victoria Road Junction to the North.

### Item 3 | Density

*'Density is not considered particularly useful for small scale schemes, such as the proposal where the standard of residential accommodation provided and the impact of the extension on the existing building and surrounding area are more important.'*

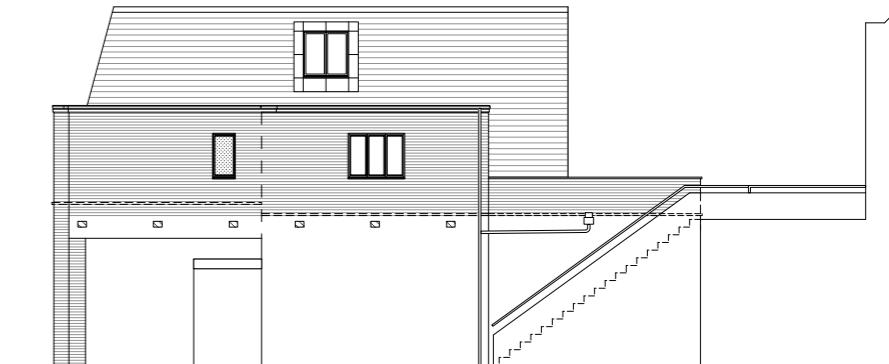
Ref no. | 720404/APP/2016/2531

The design principles of the proposal adopt an approach focused on contribution to improvement of site context, actively participating in the enhancement of quality of life for existing users.

Application no. 20094/APP/2018/794 (34-36 Victoria Road), has been allowed in light of its close proximity of 7.87m from existing residences on the basis that *'the impact of the proposal on these residential properties is not considered to be of such a degree*

*as to warrant a recommendation for refusal of the application'.*

Below: Approved side elevation, dwg A-401 RevA.



Beyond the approved application, the proposal will look to enhance the surrounding area, rather than merely abutting the existing arrangement, providing greater provision of public amenity between the existing fabric and the proposal.

### Item 4 | Prejudice of future development

Application no. 20094/APP/2018/794 (34-36 Victoria Road) proposes an additional window 'in the side elevation facing towards the extension at the rear of 26-28. This would look over the roof of the first floor flat which has 3 side windows facing windows approximately 11m away, however these windows are obscure glazed and not opening'.



The approved proposal prejudices infill developments of a similar nature. The proposal considers the entirety of the rear of the site, ensuring cohesive design that doesn't prejudice future developments.

## 5.0 Pre-Application Advice | Feedback & Response

### 5.1 Feedback & Response



- 5.1.1 Pre-app is emphasised as an important means to securing high quality development, as stated within the NPPF.
- 5.1.2 In accordance with best practice, a collaborative approach which incorporates the LPA to ensure a high quality outcome aligned with the development policy of Hillingdon, two pre-application consultations were undertaken,
- 5.1.3 The prior feedback was given on the basis of a different development proposal, nevertheless key principles of relevance were discussed with officers, these are outlined below:
- 5.1.4 Pre-Application feedback (Ref:75382/PRC/2022/63) was provided on the basis of the proposed development of 13no. residential properties along the eastern edge of the site, incorporating a raised podium level for pedestrian access and amenity to proposed and existing residents alike.
- 5.1.5 It is not necessary to repeat extensively the comments provided, instead the key principles given are outlined below:

### 5.1.6 Principle of development

The proposed development should not prejudice the potential of development and Linden Avenue Public Car Park. Forthcoming proposals should demonstrate that appropriate servicing arrangements can be maintained to the town centre uses, notably the shops at ground floor level

### 5.1.7 Design

The pre-application scheme was considered incongruous, and proposed development of an unconventional development site. In accordance with DMHB 11, development including extensions, alterations, and new buildings should incorporate principles of good design. This includes harmonising with the local context's scale, plot sizes and architectural composition. The site is identified as being two / three-storeys in height and of a symmetrical composition constructed from traditional materials.

### 5.1.8 Amenity

Salient feedback that has bearing on the proposed application include the given internal space standards and external amenity space standards, both of which are to comply with the Local Plan - Part Two. Feedback acknowledged that whilst there is an expectation that private and communal outdoor amenity space are provided on site where a shortfall of provision is present a contribution towards upgrading local public space may be required.

Future applications will require a noise assessment for approval of external noise levels including reflected and radiated noise and details of the sound insulation of the building envelope, orientation of habitable rooms away from major noise sources and of acoustically attenuated mechanical ventilation as necessary to achieve internal room and external amenity noise standards.

### 5.1.9 Highways

The development proposals need to comply with the parking standards outlined in appendix C of the Local Plan, though these requirements may vary where the variance would not negatively effect on street parking provision or congestion in the locality. It is recognised that in practice the site also have greater connectivity than suggested by the PTAL level of 3, owing to rail and bus connections.

A car free development may also be further considered subject to the findings of an on-street parking stress survey, undertaken in line with the Lambeth Council Parking Survey methodology.

## 6.0 Design Proposal

- 6.1 Design Brief
- 6.2 Site Development Strategy
- 6.3 Use / Schedule of Accommodation
- 6.4 Layout
- 6.5 Scale
- 6.6 Appearance
- 6.7 Access
- 6.8 Landscaping



## 6.1 Design Proposal | Design Brief

### Design Brief

6.1.1 The proposed development seeks to achieve the criteria identified within the following design brief:

- A To design high quality, purpose-built, residential homes. To comprise of 9no. additional single storey and duplex flats (use class C3).
- B Each home is to meet the London Housing Design Guide standards, and provide quality private amenity space associated with each new home.
- C Ensure that the proposal does not prejudice further development of Linden Avenue Public Car Park, to the west.
- D Develop a massing appropriate to the context, reducing, in keeping with the modest 3-4 storey context of the town centre location.
- E Ensure opportunities for natural daylighting and cross ventilation is, as far as possible, maximised.
- F The proposals are to satisfy the requirements of M4(1).

### Site Development Strategy

6.2.1 In response to the brief, and pre-application feedback received, the following strategy was considered to address the proposal targets:

- 1 To relocate the proposed dwellings, retaining the existing service road access, and a largely similar configuration to retain the commercial vitality of the site.
- 2 To extend the existing properties of 25-49 Victoria Road upwards, with a set back that provides both amenity opportunity and visual subservience.
- 3 To create dual aspect layouts, that respect the existing fenestration, maintaining a cohesive visual language.
- 4 Utilisation of materials such as brick, and stone window surrounds to create a cohesive street appearance along Victoria Road.
- 5 Re-configured ground floor access, enabling improved pedestrian access to both existing and proposed premises off Victoria Road, rather than the existing access via the service road.
- 6 Provision of dedicated cycling storage to encourage uptake of sustainable transport methods.

### Use / Schedule of Accommodation

6.3.1 The existing configuration of 25-49 Victoria Road comprises of 12no. retail units at ground floor, with 14no. residential units at first and second floor above.		
The proposal seeks the creation of a total of 9no. new units (use class C3) at second and third floor level. The proposed homes are evenly distributed above the existing terrace at 25-49 Victoria Road.		
6.3.3 The total schedule of accommodation comprises:		
Existing accommodation   14no. Dwellinghouses		
1no. 1b   8no. 2b   5no. 3b		
25A   2b3p	59 sq.m	
27A   2b4p	79.1 sq.m - Remodelled	
29A   2b3p	58 sq.m	
31A   2b3p	59sq.m	
33A   2b3p	55 sq.m - Remodelled	
35A   3b5p duplex	99 sq.m	
37A   3b5p duplex	99 sq.m	
39A   3b5p duplex	99 sq.m	
41A   2b3p	57 sq.m	
43A   2b3p	58 sq.m	
45A   1b1p	37 sq.m - Remodelled	
47A   3b5p duplex	99 sq.m	
33B   3b5p duplex	101.8 sq.m - Remodelled	
41B   2b3p	49 sq.m	
Proposed accommodation   9no. Dwellinghouses		
4no. 1b   1no. 2b   4no. 3b		
#1   1b2p	51.9 sq.m	
#2   3b5p duplex	101.4 sq.m	
#3   3b5p duplex	102.3 sq.m	
#4   3b5p duplex	94.8 sq.m	
#5   1b2p duplex	58 sq.m	
#6   3b6p duplex	111.7 sq.m	
#7   2b4p	71 sq.m	
#8   1b2p	55.5 sq.m	
#9   1b2p	55.1 sq.m	

## 6.4 Design Proposal | Access & Layout

### Access & Layout

6.4.1 The proposed layout encompasses a variety of considerations, ranging from the experience of the occupier, to the impact layout has on the character of the proposal as perceived by a passer-by at street level.

6.4.2 Key layout decisions and features are identified below.

1 The existing servicing access road has been retained to the rear 25-49 Victoria Road. This has been maintained to ensure the future viability of retail & commercial premises at ground floor level.

2 The retention of the servicing access road helps ensure the maintaining of a suitable separation distance between the sites residential premises and potential future developments located at Linden Avenue public car park.

3 Two new pedestrian access points have been created onto Victoria Road, improving pedestrian access to both the existing and proposed residences at upper floor levels. Benefits include:

- Greater passive surveillance of entrance points.
- Improved access conditions. Lighting, surfacing, and a secure access point all improve resident safety.
- Entrance locations are now in closer proximity to rail and bus connections, improving transport access.

4 The removal of existing access staircases to the rear of Victoria Road provides opportunity for ground floor extension of retail premises, yielding small GIA benefits for business.

5 Proposed residential properties are located solely above existing residences, ensuring compatibility of neighbouring uses.

6 Proposed one/two storey extensions retain similar proportions to the existing residential premises, enabling vertical stacking on to existing party walls, conferring benefits including consistency of fenestration and minimising flanking sound between units.

7 All proposed flats are dual aspect, maximising opportunities for natural daylighting and ventilation. Living, Dining, Kitchen spaces step directly onto private amenity spaces. each space conferring increased desirability and utility to the other.

8 Shared access walkways are overlooked by study spaces, providing passive surveillance whilst retaining the expected privacy of habitable spaces.

9 Existing bin storage arrangements are to be formalised, a new sheltered area to the eastern boundary of the site improves the bin storage capacity, providing 4,700L 100% provision requirements based on BS 5906:2005.

10 Vehicular access is as per existing arrangements. Primary vehicular access is via Linden Avenue, to the south.

11 A formalised pedestrian path is proposed along the eastern boundary of the service road to improve connections for existing residents not served by the proposed access off of Victoria Road.

## 6.4 Design Proposal | Layout

### Key:

Legend for the site plan:

- Site boundary: Red dashed line
- Demolished: Red dashed line
- Service Road boundary: Orange solid line
- Existing Building (retained): Grey square
- Soft landscaping (proposed): Green square
- Soft landscaping (existing): Yellow square
- Existing Tree (retained): Green circle
- Existing Tree (removed): Dashed circle
- Existing (internal remodelled): Blue dashed line
- Design feature, see 6.4.2: Orange circle with an 'x'

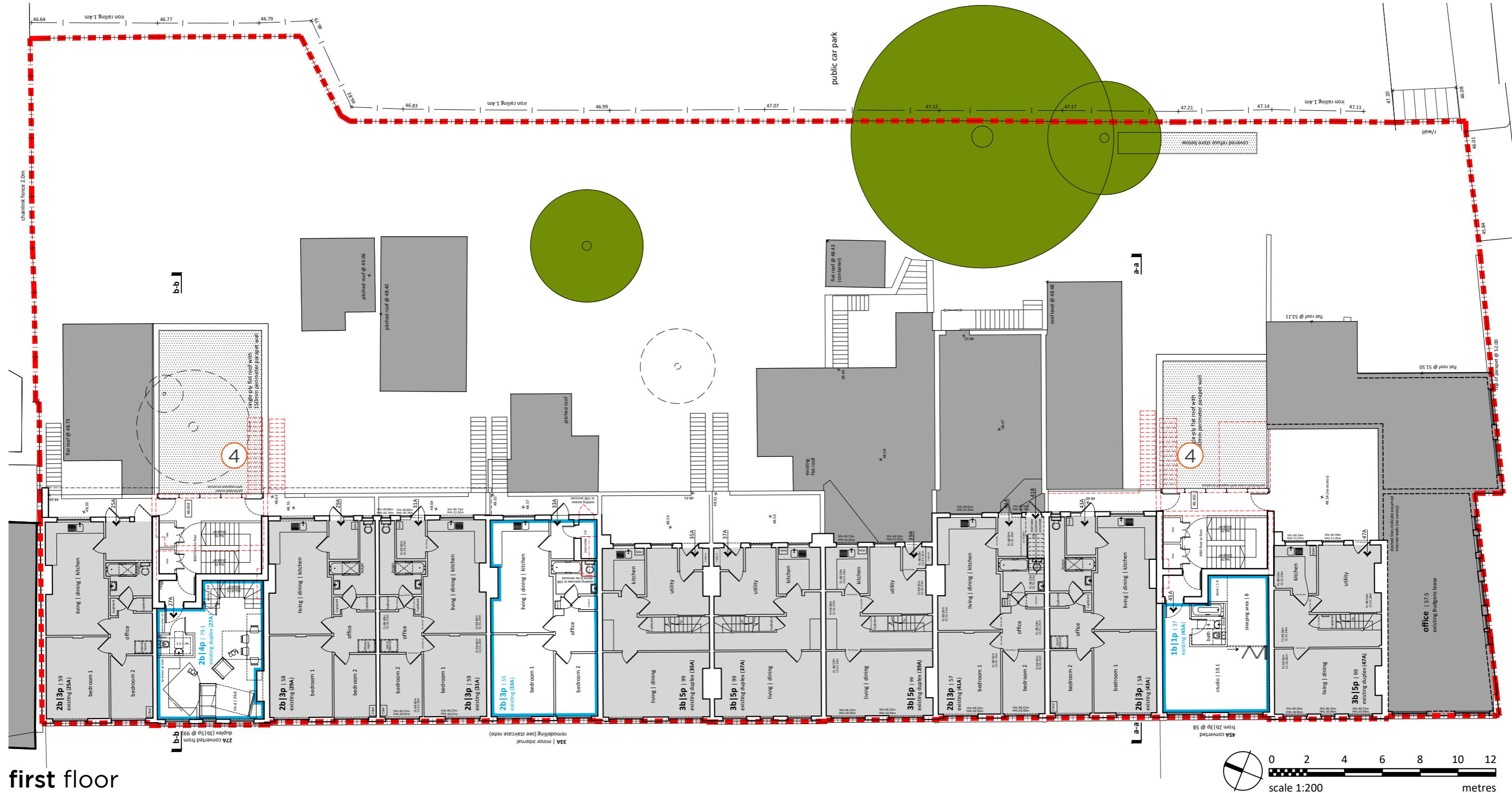


## 6.4 Design Proposal | Layout

### Key:

Legend for the site plan:

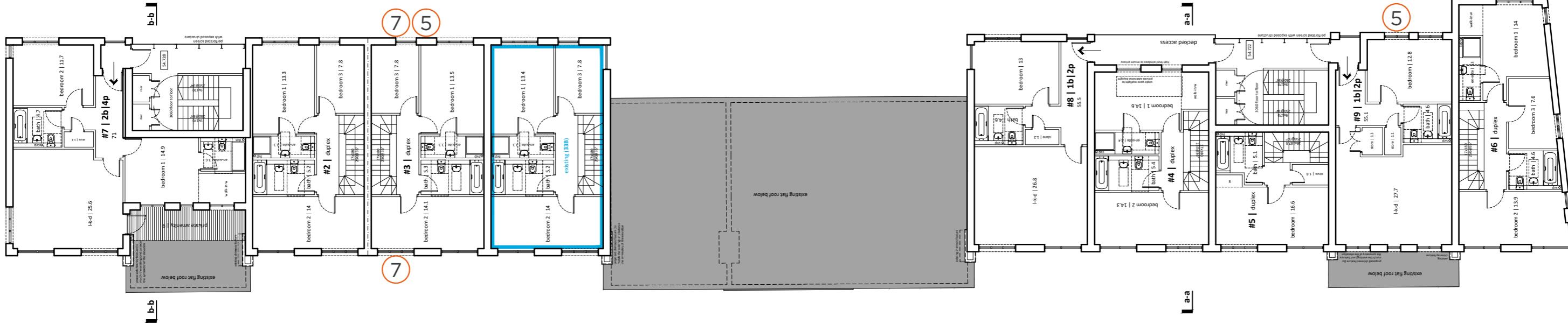
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## 6.4 Design Proposal | Layout

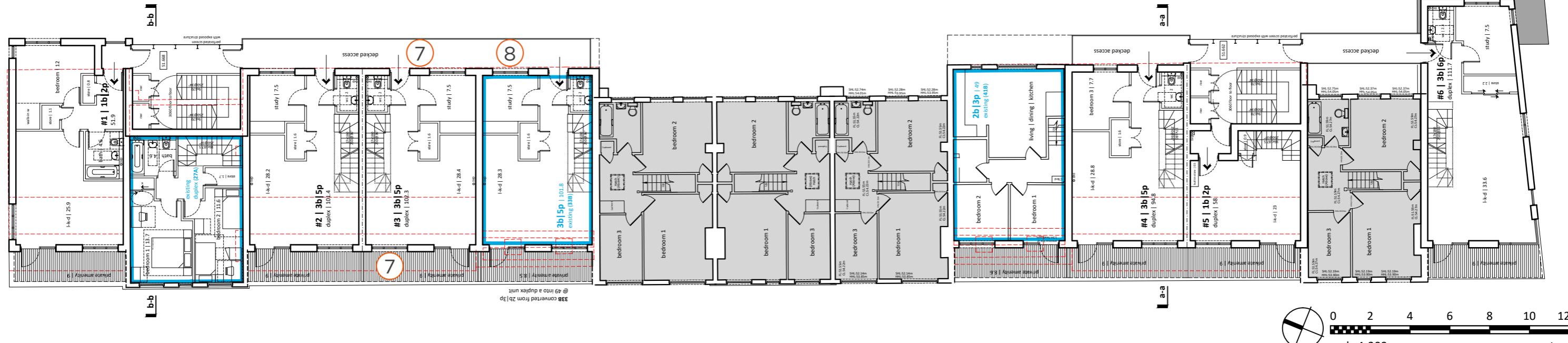
### Key:

- Site boundary
- - - Demolished
- Service Road boundary
- Existing Building (retained)
- Soft landscaping (proposed)
- Soft landscaping (existing)
- Existing Tree (retained)
- Existing Tree (removed)
- Existing (internal remodelled)
- Design feature, see 6.4.2

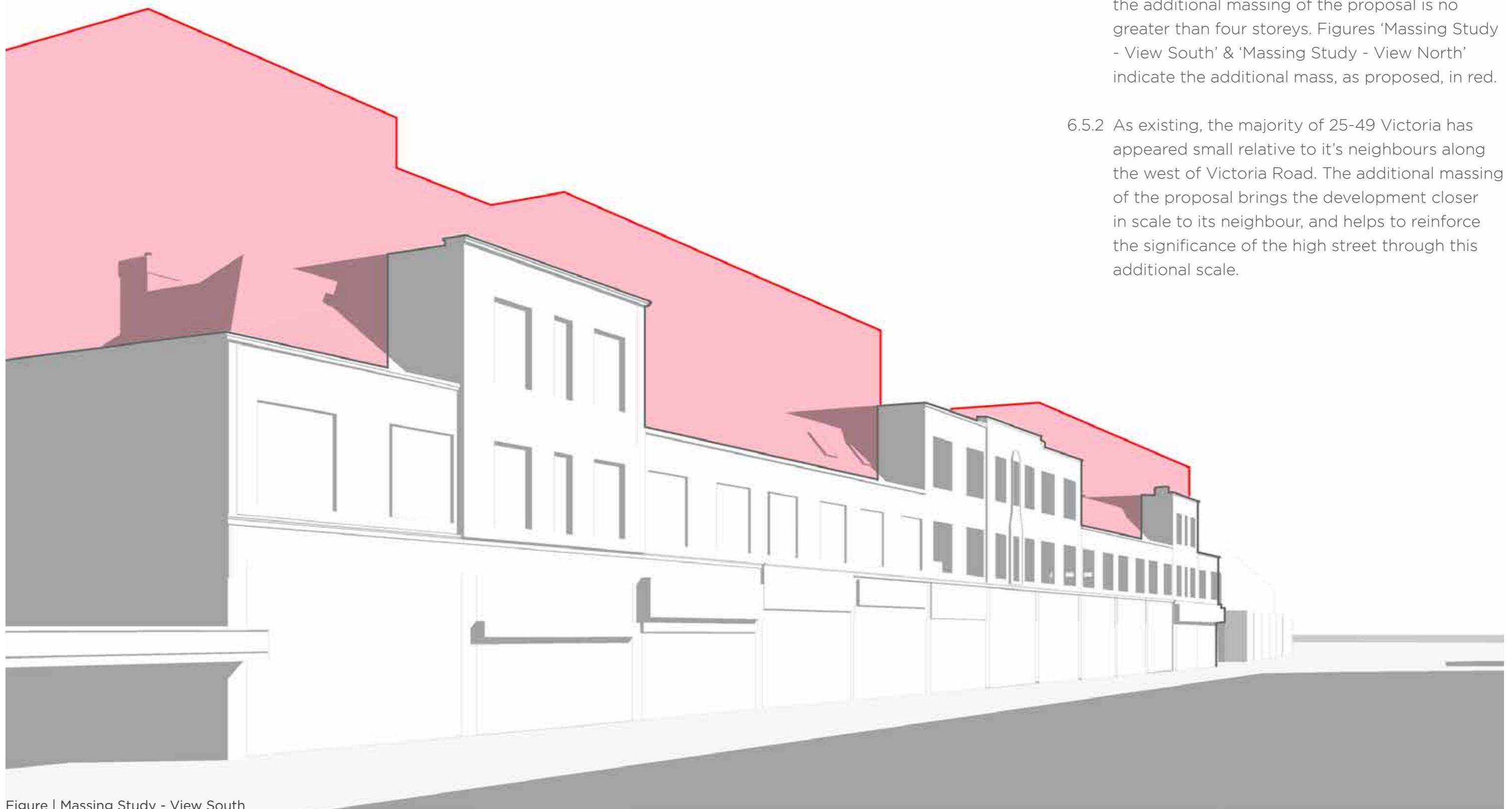


**third floor**

**second floor**



## 6.5 Design Proposal | Scale



### Scale

- 6.5.1 The proposed extension to Victoria Terrace range between one to two storeys, ensuring that overall the additional massing of the proposal is no greater than four storeys. Figures 'Massing Study - View South' & 'Massing Study - View North' indicate the additional mass, as proposed, in red.
- 6.5.2 As existing, the majority of 25-49 Victoria has appeared small relative to its neighbours along the west of Victoria Road. The additional massing of the proposal brings the development closer in scale to its neighbour, and helps to reinforce the significance of the high street through this additional scale.

Figure | Massing Study - View South

## 6.5 Design Proposal | Scale

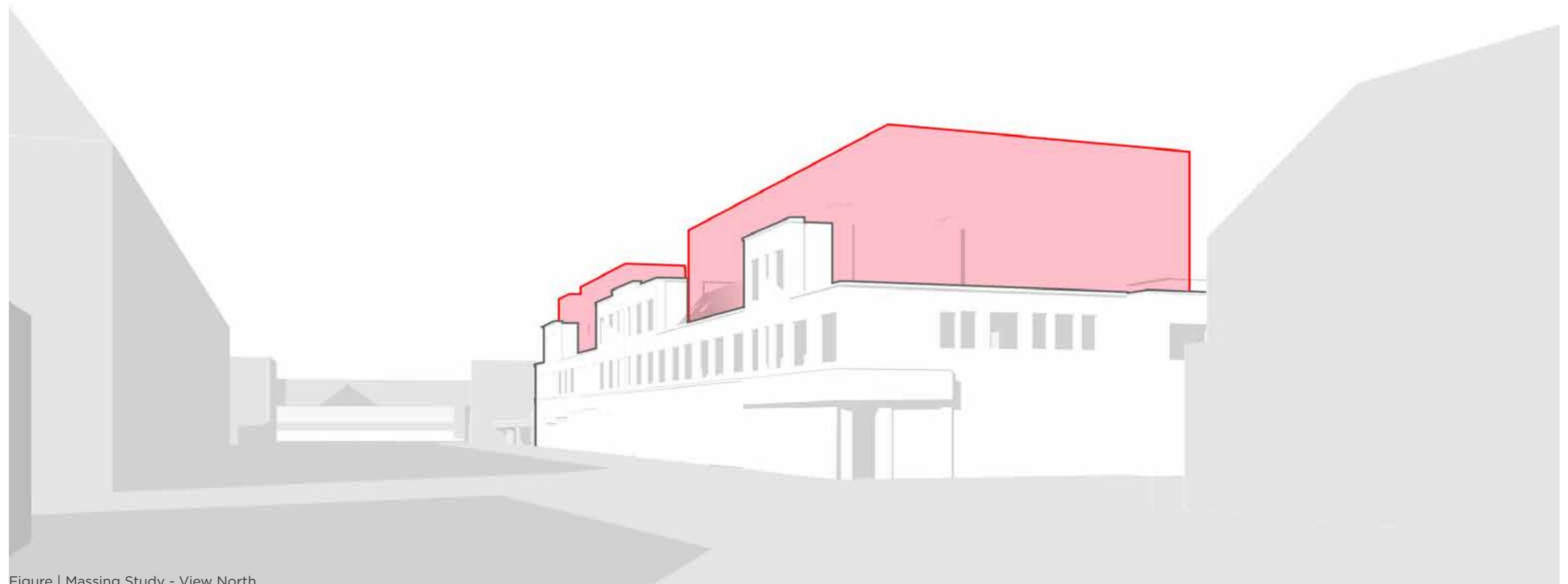


Figure | Massing Study - View North

## 6.6 Design Proposal | Appearance

### Appearance

6.6.1 The existing terrace of shops and residences along 25-49 Victoria Road comprises a symmetrical composition constructed from traditional materials including facing brickwork, tiled pitched roofs, and flat roofs set behind parapets. The proposed design seeks to create a cohesive extension that respects the character outlined above.

6.6.2 As part of the ethos for respecting the existing development key design choices include the following:

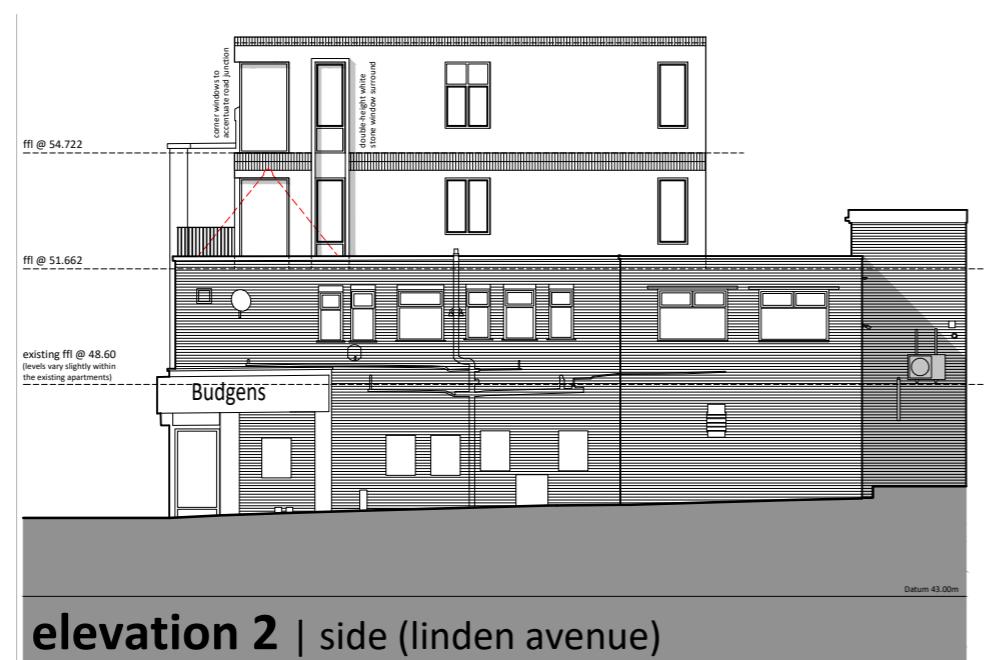
- 1 Upper floor extensions are stepped back, creating a subservient development that retains the legibility of the existing three storey protrusions at the centre and ends of the terrace.

2 The the proposal retains the symmetrical composition established by the existing terrace, and retains the emphasis placed on the centre of the terrace as a focal point.

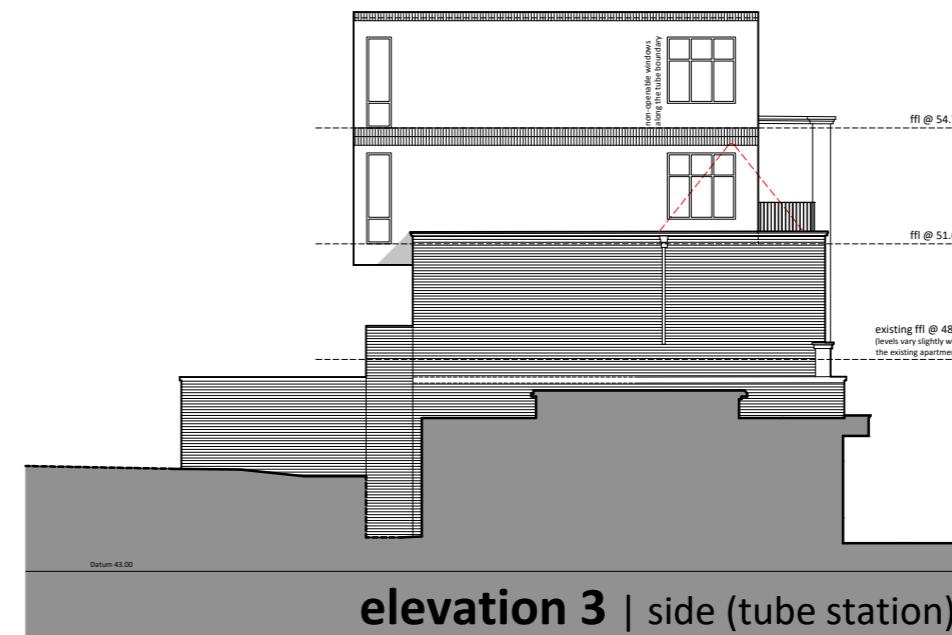
3 Fenestration retains the proportions and a gridded arrangement established by the existing premises at first and second floor level.



**elevation 1 | front (victoria road streetscene)**



**elevation 2 | side (linden avenue)**



**elevation 3 | side (tube station)**

## 6.6 Design Proposal | Appearance

4 Proposed facing materials include brick and stone, in keeping with the existing terrace and neighbouring properties along Victoria Road. The similarity in materials assists in retaining the red brick avenue of Victoria Road more widely, with stone and brick detailing adding additional visual interest.

5 The amenity spaces to the front of the proposal are of a lightweight metal, helping to retain and keep legible the silhouette of the original design, whilst also being in keeping with the adjacent terrace of 51-75 Victoria Road.

6.6.2 Figures 1-4 (below) highlight the local context of Victoria Road, and the local context that has informed the appearance of the proposed development.



Fig 1. Victoria Road - East. It can be seen how the central portion of the terrace is emphasised through additional massing and stone detailing to the window.



Fig 2. Victoria Road - East. Fenestration is regimented and legible, with similarly proportioned windows repeated and first floor, and extended to the second.



Fig 3. Victoria Road - East. Instances of stonework windows surroundings are present along the main high street, such as above.



Fig 4. Victoria Road - East. A stepped back upper floor provides elevated access, metal railing minimises the massing of the lower storey.

## 7.0 Summary

### 7.1 Closing Statements



## 7.0 Summary | Closing Statements

### Use

The proposal seeks the creation of a total of 9no. new units (use class C3) at second and third floor levels, and 26 sq.m of new commercial floorspace (Class E) at ground floor.

### Amount

The proposal seeks permission for the extension of a part two, part three storey building, extending it into a part three, part four storey building. This provides an additional 9no. residential units (use class C3), providing residential accommodation in a desirable and sustainable town centre location.

### Layout

The layout seeks to retain existing servicing arrangements, improve on pedestrian accessibility and security, and improve connections to local transport for new and existing residents alike.

### Scale

The proposal seeks to create an increased massing, that is both sympathetic to the silhouette of the existing terrace but affirming in the importance of the local high street.

### Landscaping

The proposal largely retains the existing landscaping arrangements, owing the their suitability for the mixed use of the site. Additional soft landscaping, and a demarcated footpath are proposed along the eastern boundary however where opportunity is present.

### Appearance

The proposal seeks to retain a cohesive character with the existing terrace of 25-49 Victoria Road, through the incorporation of traditional materials and a fenestration treatment in keeping with the existing upper floors. An element of distinction between the two phases is provided through the stepping of the massing.



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