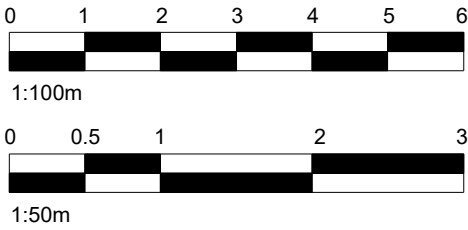


THIS DRAWING IS COPYRIGHT and must not be traced or copied in any way of form in part or whole by any means whatsoever without prior written consent and may only be used by the present owner, being our current client in relation to the property as referred to on the drawing.

- GENERAL NOTES**
- CHECK ALL DIMENSIONS ON SITE.
 - ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE STATED.
 - ALL VERTICAL MEASUREMENTS ASSUME GROUND TO BE LEVEL UNLESS OTHERWISE STATED.
 - THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH THE GENERAL NOTES.
 - WORKS TO BE CARRIED OUT WITH MATERIALS AND WORKMANSHIP IN COMPLIANCE WITH APPROVED DOCUMENT FOR REGULATION 7 (THE AMENDED BUILDING REGULATIONS 2010)
 - WORKS TO BE CARRIED OUT IN A SAFE MANNER IN ACCORDANCE WITH CDM REGULATIONS 2015.
 - OPEN UP EXISTING STRUCTURE AS REQUIRED BY THE BUILDING INSPECTOR
 - THIRD PARTY SUPPLIER TO MEASURE ON SITE BEFORE MANUFACTURING
 - GENERAL CONTRACTOR TO VERIFY FIELD CONDITIONS PRIOR TO COMMENCEMENT OF EACH PORTION OF THE WORK.
 - ANY DISCREPANCIES IN THESE DRAWINGS SHOULD BE NOTIFIED TO US PRIOR TO ANY WORKS PROCEEDING.
 - UNLESS OTHERWISE INDICATED, PLAN DIMENSIONS ARE TO COLUMN GRID ON CENTERLINES, NOMINAL SURFACE OF MASONRY, FACE OF STUDS AND FACE OF CONCRETE WALLS AND BEAM CENTRE TO CENTRES.
 - "FLOOR LINE" REFERS TO TOP OF CONCRETE SLABS. FINISH FLOORING IS INSTALLED ABOVE THE FLOOR LINE. FOR DEPRESSED FLOORS AND CURBS, SEE STRUCTURAL DRAWINGS.
 - REPETITIVE FEATURES ARE NOT ALWAYS DRAWN IN THEIR ENTIRETY AND SHALL BE COMPLETELY PROVIDED AS IF DRAWN IN FULL.
 - WHERE A DOOR IS LOCATED NEAR CORNER OF ROOM AND IS NOT LOCATED BY DIMENSION ON PLAN OR DETAILS, DIMENSION SHALL BE 100MM FROM FACE OF STUD (WALL).
 - LINE OF EXISTING FLOOR SLABS, AS SHOWN ON THE BUILDING ELEVATIONS AND SECTIONS ARE APPROXIMATE.
 - FLOOR LEVELS AND BOUNDARIES ASSUMED WHERE NOTED, DEPICTED BY LINE DASH LINES.
 - REFER TO ARCHITECTURAL, STRUCTURAL, MECHANICAL, ELECTRICAL, GENERAL SPECIFICATION AND OTHER CATEGORIES OR DRAWINGS FOR ADDITIONAL NOTES.
 - VERIFY SIZE/LOCATION/FINISH/FIRE-RATING, ETC. AND PROVIDE COMPLETE AND REQUIRED OPENINGS THROUGH FLOORS AND WALLS, ACCESS DOORS, FURRING, CURBS, ANCHORS & INSERTS.
 - CONTRACTOR TO CARRY OUT MOST LOGICAL SOLUTION BUT TO CHECK WITH ARCHITECT OR ENGINEER IF UNSURE, REQUESTS BY CLIENTS THAT DEVIATE FROM DESIGN VOIDS THE DESIGN LIABILITY.
 - SEE STRUCTURAL GENERAL NOTES AND PLANS TO COMPLEMENT ARCHITECTURAL PLANS AT ALL TIMES, DO NOT ASSUME ANYTHING.



No.:	Revision:	Date:
Bischell - Design & Build - Suite 114 80 Cumberland House, Scrubs Lane Hammersmith & Fulham, NW106RF www.bischell.co.uk		
Client: -- Bartram Close, Uxbridge UB8 3AX		
Drawing Title: Site Plan - Condition 4		
Status: BR		
Scale:	1 : 100 @A2	Date: 05/04/2022
Drawn By:	Author	
Drawing No.:	022	Rev:

CONSTRUCTION METHOD STATEMENT

BARTRAM CLOSE, UXBRIDGE, UB8 3AX

Reference is made to the planning approval of this site 75240/APP/2021/4431 from Hillingdon Council and a request for the Construction Methodology in respect of work procedures to be developed and submitted before the works commence (Condition No. 04 as stated below)

No site clearance or construction work shall take place until the details have been submitted to, and approved in writing by, the Local Planning Authority with respect to:

1. A method statement outlining the sequence of development on the site including demolition, building works and tree protection measures.

It is the paramount intention of the Principal Contractor under CDM 2015 Regulations, to protect the health, safety and welfare of all persons who are associated with or who may be affected by the proposed works.

The planning permission has been granted for the erection of two, one-bedroom chalet type semi-detached bungalows with car parking and associated facilities on the site of former garages at Bartram Close in Hillingdon.

The only access/ egress route for both pedestrian and vehicle use is from Bartram Close. The site is well separated from neighboring dwellings and that the access route offers reasonable width with made up surfaces throughout the site. Risks to pedestrians are associated with crossing the site access route from Bartram Close and using that access route together with site vehicular traffic turning into and out of this route with consideration for road users passing the site access.

The site will be suitably contained and vehicular movements will be managed, appropriately. All drivers will be made aware of site procedures and restrictions in advance of arrival. Suitable hazard warning signage will be posted at the entrance and other vulnerable areas

SEQUENCE OF DEVELOPMENT

TREE PROTECTION MEASURES

Multi Stem Conifer Tree is located near the boundary line. This tree is proposed to be pruned. Ensure to clear the neighboring area from any spill over and make it clean.

Once the tree is pruned, erect fence surrounding the tree as per the drawing submitted (subjected to approval) to protect the tree. Such fencing should be minimum height of 1.5m.

It has been noted that no site clearance works are needed. However, it will be ensured that no site clearance or development shall be commenced until the erection of the fencing.

Thereafter, the development shall be implemented in accordance with the approved details. The fencing shall be retained in position until development is completed. The area within the approved protective fencing shall remain undisturbed during the course of the works and in particular in these areas:

- There shall be no changes in ground levels;
- No materials or plant shall be stored;
- No buildings or temporary buildings shall be erected or stationed.
- No materials or waste shall be burnt; and.
- No drain runs or other trenches shall be dug or otherwise created, without the prior written consent of the Local Planning Authority.

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LOADING AND UNLOADING OF PLANT AND MATERIALS

This concerns traffic management in general both within the site and externally with consideration for the pedestrian and vehicle route turning off Bartram Close with outside pedestrians crossing the entrance.

All vehicle movements into the site will be pre-planned and arrivals and departures will be conducted under the direction of banksmen. Drivers arriving will be pre-instructed in respect of the on-site traffic management plan and directed to give pre-warning of their arrivals to enable banksmen to be in place before these arrivals.

It is not proposed to permit any drivers associated with the works to lay up or park outside the site – on the road or the entrance route both of which will be kept clear at all times except for vehicular movements which are not likely to be excessive.

The 1.5M pedestrian splay has been proposed for both the entrances ensuring the clear visibility wide. Ample hazard warning signage will be installed and this will be particularly useful to make pedestrians aware of traffic movements as vehicles leave the buildings fronting the pavement.

STORAGE OF PLANT AND MATERIALS USED IN CONSTRUCTING THE DEVELOPMENT

No materials, equipment or other items connected with the works will be left outside the site. All such items can and will be comfortably contained within the working environment and site footprint.

HOURS OF WORK

Development will take place during normal working hours ie 8.00am to 5.00pm Monday to Friday and no work to take place at weekends or Bank holidays.

Works will not take place outside normal working hours unless they can be suitably contained and restricted.

Due care and consideration will be taken in respect of all those who may be affected by the works and they will be notified accordingly.

Unavoidable noisy and dusty work will be contained as far as possible and in the event of unavoidable circumstances, affected parties will be notified beforehand.

All debris arising from the site to be immediately loaded into skips. Full skips to be netted and transported from the site to licenced waste disposal tips.

No fires or burning of materials to take place on the site.

All immediate neighbours will be consulted before the works commence and any reasonable concerns they may have will be taken into account both before and during the works.

HEALTH AND SAFETY

The site will be entirely fenced off and to clearly display health a safety notices requiring only authorised persons onto the site and requiring all operatives working on the site to wear safety helmets and steel capped shoes.

CONCLUSION

The Principal Contractor should operate with approved sub-contractors who have to pass a rigorous appointment procedure with the submission of Risk Assessments and Method Statements [CDM 2015] before start on site.

All those selected to work on site must receive a recorded Site Induction