

National Grid Electricity Transmission plc
ZC Overhead Line Diversion in Colne Valley for HS2
Background - Reinstatement Note

This background note is prepared in support of a request for the agreement to a site restoration scheme under Schedule 16 paragraph 5 (1) of the High Speed Rail (London – West Midlands) Act 2017.

The S16 submission comprises the following drawings and specifications:

- Schedule setting out restoration and planting.
- ZC Background Reinstatement Note (this document) including Sketch plans to show location of areas where site restoration will take place.
- ZC Habitat Restoration Plan

Background

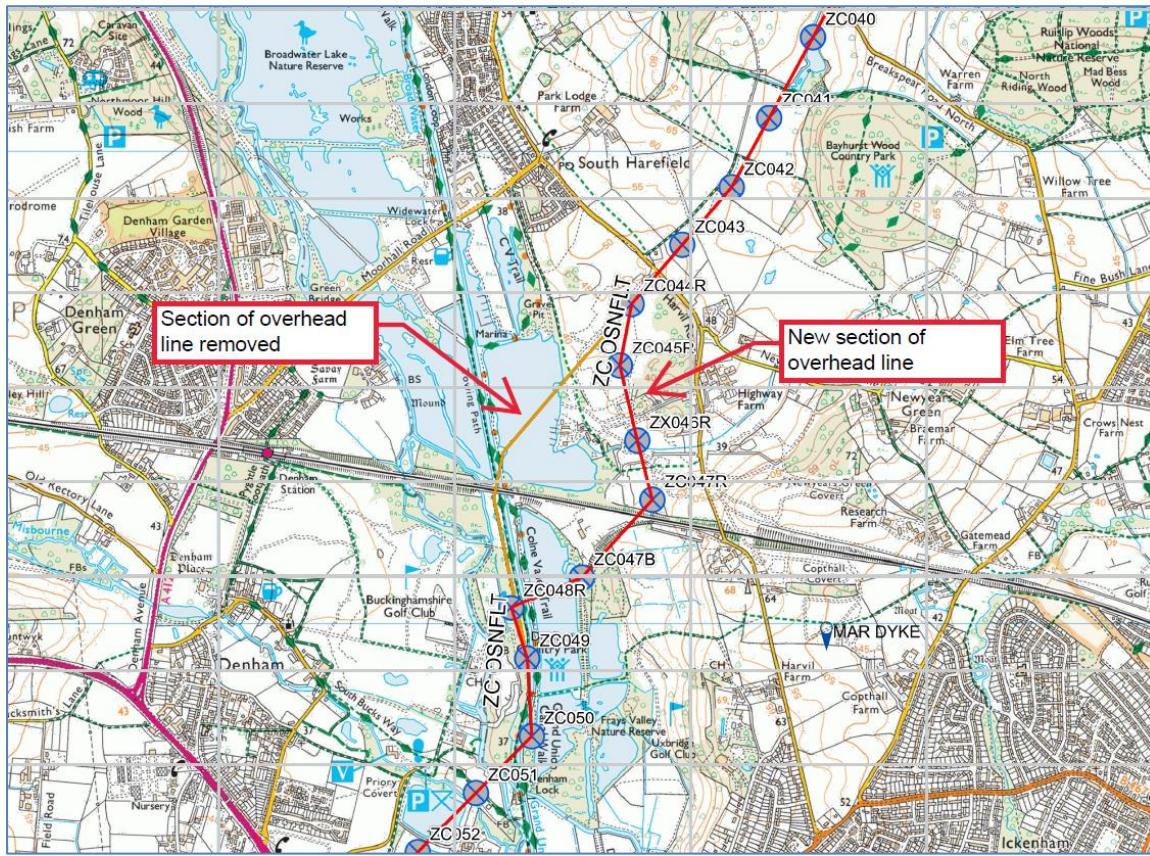
National Grid was required to modify its overhead line (OHL) network on the ZC route to allow construction of the High Speed 2 (HS2) railway line within the Colne Valley where HS2 crosses the National Grid's existing ZC overhead line in span ZC45-46 on a proposed viaduct. The works, in summary, for the diversion of the ZC route between pylons ZC44 and ZC48 comprised:

- Removal of five existing pylons - ZC44, ZC45, ZC46, ZC47 and ZC48;
- Construction of six new pylons - ZC44R, ZC45R, ZC46R, ZC47R, ZC47A, and ZC48R;
- Re-wiring of overhead line between pylons ZC42 and ZC49;
- Installation of a temporary mast (ZC44T) and overhead line between pylons ZC43 and ZC45;
- Earthing works at pylons ZC38, ZC39, ZC50, ZC51 and ZC52.

These works took place within the Colne Valley. The works are split across the administrative areas of London Borough of Hillingdon and Buckinghamshire County Council.

The works on the ZC Overhead Line took place between 2019 and 2024. The new line was built and energised in 2021 and the removal of the old pylons completed in December 2021. Reinstatement works were started in March 2022 and final site clearance was completed in May 2024. A temporary site compound was established for these works to the west of Harvill Road which accommodated the storage of plant, materials and site offices. This temporary compound and land held temporarily by National Grid to the west of Harvill Road has been transferred to other parties acting on behalf of HS2, who are still working at the site, and such the land has not been fully reinstated, and is not covered by this submission.

Location Plan – ZC Overhead Line Diversion



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Works Undertaken at Site

In order to undertake the works a number of temporary access improvements were made from the public highway, as approved within separate Schedule 4 submissions made to London Borough of Hillingdon and Buckinghamshire Council. S33 submission for temporary bridges across watercourses were also made to the EA and LLFA.

Access and accommodation works for the project included:

- Installation of temporary stone access tracks and hard standings to support new and temporary pylon/temporary mast construction – including creation of new/widening of existing temporary access points through hedgerows between fields.
- Installation of temporary trackway to access pylons – including creation of new/widening of existing temporary access points through hedgerows between fields.
- Erection of temporary fencing.
- Erection of scaffolding
- Installation of temporary culverts and bridges to cross ditches (approved within separate Schedule 33 submissions to relevant LLFA).
- Installation of a temporary bridge over the River Colne.

Prior to the works taking place the land was being used for a variety of purposes including agricultural use, minerals yard and for recreational purposes including, woodland, fishing lakes, golf courses and Denham Country Park. The land, once no longer required by HS2 in due course, is to revert to its previous uses save for where it is in the HS2 trace.

Access to the overhead line works in Denham Country Park was taken outside of Act limits via voluntary agreement with the landowners, this was done to minimise ecological impacts as a GCN pond was located within the original access route. Information within this submission covers the restoration of the out of Act working area but does not technically form part of the Schedule 16 submission.

Site Restoration Scope

National Grid's contractor, Morrison has now left site. Land taken via temporary possession for the ZC Overhead Line has now been either passed onto to other Contractors acting on behalf of HS2 or returned to HS2 so that it can be handed directly back to landowners in due course. Some land returned by National Grid to other parties will not be returned to landowners and has been taken by HS2 on a permanent basis in the HS2 trace. This restoration scheme for agreement details the scheme which has been implemented in areas where no further HS2 works are expected so to ensure the land once returned to landowners is provided in a satisfactory condition. It relates only to land taken on a temporary possession basis and forms land outside of the area that will be retained for HS2 main works.

The restoration for each land parcel has been discussed and agreed with individual landowners, but has in the majority of cases involved the removal of temporary fencing, stone roads and hardstanding, together with culverts, gates and trackway installed to deliver the overhead line works. Where required the land has been levelled to restore the land to its previous condition, and where topsoil was stripped to facilitate the work it has been returned in accordance with the ZC diversion schemes Soil Management Plan. Sections of hedgerow where these were removed to facilitate access and working areas have been replanted with same species initially removed, where no further HS2 works are to take place.

Restoration activities such as removal of trackway, stone, gates, fencing and culverts and has been undertaken by National Grid's main works contractor. Where restoration is required following the completion of contractors works in some instances this has/ will be undertaken by the directly affected landowner e.g Buckinghamshire Golf Course. In these cases individual agreements have been reached with the directly affected landowners for them to carry out restoration of site to their own satisfaction. Activities that will be undertaken by landowners include re-seeding of grassland and management of replanted hedgerows/woodland depending on the landowners preference. Landscape works undertaken were done by a sub-contractor to the National Grid Main works contractor Morrison.

In those areas where land has been passed onto another HS2 contractor, soil stockpiles remain, these will be restored by other parties acting on behalf of HS2 at a later date as part of the main HS2 works. The ZC main works compound area and access tracks to pylons to the west of Harvill Road have been left in at request of HS2 and are being utilised by follow on

contractors at site, these will be restored by other parities acting on behalf of HS2 at a later date as part of the main HS2 works.

Habitat Restoration Plan

A site specific habitat restoration plan was developed to guide habitat reinstatement, compensation and enhancement requirements for the ZC Colne Valley Overhead Line Diversion. It covers those parts of the site reinstatement where there are specific ecological requirements, including compensation or enhancement that is a requirement of bat and great crested newt (GCN) licenced mitigation, or ecological enhancements that have been agreed with stakeholders such as Denham Country Park. Where works are proposed within Denham Country Park the scope was developed in consultation with Buckinghamshire Country Council, Park Manager Andrew Fowler.

Appendices

A – Reinstatement Note ZC 43 -42

B – Reinstatement Note ZC 46

C – Reinstatement Note ZC 47R – 47A

D – Reinstatement Note ZC 47 – 48A

E - Reinstatement Note ZC – Access to Denham Country Park