

DESIGN & ACCESS STATEMENT

FEBRUARY 2024



PROJECT:

UNIT 4, SWALLOWFIELD WAY, HAYES

CLIENT:

7 STAR FURNITURE LTD

WAMM
Architecture & Building Consultants

1.1 INTRODUCTION



Aerial photograph showing the site location

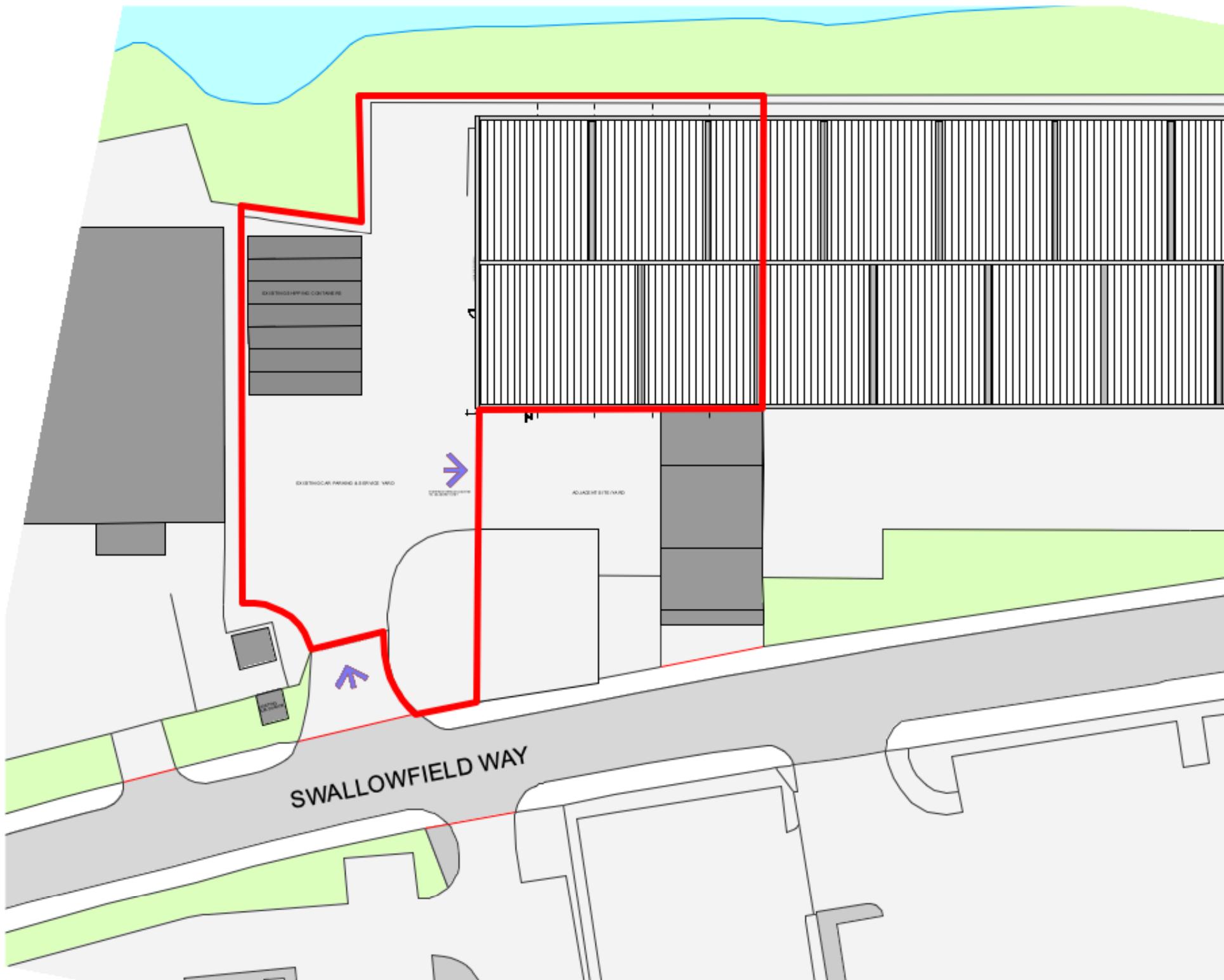
This proposal has been prepared by WAMM Consulting on behalf of 7 Star Furniture Ltd for the extension of the current B2/B8 warehouse at Unit 4 Swallowfield Way, Hayes.

The application proposes the erection of a two storey extension to provide additional warehouse space. The extension would be located to the side of the existing unit.

Internally the extension would provide a reception, offices and W/C facilities. A new parking layout is also proposed.

This application follows the extant full planning consent for a 10m warehouse and showroom extension under ref:72037/APP/2020/2554.

2.1 THE SITE - LOCATION



1 EXISTING SITE PLAN
Scale: 1:500

Site Location Key Points:

- Application site area is 0.23ha (0.56 acres).
- Currently partially occupied by 7 Star Furniture warehouse and container storage.
- Located within an area with established B2/B8 use.
- Within an area of varied scale and industrial and commercial finishes
- Benefits from close proximity main arterail routes and Heathrow
- Vehicle access is currently off Swallowfield Way
- Shared access with adjacent commercial unit.
- Backing onto the Grand Union Canal and located within Flood zone 1 - which has a low probability of flooding.

2.2 SITE & CONTEXT PHOTOGRAPHS



3.1 PLANNING CONTEXT

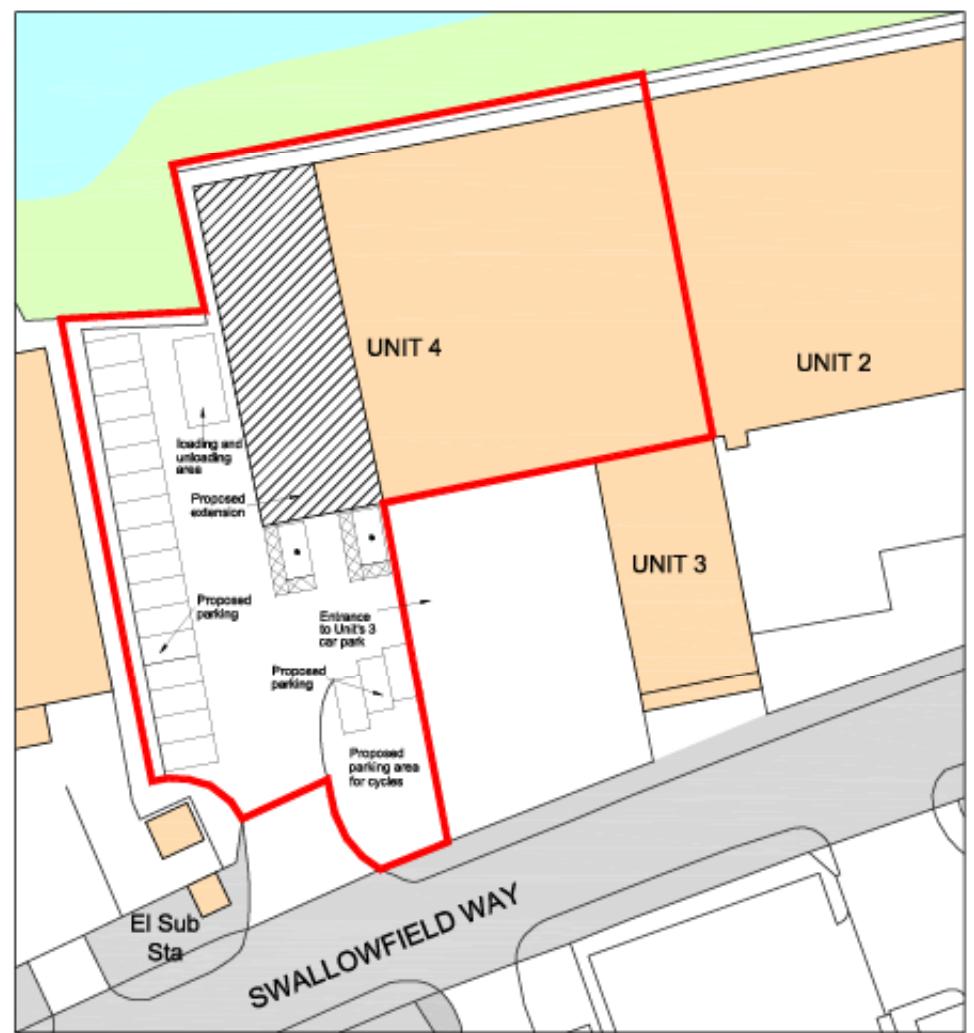
The details of the current application proposals follows the Extant consent on the site dated 2006 followed by a lapsed planning consent in 2009.

Extant Planning Consent

Application ref: 72037/APP/2020/2554 -20th August 2020

Erection of a two storey extension to provide additional warehouse space and a showroom to an existing industrial building.

It is considered that the increase in floorspace was acceptable in principle and that the design and appearance of the extension would not cause harm to the character and appearance of the existing building, the streetscene and the wider area. Furthermore the development would not give rise to conditions that would be prejudicial to pedestrian and highway safety.

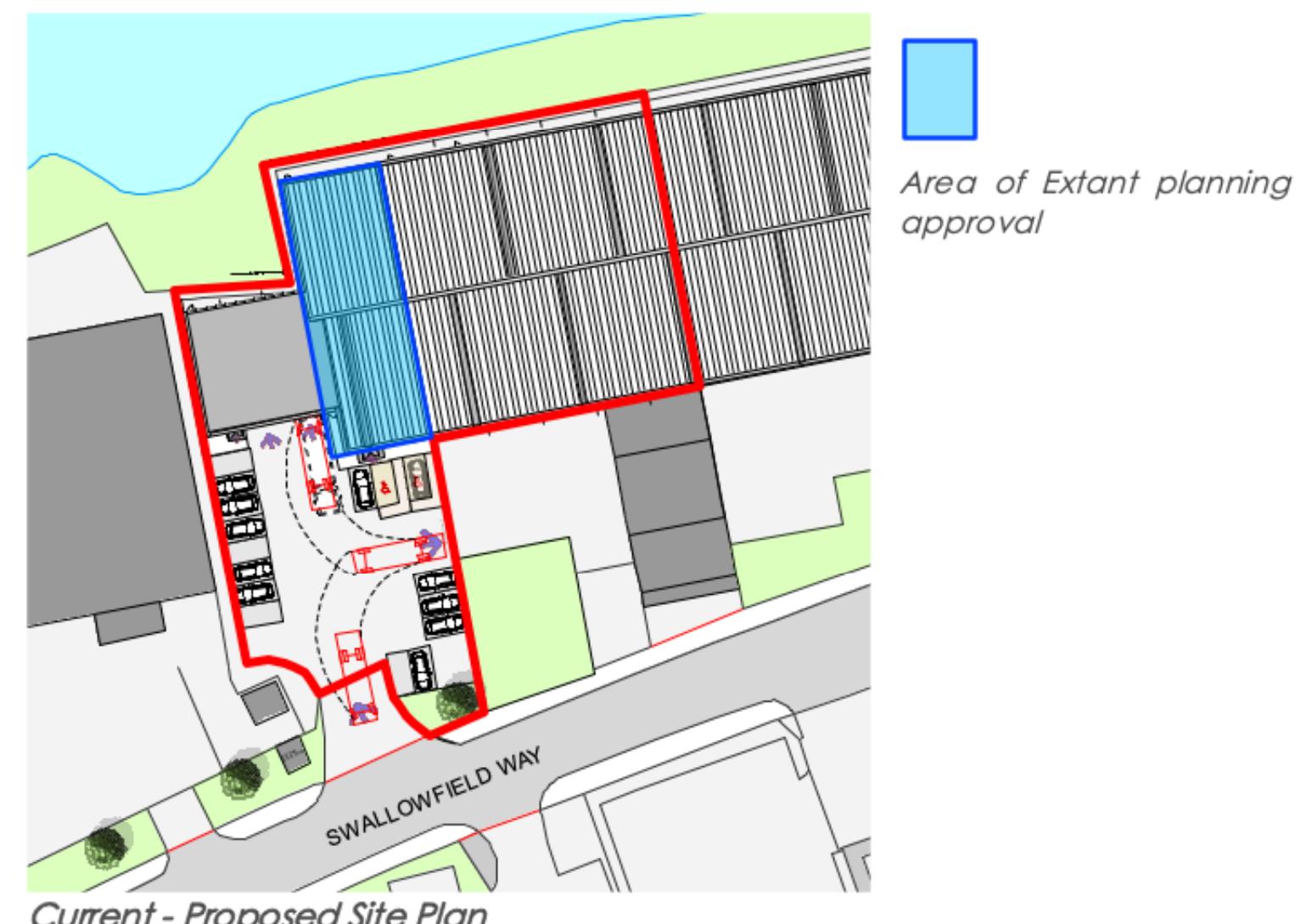


Approved - Proposed Site Plan

Current Proposals - Full Planning

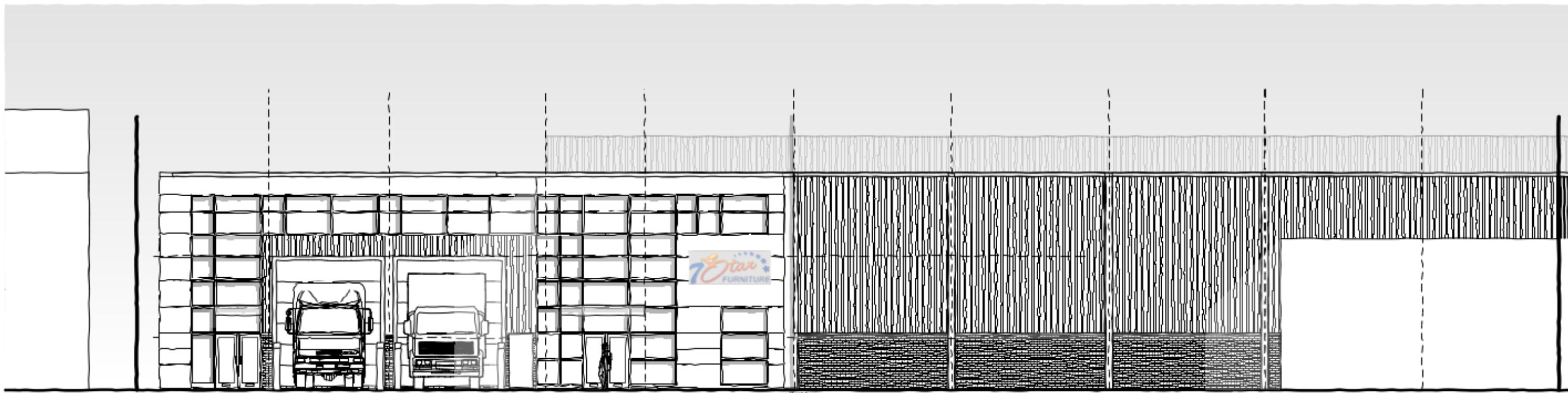
The application proposals look to include both the extant area extending further to the west with the aim of maximising the potential of the site.

The appearance of the extension is similar in nature to the approved with the aim of creating a sympathetic extension within the site and established streetscape.



Current - Proposed Site Plan

3.2 DESIGN APPROACH

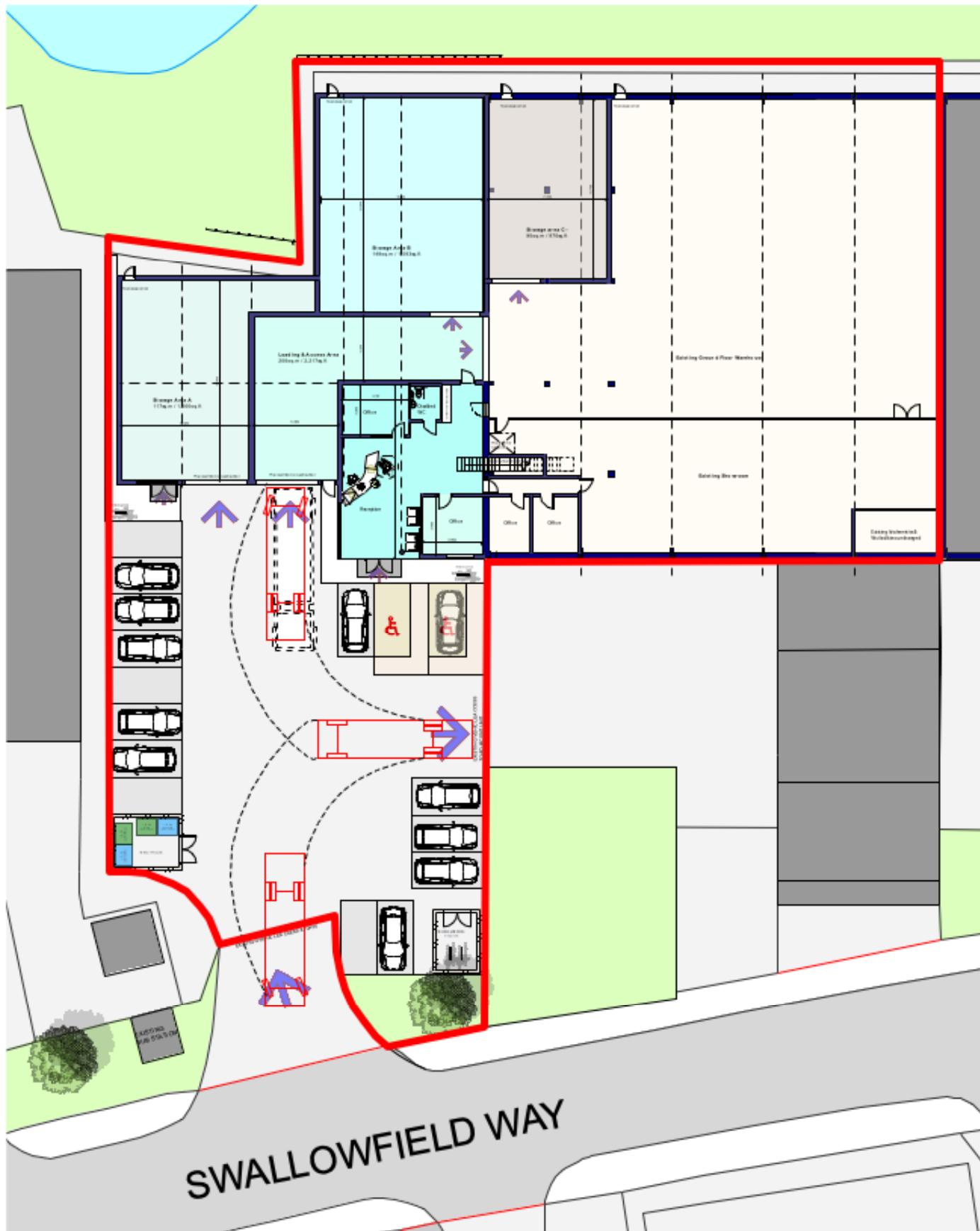


Proposed sketch view of development

KEY DESIGN REQUIREMENTS:

- Create a high quality commercial building respecting the scale massing of the existing context.
- Utilise the existing vehicle site access of Swallowfield way while maintaining the shared access of the adjacent commercial unit.
- Provide optimised loading and parking to the site frontage.
- Provide green areas to the frontage to soften and improve the appearance within the streetscape.
- Utilise a sympathetic palette of high quality materials to enhance the existing building and character of the local area.
- Adopt effective strategy creating a sustainable development.

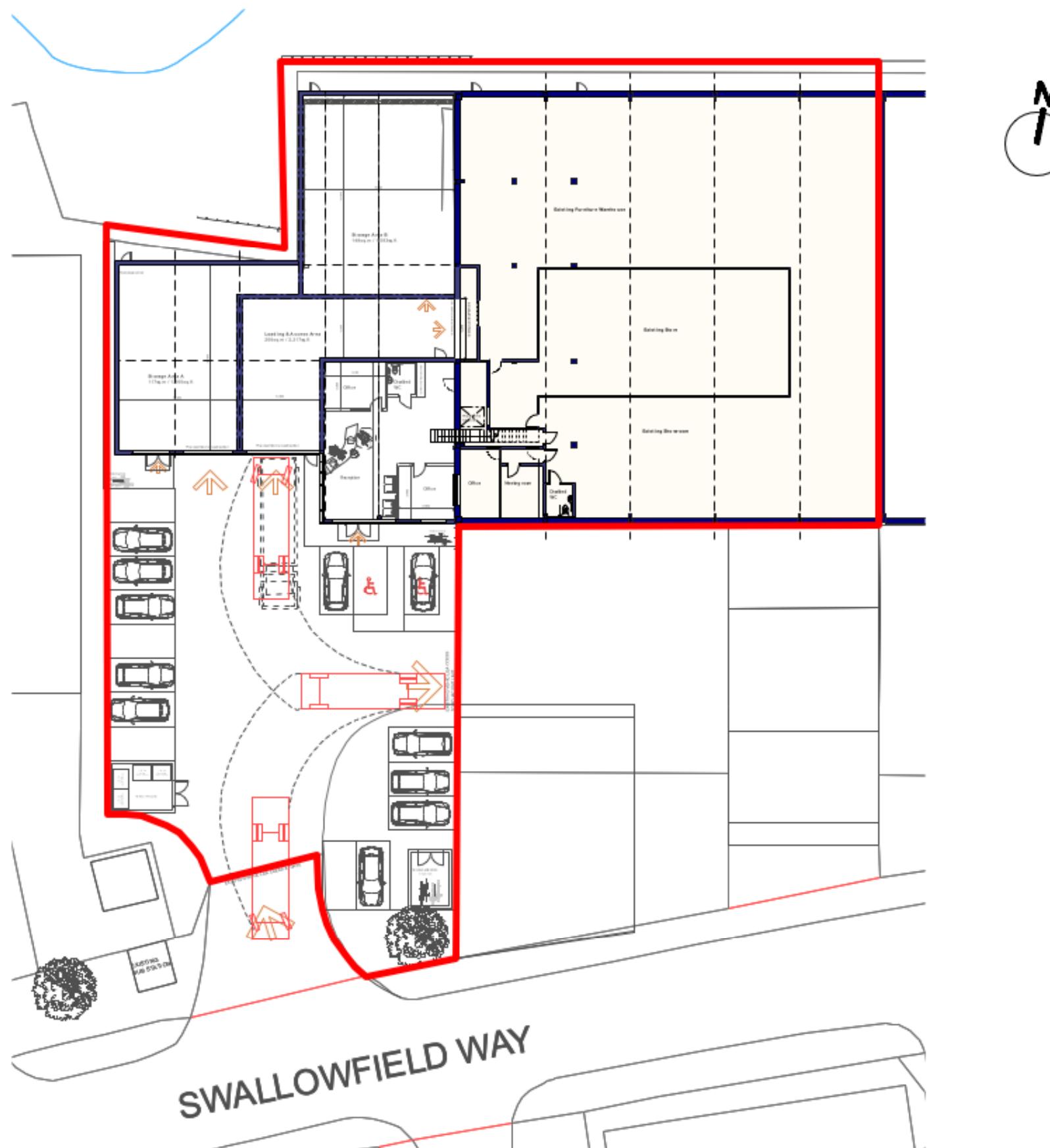
4.1 PROPOSED SITE LAYOUT & GROUND FLOOR PLAN



KEY POINTS:

- Existing vehicle, servicing and pedestrian access of Swallowfield way is maintained.
- The existing unit remains unchanged with extension proposed to the west, stepped for service loading access and extended to the site boundary.
- New car parking & service loading is proposed to the site frontage with the aim of optimising the use of this area.
- New greening to the site frontage proposed.
- 16 no Car parking spaces including 2 disabled spaces together with provision for on site turning.
- The internal accommodation comprises a main entrance and reception area together with segregated storage areas at ground floor for each department of the occupiers business.
- 2 x New service loading doors are proposed to the site frontage.
- A secure cycle store is provided to the site frontage together with short stay spaces directly in front of the commercial unit.

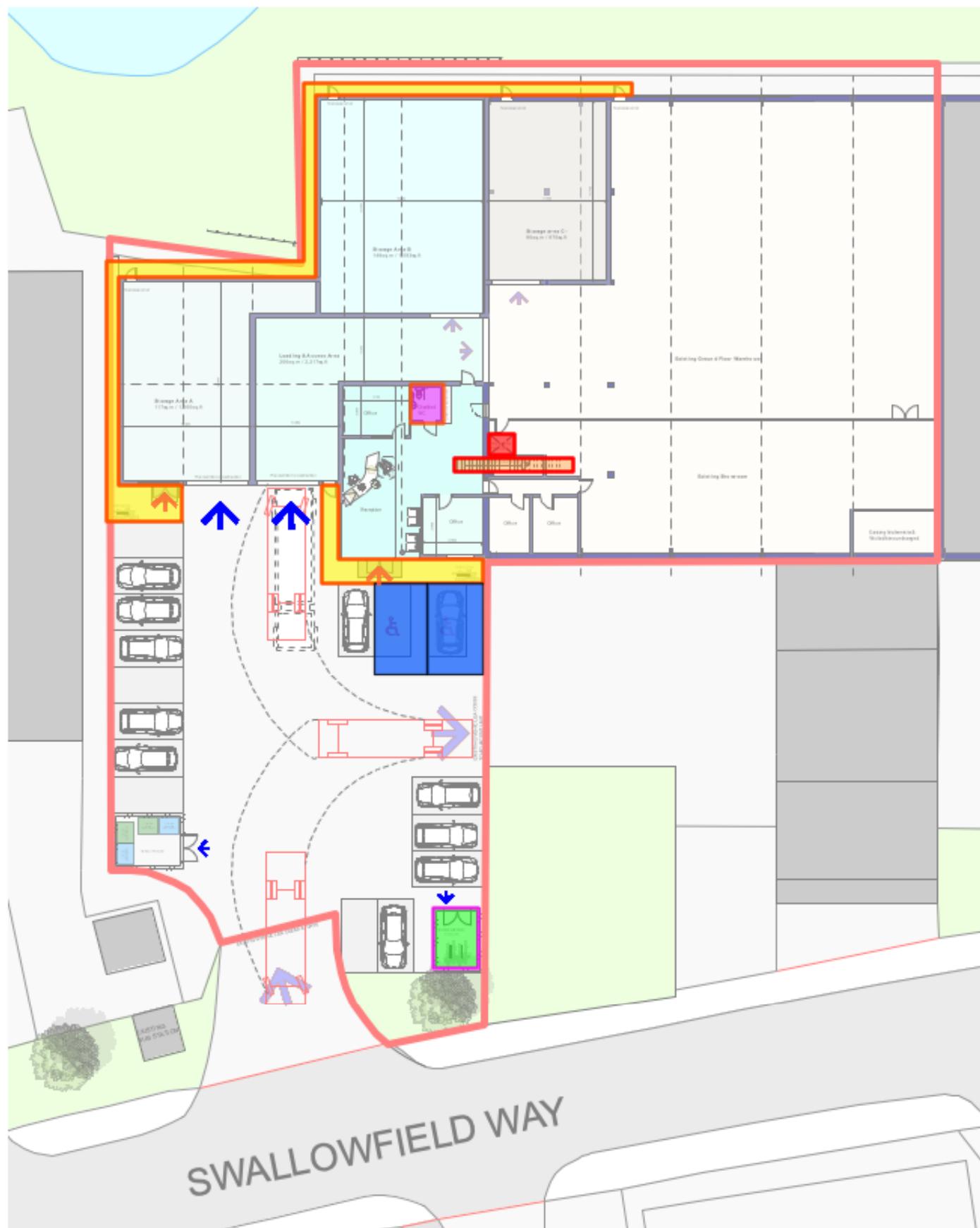
4.2 PROPOSED FIRST FLOOR PLAN



KEY POINTS:

- The proposed first floor accommodation is unchanged comprising a first floor showroom and storage areas.
- The existing staircase has been reconfigured to access the first floor accommodation from the ground floor reception area.

5.1 ACCESSIBILITY



Proposed Access Plan



Approach Gradients

All approaches to entrances are level for ease of access.

Entrances

Illuminated and level accesses into the primary and secondary access points into the building

Bicycle parking

Provision for 4 no. secure bike parking spaces

4 short term cycle parking spaces are located to the front of the site.

Car parking

2 x Disabled parking bays provided on site

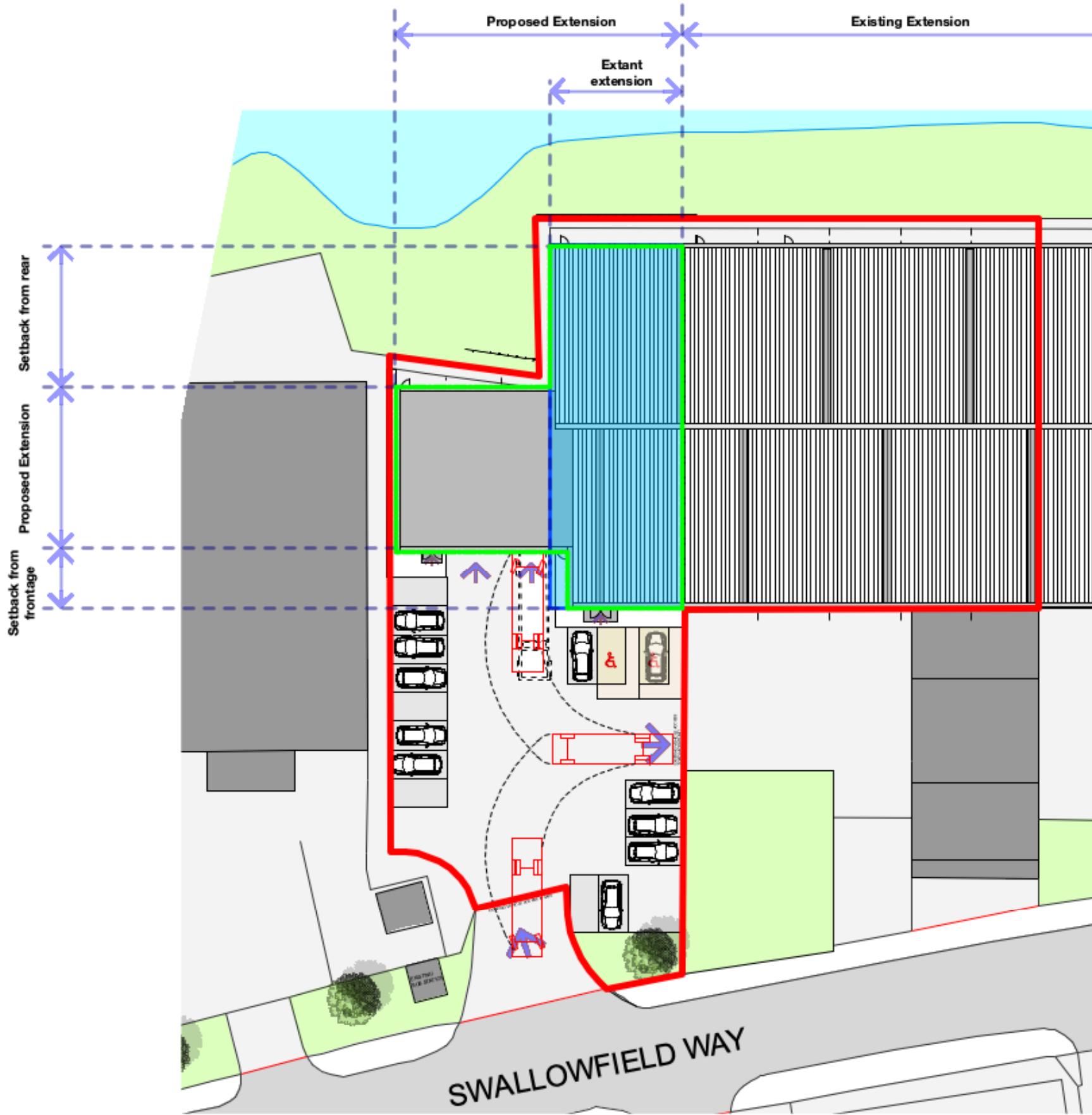
Stair & Lift

A fully compliant ambulant stair providing access to the first floor accommodation. Future provision for a DDA lift has been provided.

Drawing Key

- Bicycle parking for commercial unit
- Disabled parking bays directly in front of proposed unit
- Level access footpaths into all parts of site from surrounding car parking areas
- Level access to primary entrance and showroom
- Level access to service areas
- Ambulant stair access.
- DDA compliant WC
- Provision for a future DDA Lift

6.1 MASSING & LAYOUT



Proposed Site Layout - Massing

KEY POINTS:

- The proposed extension aligns with the extant footprint and sets back from both the frontage and rear where it extends further to the western boundary.
- The roof is pitched over the extant portion and flat over the further extension thereby providing a consistent elevation parapet around the proposed extension.
- The proposed extension is set back to the western portion of the extension to both provide a transitional form and allow for vehicle servicing to the development.

DRAWING KEY

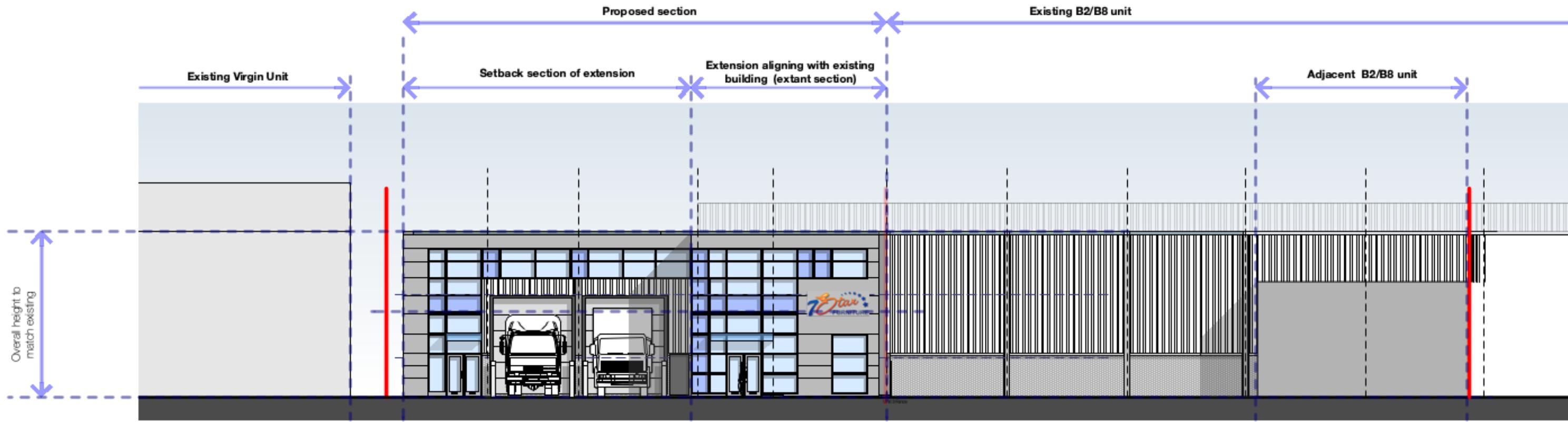


Outline of proposed extension



Footprint of Extant extension

6.2 ELEVATION FORM & MASSING

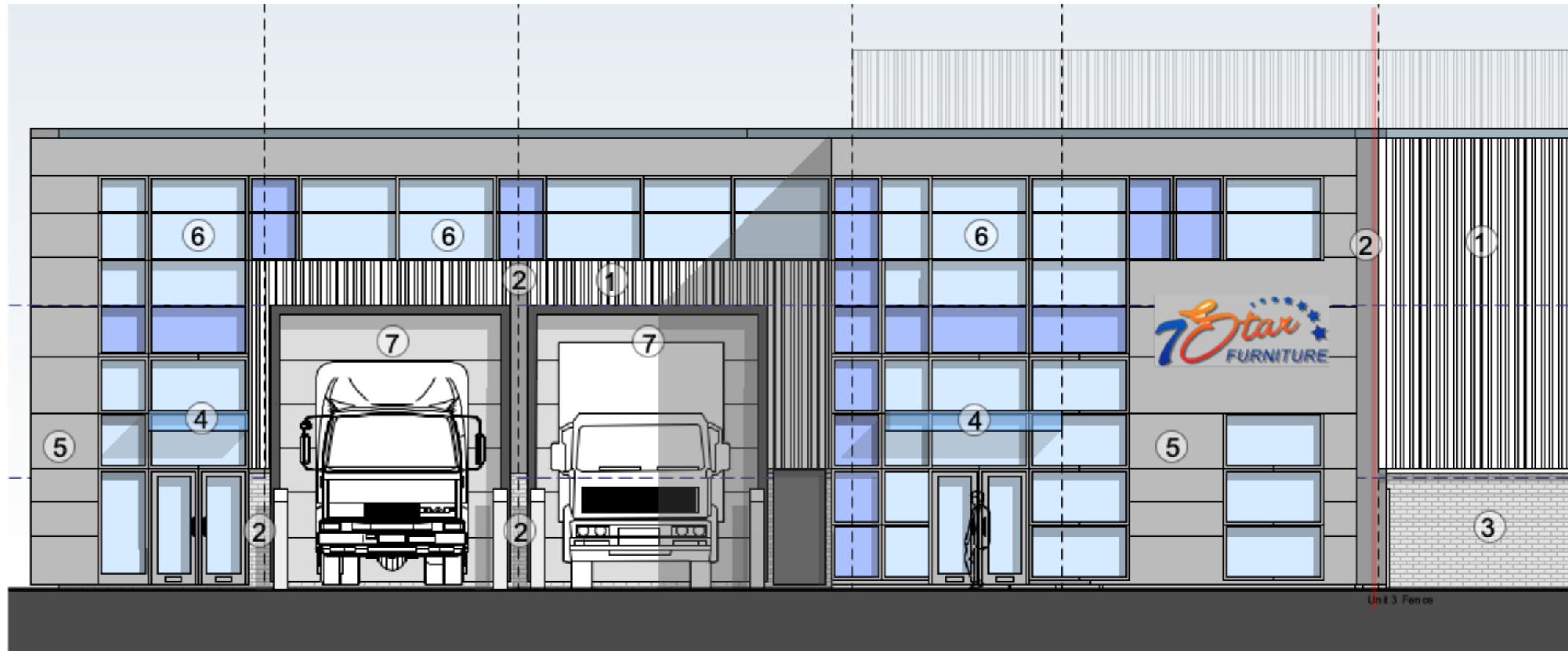


Swallowfield Way - Streetscape Context Massing

KEY POINTS:

- The height of the extension aligns with the existing building along the entire frontage to Swallowfield Way
- The western section of the extension is set back to form a transitional from to the adjacent Virgin unit.
- The proposed glazing defines the primary entrance points as well as provides natural light into the warehouse areas above the service loading areas of the extension
- The form of the development is both sympathetic to the existing building and context along Swallowfield Way.

6.3 MATERIALS



Proposed Elevation - Swallowfield Way

Proposed Material Selection

- Where relevant the existing light and grey profile cladding is to match the existing building.
- The lower portion of the building to accommodate brickwork panels to the side and rear to match existing.
- New Microrib flat panel cladding to the primary elevation around the glazing and building entrances.
- Curtain wall glazing to the primary elevation and showroom
- Sectional overhead loading doors to the service area



1. Existing light cladding
2. Existing grey cladding
3. Existing brickwork

5. Microrib cladding

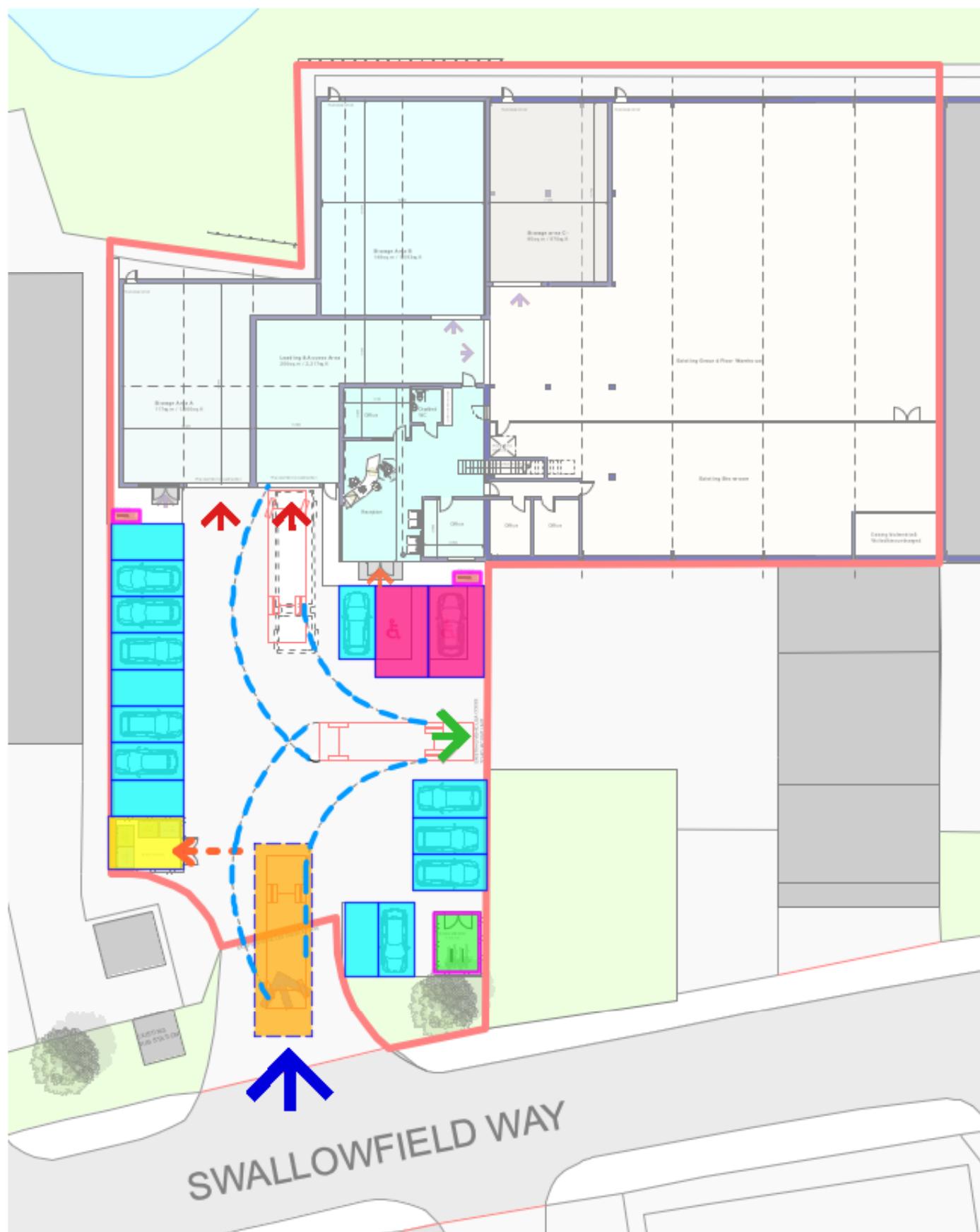


6. Curtian wall glazing



7. Sectional overhead loading door

7.1 VEHICULAR & CYCLE PARKING



PROPOSED SITE PLAN - Transport & Servicing

Access

2 no. Disabled car parking space have been provided close to the main entrance to the building.

The primary pedestrian and vehicle access is located as existing off Swallowfield Way.

Cycle Parking Spaces

4x Secure cycle spaces with provision for loading / unloading , Charging points for vehicles and cycles are provided. 4 Short term cycle spaces have been provided.

Refuse Collection

Refuse/ recycling store meet the Council waste storage requirements and BS 5906 :1980. The Bin stores are conveniently located for collection from the site access of Swallowfield Way.

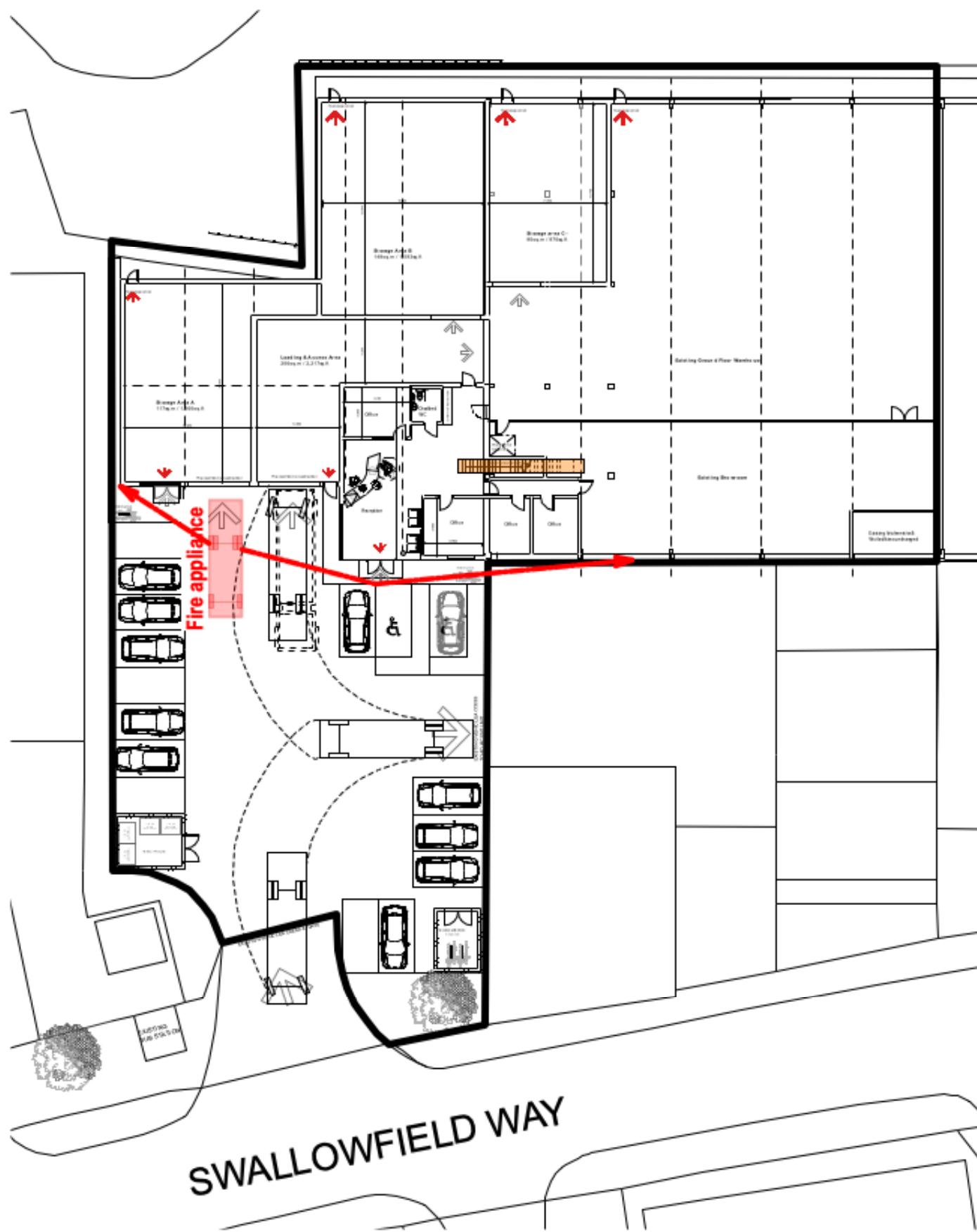
Service Loading

2 new service loading doors are provided with adequate turning and access on site. Service access to the adjacent site is maintained.

DRAWING KEY:

- Existing vehicle & pedestrian access from Swallowfield Way
- Secure cycle spaces in excess of the LPA requirements.
- Short stay cycle spaces
- Refuse & Recycle collection area.
- Car parking spaces
- Footpath to refuse collection & cycle store
- Temporary parking for refuse collection vehicle
- Refuse / re-cycle areas for residents
- DDA Compliant car parking space
- Tracking for service vehicle
- Existing access to adjacent site maintained.
- Proposed service loading doors

8.1 FIRE STRATEGY



Building Regulations Compliance

Storage and other non-residential 7(b)) for the storage or deposit of goods or materials

any building not within purpose groups 1 to 6.

Means of warning and escape

B1. The building shall be designed and constructed so that there are appropriate provisions for the early warning of fire, and appropriate means of escape in case of fire from the building to a place of safety outside the building capable of being safely and effectively used at all material times.

Escape routes & Stairs are suitably located, sufficient in number and of adequate capacity.

An electrically operated **fire alarm system** would be provided. In some situations, the alarm should be operated by a fire detection system. The detailed specification would be compatible with the fire strategy for the existing and extension to the **building**.

Fire Fighting

Provisions covering access and facilities for the fire service are to safeguard the health and safety of people in and around the **building**.

Most firefighting is carried out within the **building**. In the Secretary of State's view, requirement B5 is met by achieving all of the following.

- External access enabling fire appliances to be used near the **building**.
- Access into and within the **building** for firefighting personnel to both:
 - search for and rescue people
 - fight fire.
- Provision for internal fire facilities for firefighters to complete their tasks.

The following considerations have been assessed and met within the proposed layouts:

Total floor area = 2000–8000sq.m

Provide vehicle access to: Up to 11m in height 15% of perimeter ✓ compliant

Type of appliance: Pump High reach ✓ compliant

Table 15.2 Typical fire and rescue service vehicle access route specification						
Appliance type	Minimum width of road between kerbs [m]	Minimum width of gateways [m]	Minimum turning circle between kerbs [m]	Minimum turning circle between walls [m]	Minimum clearance height [m]	Minimum carrying capacity [tonnes]
Pump	3.7	3.1	16.8	19.2	3.7	12.5
High reach	3.7	3.1	26.0	29.0	4.0	17.0

NOTES:

1. Fire appliances are not standardised. The building control body may, in consultation with the local fire and rescue service, use other dimensions.

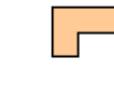
2. The roadbase can be designed to 12.5 tonne capacity. Structures such as bridges should have the full 17-tonne capacity. The weight of high reach appliances is distributed over a number of axles, so infrequent use of a route designed to accommodate 12.5 tonnes should not cause damage.

✓ compliant

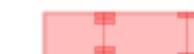
DRAWING KEY:



Escape door & Routes



Escape stairwells



Fire Appliance

9.1 SUMMARY OF THE PROPOSALS

SCHEDULE OF AREAS

Unit 4, Swallowfield Way, Hayes

DRAWING REF: 2329 PL1 Series

Existing Unit					GIA(sq.m)	GIA(sq.ft)
	Ground Floor	Showroom	office	15	858	873
	Mezzanine			14		14
	First Floor	208	48	617	873	9397
Total		208	77	1475	1760	18945

Proposed Extension					GIA(sq.m)	GIA(sq.ft)
	Ground Floor	Showroom	office	101	379	480
Total		0	101	379	480	5167

		Showroom	office	Warehouse	GIA(sq.m)	GIA(sq.ft)
OVERALL TOTAL		208	178	1854	2240	24111

9.2 SUMMARY



The proposed extension both replaces the unsightly temporary storage units on site and provides a high quality building with much needed storage accommodation for this local business to meet its growth objectives.

The proposed extension is visually sympathetic to the surrounding area sits comfortably within the established streetscape.

A large portion of the proposed extension sits within the footprint of the extant consent on the site and extends further to meet the operational needs of the business.

It would provide a high quality commercial accommodation compatible with the DDA and LPA standards;