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## **DESIGN AND ACCESS STATEMENT**

Craufurd Industrial Estate  
Silverdale Road  
Hayes  
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February 2026

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## 1. Introduction

This Design and Access Statement has been prepared by Watkins Gray International LLP on behalf of the Applicant, RGP Holdings Ltd. It relates to the proposed development of two B2-B8 Use Class industrial units on a site located south-east of Station Road in Hayes Town Centre, fronting on to Silverdale Road.

The planning application to which this Statement relates has been prepared following two previous Pre-Application meetings with Hillingdon Council on 18th May 2023 and 23rd April 2025.

The proposed development site (the site) is part of a wider site under the same ownership, which includes Craufurd Industrial Estate. The Site, edged in red (Fig.1), forms the eastern portion of the wider Craufurd Industrial Estate, edged in blue. The Site is already in industrial use and following pre-application advice the subject application covered herein is for temporary planning permission for the erection of two single storey buildings for the continued use of the Site as industrial. The proposals form an extension to Craufurd Industrial Estate.

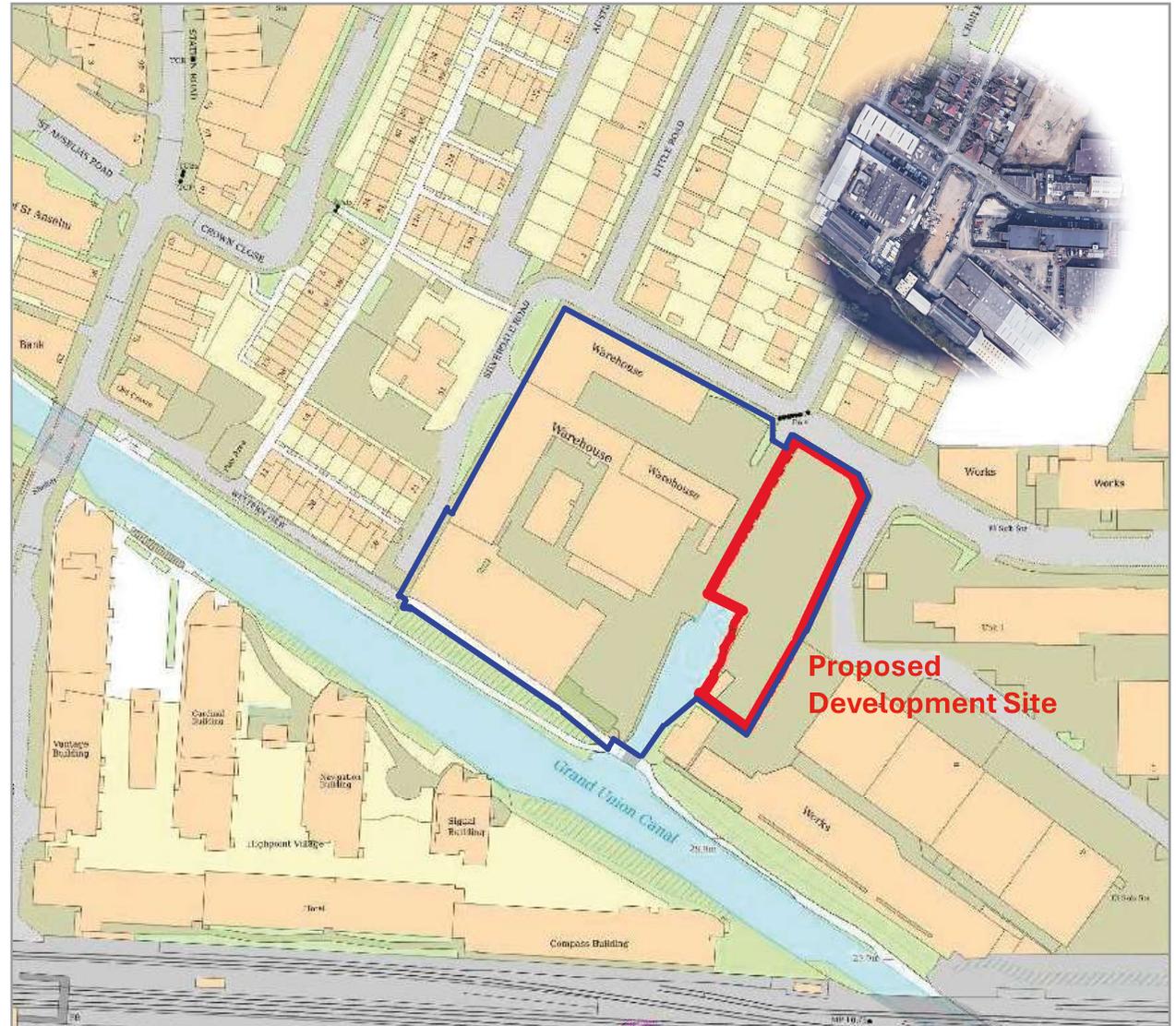


Fig.1 Location Plan

The new buildings subject to this application are proposed to be temporary. This is because the long term vision for the wider site is to bring forward a comprehensive, residential-led, mixed use redevelopment for the entire landholding. This reflects the Local Plan vision for the area and Site Allocation. The work on the residential masterplan has begun and has been subject to early pre-application meetings with the Council. This application seeks to make best use of the Site as an interim use and seeks temporary planning permission for buildings of high quality.

This Statement illustrates the design and sets out the processes of analysis and consultation that have resulted in the development proposal in this planning application. It covers design and access and should be read in conjunction with the following additional documents which have been submitted to support the planning application.

#### Other Application Documents:-

- Planning Application Forms (incl. Certificates, CIL Additional Information Form)
- Architectural Drawings
- Planning Statement
- Ecological Survey and Report/Biodiversity Statement
- Daylight/Sunlight Assessment (within DAS)
- Drainage Strategy
- Fire Statement
- Heritage Impact Statement
- Contaminated Land Survey and Report
- Noise and Vibration Impact Assessment
- Transport Statement and Servicing Plan
- Landscape Strategy
- Existing and proposed drawings

## 2.The Development Site

### 2.1 Location

The site is located east of Station Road in Hayes Town Centre, fronting on to Silverdale Road, OS Grid Reference: 51.504101- 041626. The application site is edged red on the location plan (Fig. 2) and forms part of the Applicant's wider land ownership, including Craufurd Industrial Estate, edged blue.

The application site (the site) is approximately 0.23ha in area and it forms part of the general industrial hinterland east of Hayes town centre up to the A312 and as such has been in long-standing commercial/industrial use.

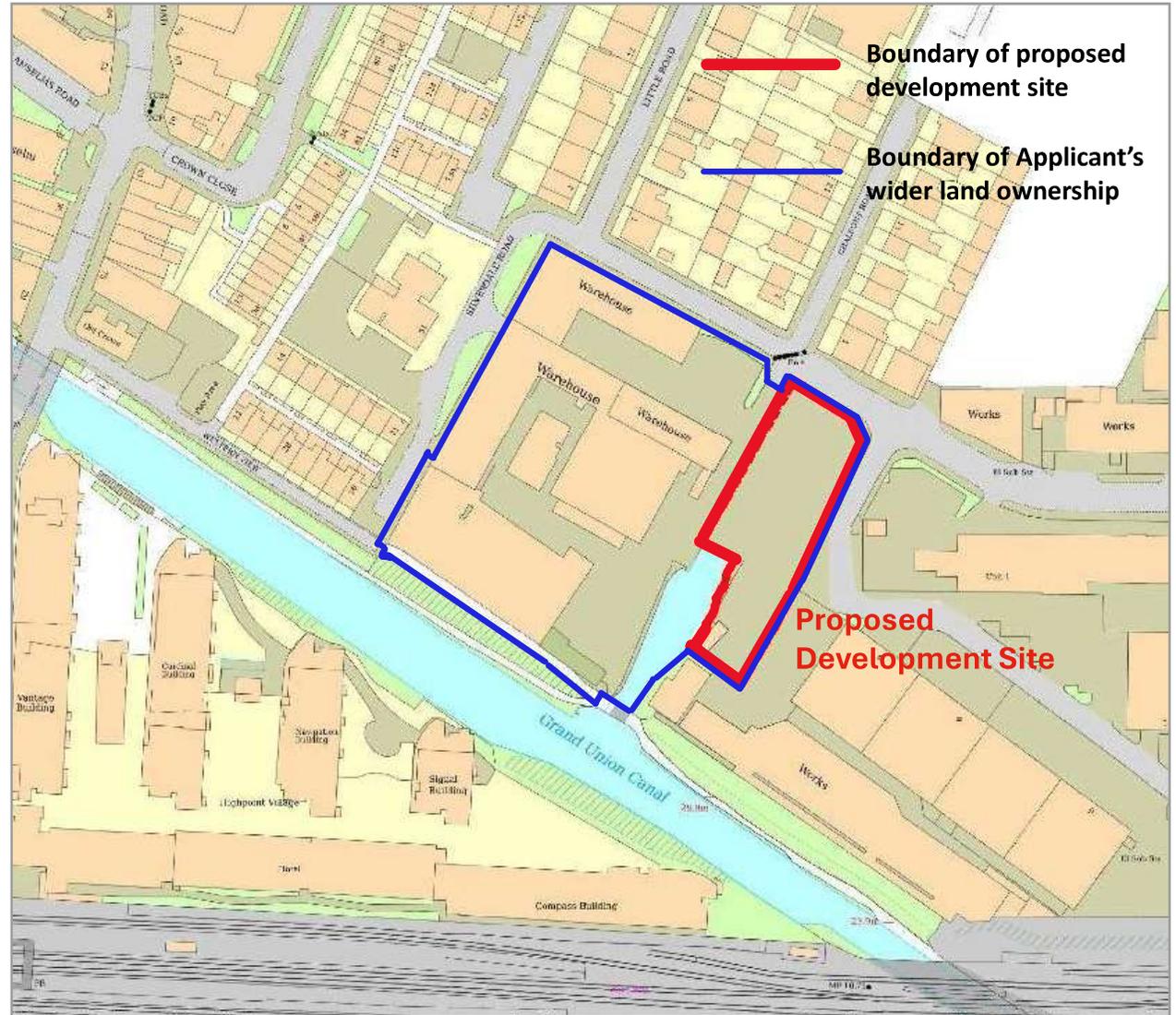


Fig.2 Location Plan and Ownership -

## 2.2 Existing Site and Surroundings

The site has been an industrial site, part of the wider Crauford Industrial Estate, for many years (Fig.3). Its use is therefore B2/B8.

It is accessed from a secondary service road (no name) which is in turn served by Silverdale Road on the northern boundary of the site.

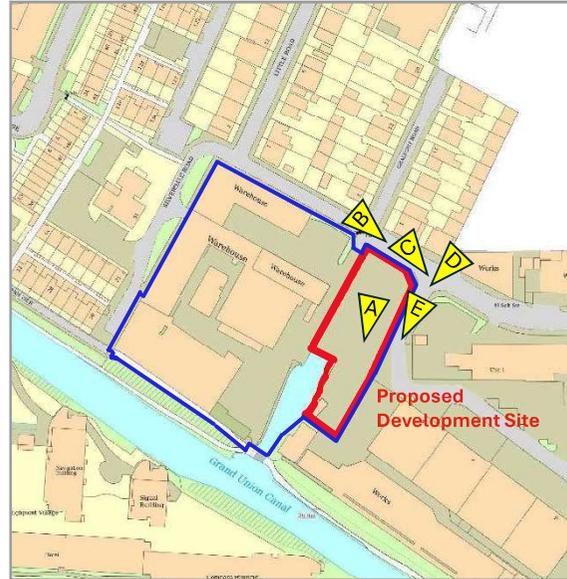


Fig.3 Existing Vehicle Movements - Photo Views



Fig. 3A Existing Vehicle Movements



Fig. 3B Existing Vehicle Movements



Fig. 3E Existing Vehicle Movements



Fig. 3D Existing Vehicle Movements



Fig. 3C Existing Vehicle Movements

The site is enclosed by a range of ad hoc fences, walls and retaining structures (Fig.4) including the boundary along Shackles Dock which adjoins the south-west corner of the site (Fig.5). It contains a range of hard standings, loading bays, containers including those containing accommodation, various items of plant/equipment and a number of open materials storage areas. There is therefore an opportunity to regularise and improve the existing use and operation.

Further detail is provided in “Planning Context” and the separate Planning Statement by DP9.

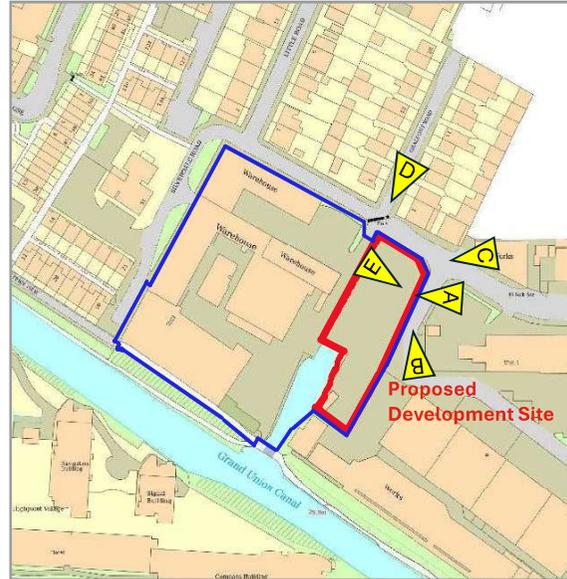


Fig.4 Existing Structures and Boundaries - Photo Views



Fig. 4A Existing Structures and Boundaries



Fig. 4B Existing Structures and Boundaries



Fig. 4E Existing Structures and Boundaries



Fig. 4D Existing Structures and Boundaries



Fig. 4C Existing Structures and Boundaries

Shackles Dock ① is an historic inlet from the Grand Union Canal which is located to the south of the subject site and is currently disused with refuse and overgrown plants particularly towards the dock entrance and the Grand Union Canal (Fig.5). Whilst the site adjoins Shackles Dock it has no direct access to the nearby Grand Union Canal; a cash and carry outlet is situated directly to the south of the site which fronts on to the Canal (Fig.5B and 5C).

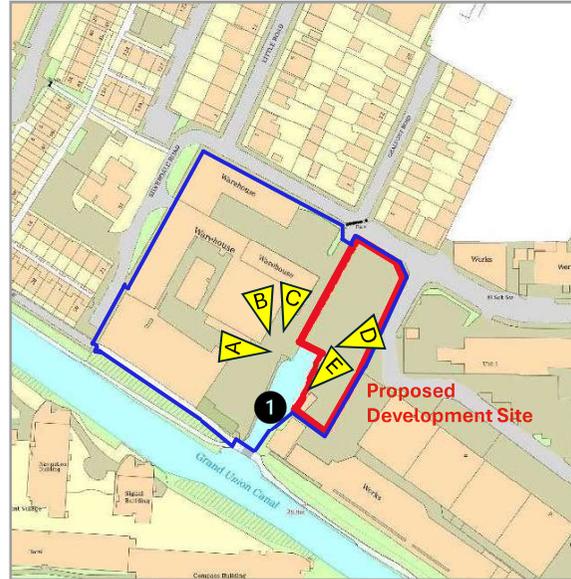


Fig.5 Existing Structures and Boundaries - Photo Views



Fig.5A Shackles Dock



Fig.5B Shackles Dock



Fig.5E Shackles Dock



Fig.5D Shackles Dock



Fig.5C Shackles Dock

In terms of the immediate surrounding area (Fig.6), there are a wide variety of buildings with little cohesion in terms of built form, age, character and identity. Nearby the subject site is the Grade II Listed Benlow Works ②, the residential streets of Little Road and Chalfont Road ③, and the residential development, Chailey Place ④.

Immediately adjacent to the north-west there are a mixture of single and two storey industrial buildings on the current Craufurd Industrial Estate ⑤, to which this development site will be an extension (Fig.6A,B)

To the east and south-east are single to four storey industrial buildings ⑥ including the Benlow Works (Fig.6C,D).

To the north of the site, on the other side of Silverdale Road are a mixture of the two storey, typical interwar, semi-detached houses with gardens ③ (Fig.6E,F), along with single and two storey industrial and office buildings ⑦ (Fig.6G).

Further to the west of the site there is an existing housing estate ⑧ comprising a tower of some 17 storeys nearest the site and which is understood to form part of the latter stages of the Hayes Town Centre (HTC) redevelopment. Planning permission has been granted for the redevelopment of the estate to erect new residential buildings of three, four and fifteen storeys in height. Beyond this estate to the west is Hayes town centre comprising Station Road and Botwell Lane ⑨.

Further to the north of the site is the Chailey Place development ④ (constructed around 2021) consisting of buildings of between one and ten storeys (Fig.6I).

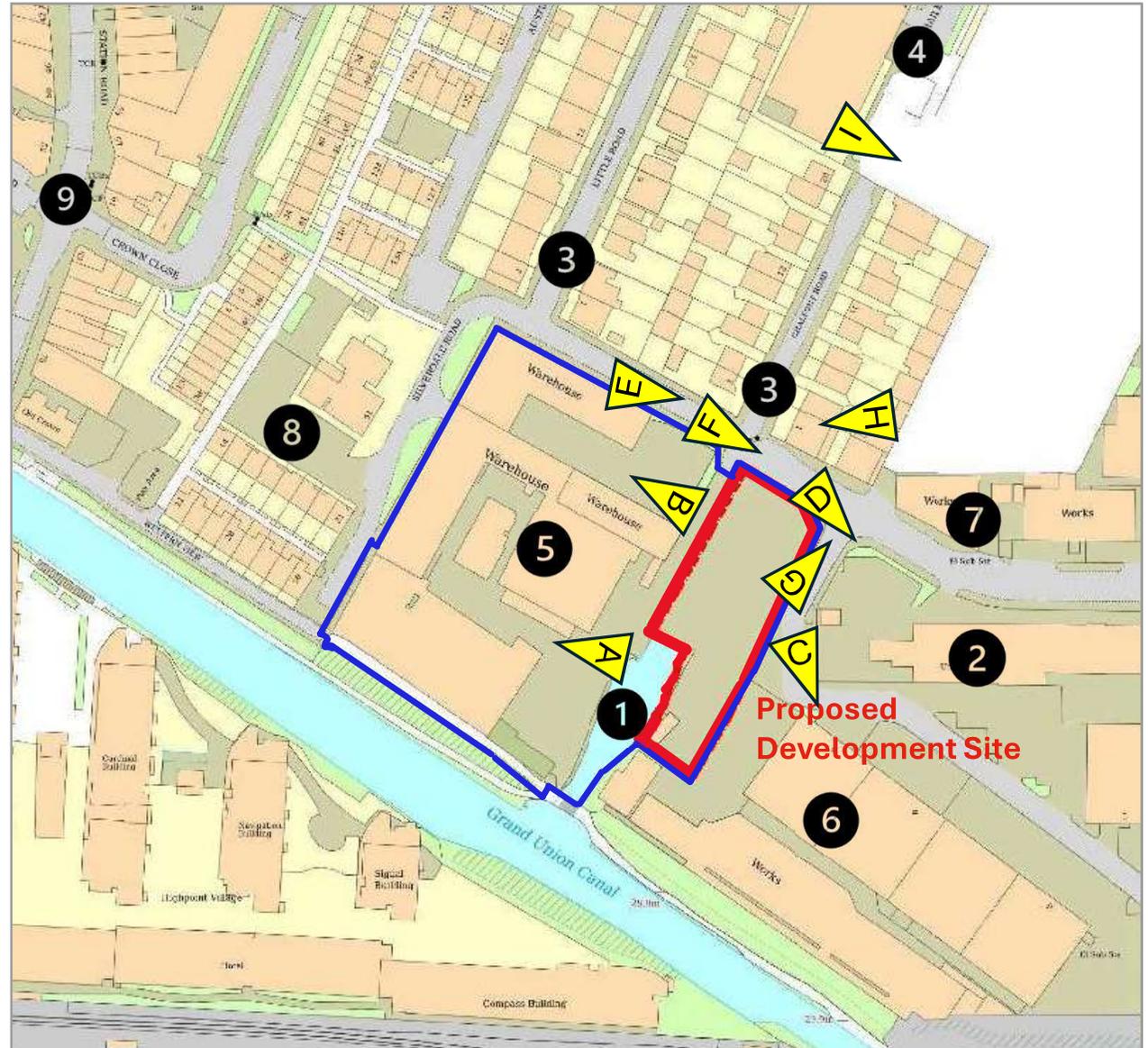


Fig.6 Immediate Surrounding Area



Fig.6A Craufurd Industrial Estate to north-west



Fig.6B Craufurd Industrial Estate to north-west



Fig.6C Buildings to east and south-east



Fig.6D Benlow Works to east



Fig.6E Residential to north



Fig.6F Residential, industrial to north



Fig.6G Industrial to north



Fig.6H HTC Estate to west



Fig.6I Chailey Place to north

## 2.3 Transport and Connections

Vehicular access to and from the site is provided via Silverdale Road which connects to the A312 (The Parkway), via Pump Lane and Bilton Way, and with that, direct access to and from the M4 to the south (approximately 2km from the site), the A4020 (Uxbridge Road) to the north (approximately 2km from the site) and the A40 to the north (approximately 6km from the site).

These vehicular routes connecting Silverdale Road to the wider road network also serve the Silverdale Industrial Estate to the east of the Site.

Cycle access to the site is via the local and well-connected road network as well as a segregated cycle route on Station Road (Fig.7A). The terrain is generally flat and therefore not considered to be an impediment to cycling around the vicinity of the site.

Whilst there is a short, direct pedestrian link to Station Road, via Silverdale Road and Crown Close, it is via a small, visually unsupervised footpath through the HTC estate. Alternatively, there are longer and more overlooked pedestrian links to Station Road and Botwell Lane via Western View to the south and Pump Lane to the north (Fig 7A).

Station Road is well served by the local bus network (with 9 services - Fig7B) and leads to Hayes town centre and Hayes and Harlington Station, approximately 500m from the Site, where there are both Great Western Railway and TfL (Elizabeth Line) services. The station also has stops for both local bus services and National Express bus services.

The site is also very close to the Grand Union Canal which is a key artery in the connections between the country wide canal network and London.

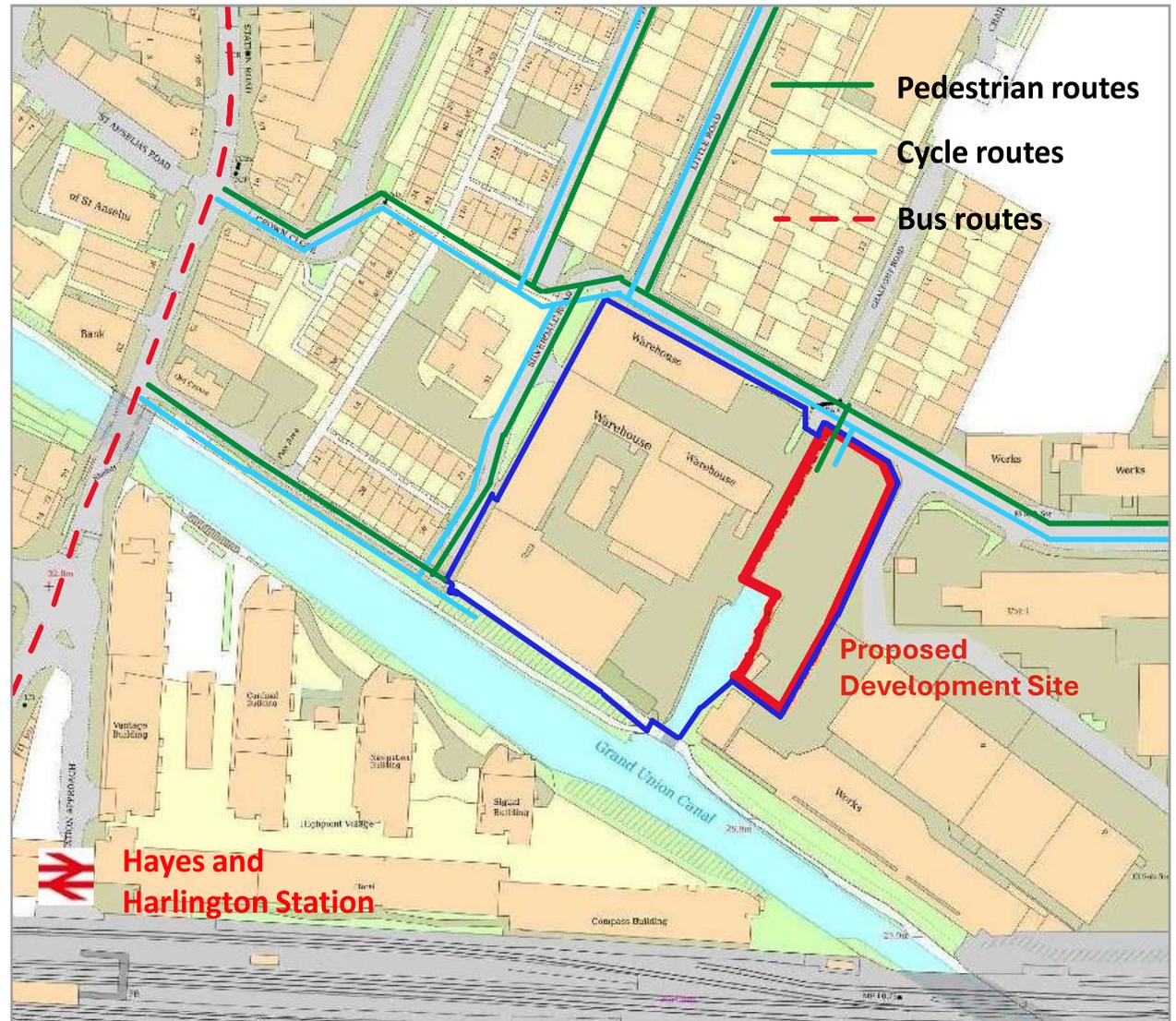


Fig.7A Transport, Connections and Amenities

Shackles Dock, which is outside the subject site, has no direct pedestrian access either from the site or with the Grand Union Canal Walk.

Further details on transport and connections are provided in the Transport Statement and Servicing Plan.

## 2.4 Local Amenities

The site benefits from easy links with local facilities including a Post Office, banks, chemists, cafes, restaurants and general groceries shopping on or around Station Road and Botwell Lane. Just north of Botwell Lane is Botwell Green Leisure Centre and Library, with associated external areas, as well as the Council's 'One Stop Shop' for Hayes and Botwell Green.

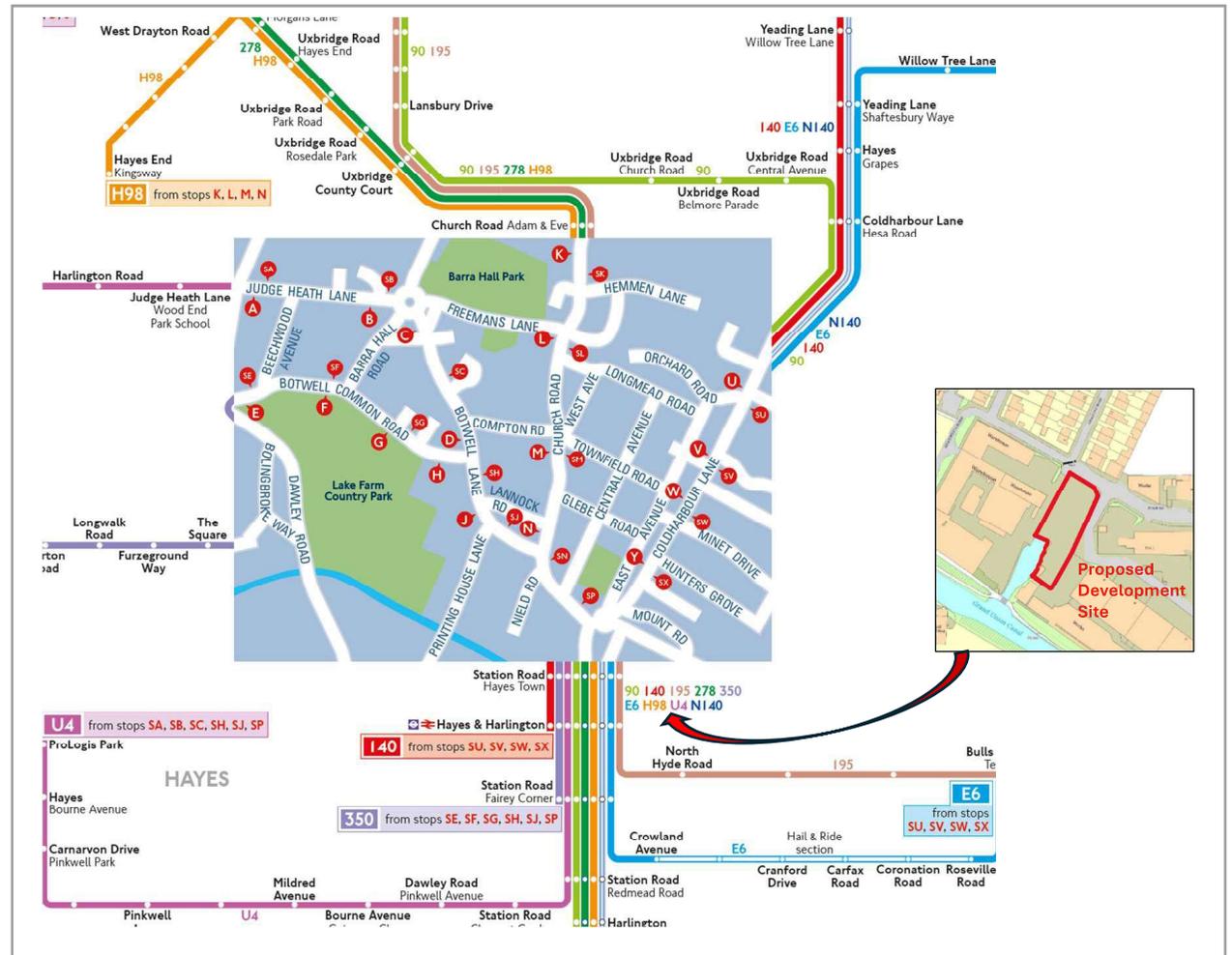


Fig. 7B Local bus services

## 2.5 Historical Context

After agriculture, brick making was the first industry to appear in Hayes with much of the land surrounding the Grand Union Canal used for brick earth extraction (brick fields).

Initial development around the site took place from around 1865 with the site used as both agricultural land and brick fields. Whilst early expansion was slow Shackles Dock ① was subsequently extended northwards from the Grand Union Canal to provide an additional wharf to serve the X-Chain Patents Company as well as servicing other industrial facilities such as the Aeolian Pianola Piano Factory.

Housing started to be built around the industrial facilities in the early nineteenth century and by 1935 the housing north-west of Silverdale Road ③ was largely as it is today.

The various industrial facilities continued to expand until the 1960s when several industries within Hayes closed. At the same time the decline of the use of waterways in the transportation of goods in turn caused the decline of Shackle's Dock and by the 1970s the extension of the dock was almost entirely filled in. Today it remains unused and overgrown.

The Benlow Works ② (Fig.8,9), a Grade II Listed Building, was originally the Orchestrelle Factory, built between 1909 and 1911 and operated by the American Aeolian Company which was known as the Orchestrelle Company in the UK. Over the years the building has been used by a variety of companies including the 'X' Chair Patent Company. It now lies derelict and is on Historic England's Heritage at Risk Register.

Further detail is provided in the separate Heritage Statement by Gareth Jones Heritage Planning.

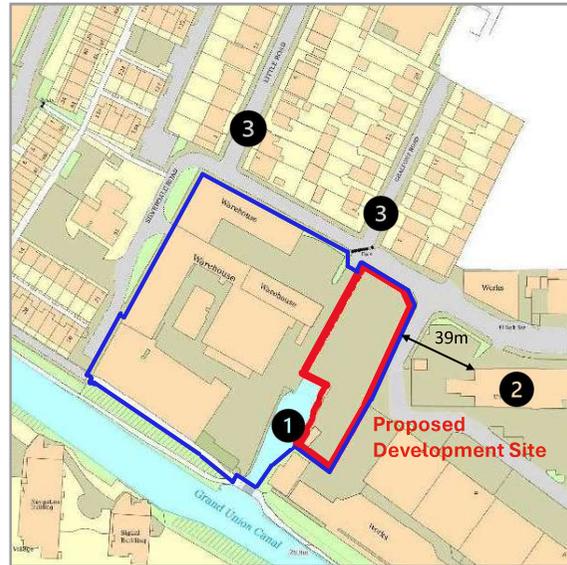


Fig.8 Historical context and location of Benlow Works



Fig.9 Benlow Works looking east

## 2.6 Planning Context

There have been a number of planning applications at the Site, however, many of these applications are not considered relevant in the context of the emerging proposals for the Site.

Based on the available planning applications and our understanding of the Site as part of the wider Crauford Industrial Estate, it is considered that the current use of the Site comprises of general industrial and storage uses (Class B2 and B8)

Further detail is provided in the separate Planning Statement by DP9.

### 3.The Initial Brief and Design Proposal

As set out herein, the subject site adjoins the existing Crauford Industrial Estate which, being fully let, is a thriving hub of local industry.

Given the existing uses of the subject site the proposals as set out herein will deliver a high quality more intensive use, complementary to the established adjoining estate but will yet reduce noise and dust pollution.

The brief was to make best use of this industrial land for the interim period. The existing use is to be regularised, improved and controlled. This will create a more neighbourly industrial use.

The initial brief was to develop a design proposal for a larger site (Fig.10) and to combine this with the eastern part of the site which is now the subject of this planning application. This brief sought to replace the existing industrial building on part of the larger site and to erect additional industrial buildings. The whole development proposal would provide additional facilities to an existing industrial estate in advance of a later resi-led development.

The brief then evolved following early pre-application engagement and the subsequent refurbishment of an existing building on the Estate. The scope of this application now just relates to the open-air works part of the site, which can be improved and regularised in the short-medium term, with the omission of a building on the Crauford Estate that has been subsequently refurbished.

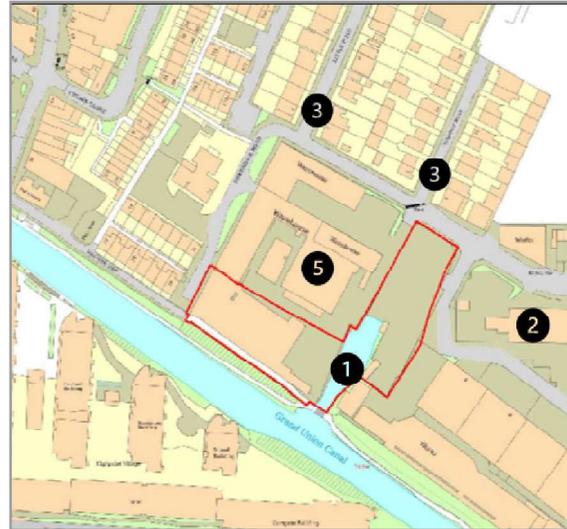


Fig.10 Initial Site

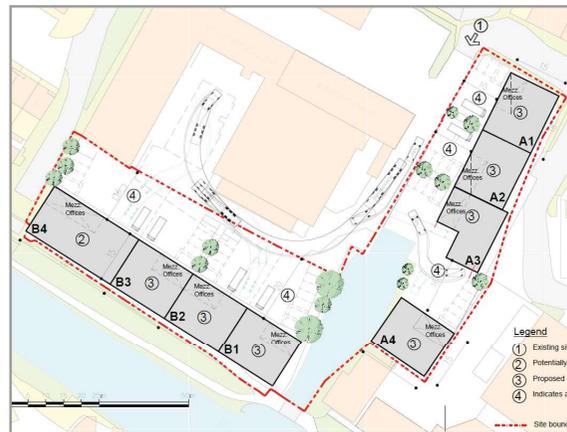


Fig.11 Initial Proposal

### 4. Scheme Evolution

The scheme has evolved following a series of pre-application meetings with the Council. The original proposal included the wider Site as shown below (Fig.11) and included the erection of 3 buildings and the creation of 2,187 sqm of industrial floorspace. This proposal was subject to a pre-application meeting with the Council on 18th May 2023.

## 5.0 Pre-Application Advice

### 5.1 August 2023

Following the Pre-Application submission on 23rd January 2023 a meeting was held with Hillingdon Council on 18th May 2023 to review the consultations and initial responses, and this was then followed by the Council's formal Pre-Application Advice in a letter dated 22nd August 2023.

In summary the key feedback from the Pre-Application submission was as follows:-

- The massing of the single storey building was accepted
- It should be explored whether the water inlet (Shackles Dock) with its heritage and nature conservation potential could be integrated into the scheme as a placemaking feature
- Consider aligning the back building line to the adjacent site (the Benlow Works) access road to the south-east of the site
- The materiality of the proposed buildings should respond positively to the Grade II Listed Building, the Benlow Works, to create a positive interface between them
- Details of pedestrian and cyclist access, and cycle parking to be provided to deliver a safe environment for both pedestrians and cyclists
- The principle of the proposed development as commercial use within the same land use, with a slightly lower quantum of floorspace than existing is accepted
- There will need to be demountable structures to satisfy any 'meanwhile' consent

Following this pre-application meeting, the scope and brief evolved to just include the open-air works

part of the Site. The team reviewed the feedback and sought to explore, test and incorporate suggestions where possible. As a result of the changes a second pre-application was considered necessary and it was submitted to Hillingdon Council to discuss the revised and reduced proposal.

### 5.2 June/July 2025

The revised design proposal submitted in the second Pre-Application (Fig.12, 13) on 19th March 2025 addressed the key feedback from the first Pre-Application with the following changes and improvements to the design:-

- Reduced extent of development site that only focuses on improving and regularising the open-air works.
- Fewer new industrial units
- Temporary Consent
- Revisions to create a consistent alignment of the back building line in direct response to feedback
- Creation of a separate cyclist and pedestrian entrance
- Improved amenity, placemaking, planting and nature conservation around the water inlet in direct response to feedback received.
- Further evidence and assessment of the reduced proposal in relation to surrounding residential properties to demonstrate there will be no overlooking or loss of privacy
- Improved visual amenity for nearby residential occupants

- Further evidence to demonstrate acceptable daylight, sunlight and overshadowing levels will be maintained to surrounding residential properties (i.e. falls within BRE guidance)
- Revised elevations and materiality approach which responds positively to the nearby listed building
- Safe and secure pedestrian cyclist entrance with direct access to local routes

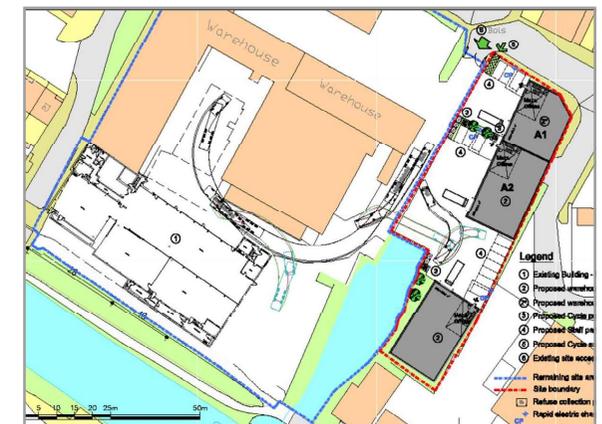


Fig.12 March Pre-App Proposal - Plan

Following the Pre-Application submission a meeting was held with Hillingdon Council on 23rd April 2025 which discussed the revised development proposal. The interim nature of the Site was also discussed in relation to the long-term vision for a residential-led development on the Site, which was subject to a separate pre-application meeting. This constructive meeting was then followed by written feedback on 4th June 2025 and further email advice from Hillingdon Council in an email dated 11th July 2025.

Key feedback from the meeting focused on the number of buildings being created and better understanding how different uses (vehicle, cyclists, pedestrians) would access the Site. In response, a further revision was made to the proposal to reduce the number of industrial units to two.

Formal written feedback was received in July 2025. This is summarised below:-

- The interim industrial use was accepted and the benefits of regularising the use and controlling units impacts through the creation of new buildings was welcomed.
- The approach to architecture and materiality was considered appropriate.
- Acknowledged and welcomed the proposed set back, but questioned if any way to increase setback or buffer or show amenity impacts are acceptable.
- Questioned if further landscape is possible and also asked to demonstrate there is sufficient room for vehicles and workers to move around.
- Separate routes for pedestrians and cyclists for safety and security need to be demonstrated.

- Insufficient space around Shackles Dock for placemaking
- Encouraged to explore more placemaking opportunities and more landscaping where possible. Need to include an Urban Greening Factory assessment.
- Noise impact assessment required with the application
- Details required on the demountability of the buildings and their removal from site



Fig.13A March Pre-App Proposal - 3D Views

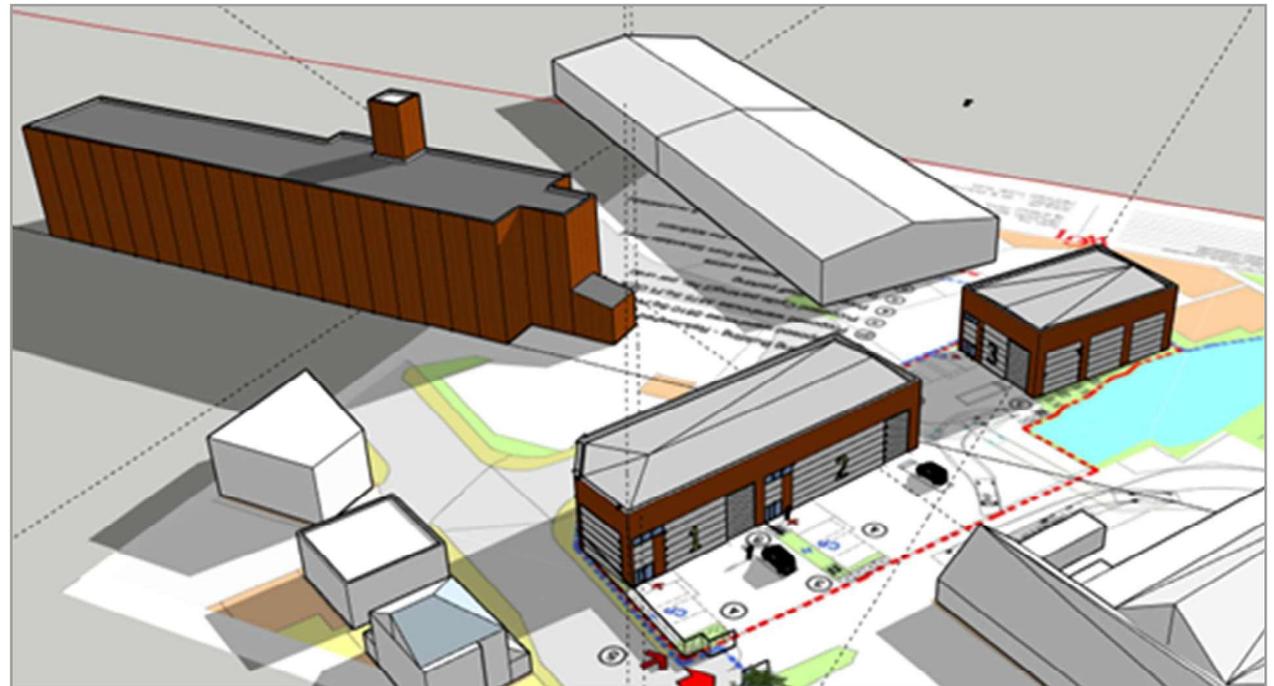


Fig.13B March Pre-App Proposal - 3D Views

## 6. The Design

The proposal has evolved and been refined further following this comprehensive pre-application process. This has informed the full planning application.

### 6.1 The Site

The Site subject to this planning application is edged in red on the location plan (Fig.14). As outlined earlier this is currently used for open storage/ industrial uses.

The site of the existing Craufurd Industrial Estate is edged green on the location plan. Both sites are under the ownership of the same party the extent of which is edged blue on the location plan. The wider industrial estate shares an existing, common estate access road.

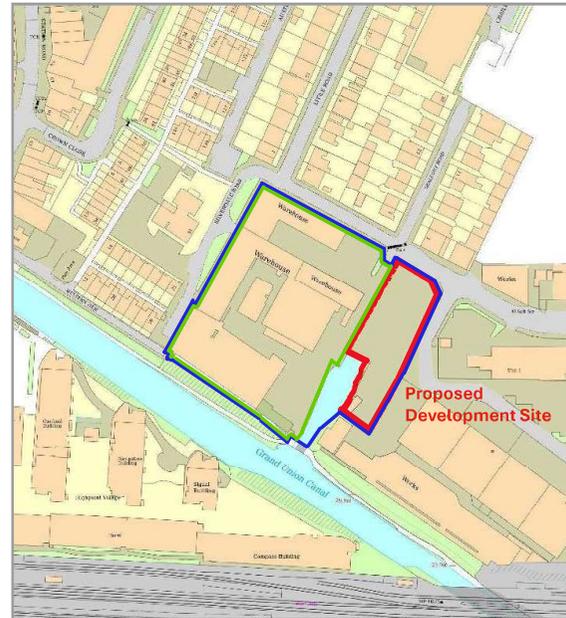


Fig.14 Planning Application Site

### 6.2 The Concept

The approach is to design an efficient and functional site layout with simple, small scale, efficient industrial buildings which would fit into their largely industrial surroundings and yet respect both the residential properties to the north of the site as well as the heritage of the Grade II Listed Benlow Works nearby to the east. There is an opportunity to regularise and improve the existing use and operation of the industrial land and enhance its relationship to the local context all within the context of a temporary building.

In terms of built form the development also responds to the existing road layout around the site in creating street frontage, as well as the (unused) Shackles Dock water inlet to the south-west of the site. Furthermore, in providing a shared access road with the existing Craufurd Industrial Estate, it gave the opportunity to improve and reinforce the entrance to Craufurd Industrial Estate from Silverdale Road. Furthermore, in changing from the existing access on the eastern boundary of the site the consolidation of the access and vehicular movements to and from the wider site will make the environment for pedestrians and cyclists on Silverdale Road safer. To reinforce this improvement a separate access point for to the site pedestrians and cyclists has been included in the proposal.

In developing the design with this conceptual approach, we have also responded to and incorporated feedback from the Council from the pre-application process to enhance and refine the scheme, albeit within the context of a temporary planning consent.

### 6.3 The Layout

The two proposed B2/B8 class buildings, Units A1 and A2, are positioned to align with the service road and service yard to the east of the site ①, and Silverdale Road to the north ② giving two clear frontages parallel to those boundaries (Fig.15). The western frontage to Unit A1 faces the parking ③ and service courtyards ④ and includes both the building entrance (for pedestrians) ⑤ and the loading bay ⑥. In Unit A2, the western frontage faces Shackles Dock and includes the building entrance ⑤, and the northern frontage which faces the service courtyard as it extends between the two buildings includes the loading bay ⑥.

By aligning the rectangular shaped buildings and service courtyard in this way, it consolidates the servicing of the extended Craufurd Industrial Estate through one clearly identifiable entrance off Silverdale Road ⑦. The service area ④ is contained within the industrial estate and has minimal impact and visibility to the surrounding area. This allows for safer vehicle movements in and out of the site and within the extended site off the Estate access road ⑧. It also reinforces the identity of the extended Estate with the single vehicular access point off Silverdale road ⑦.

The northern gable frontage on Silverdale Road ⑨ has been moved approximately 1.8m further from 1 Chalfont Road to increase the boundary area for landscaping in response to feedback and to make a more contextually and neighbourly building. Further details are provided in "Amenity for Nearby Residents" and "Daylight/Sunlight Impact", as well as the separate "Landscape Strategy" document.

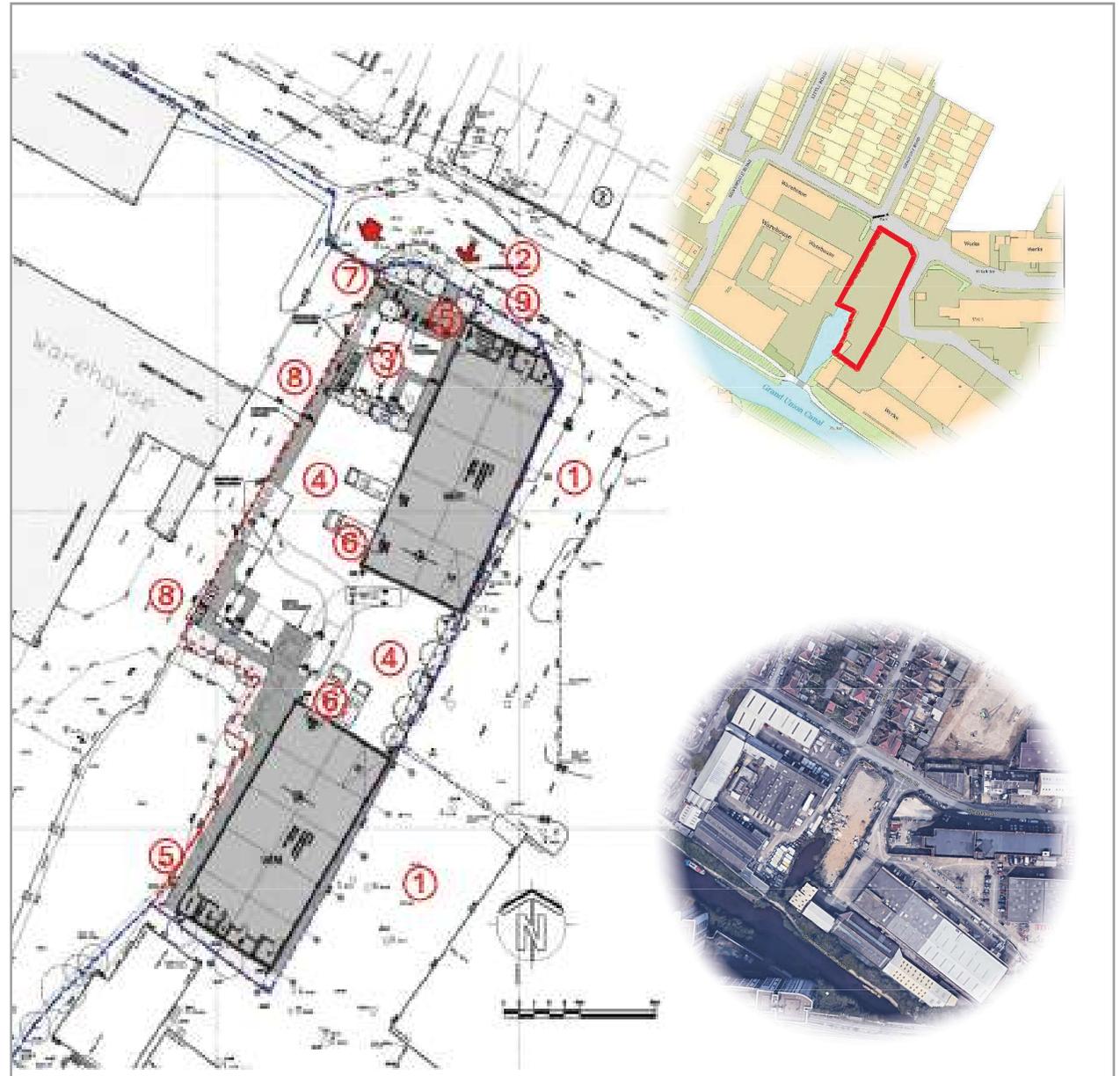


Fig.15A Site Layout (extract from Drg. No. 122016 P1002 P03)

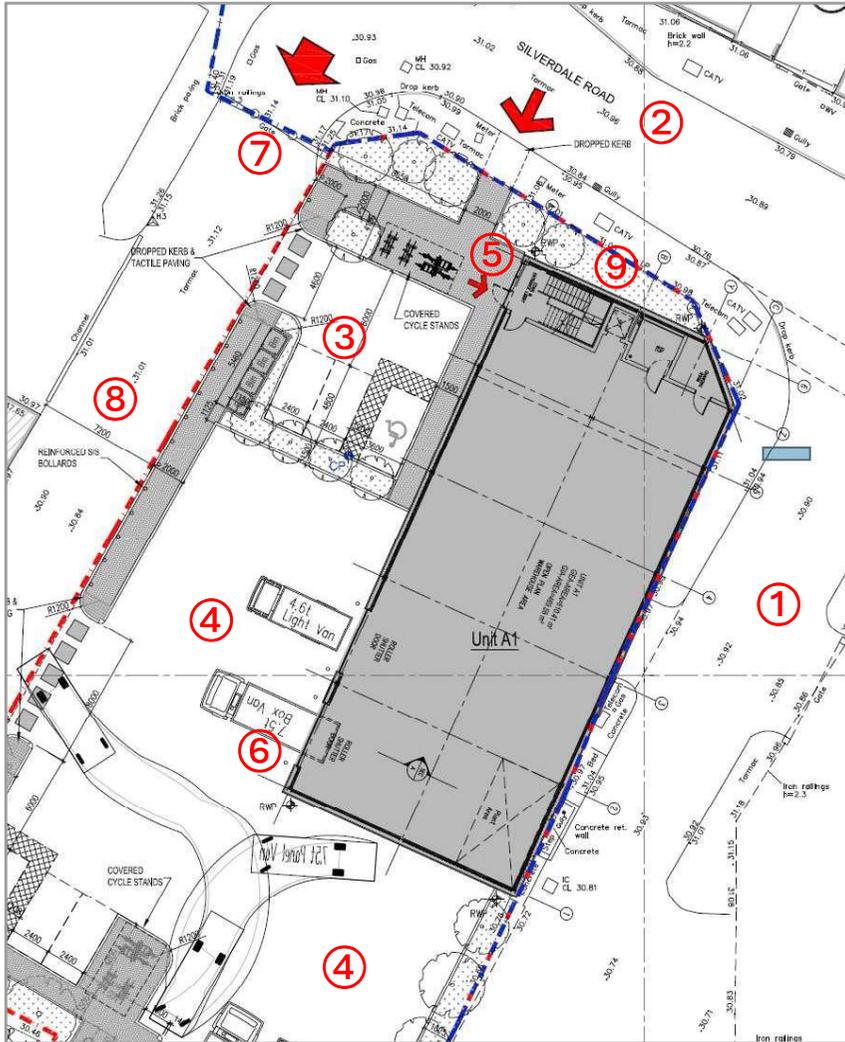


Fig.15B Site Layout (extract from Drg. No. 122016 P1002 P03)

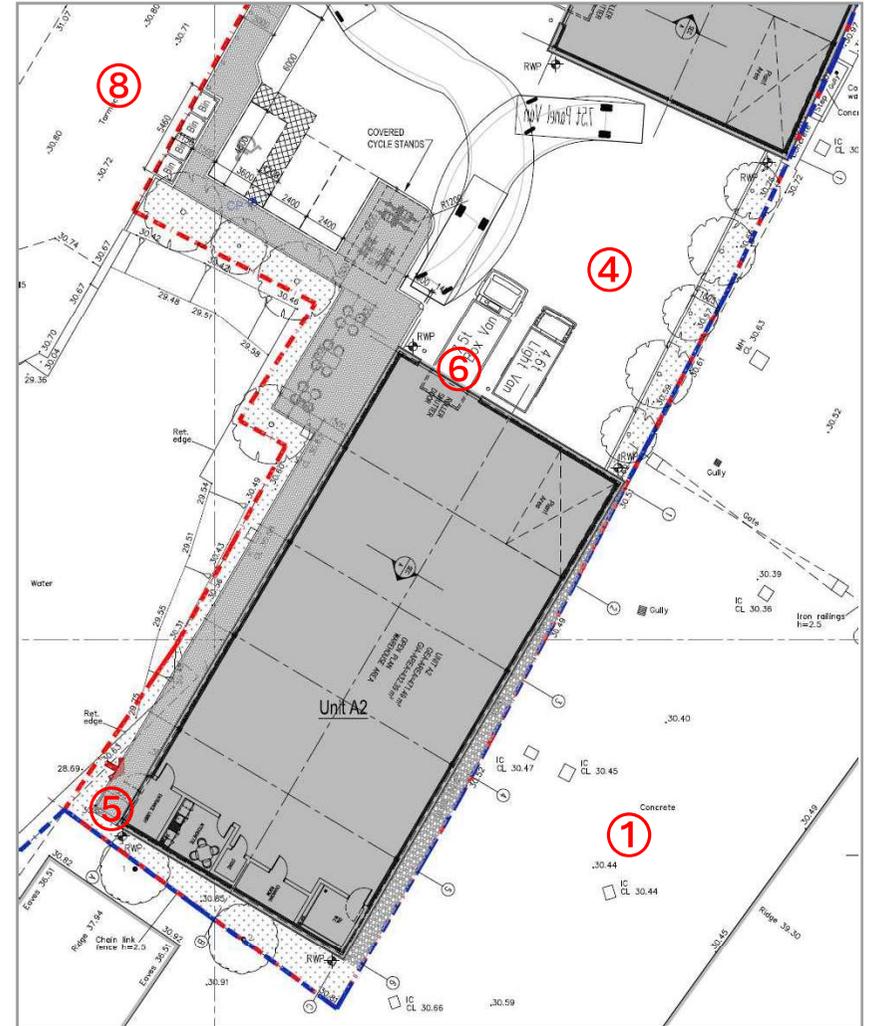


Fig.15C Site Layout (extract from Drg. No. 122016 P1002 P03)

## 6.4 Access and Circulation

The vehicular access from the Estate access road ⑧ to the service courtyard ④ is via an 8m zebra crossing ⑩ to the pedestrian footpath, illustrated with the tracking for a 7.5 ton box van, and further details of access and tracking are given in the Transport Statement. The service courtyard provides parking and direct access to the two loading bays.

In terms of parking, a separate car parking courtyard ③ to Unit A1 is accessed directly from the Estate access road via a 4.6m zebra crossing ⑪ and provides 3 parking spaces including one space designated for use by disabled people and one with a shared electric charge point for 2 cars.

Both car and commercial parking are provided within the service courtyard, with an identical car parking provision to the courtyard to Unit A1.

A separate and secure access gate is provided on Silverdale Road ⑫, near the entrance to Unit A1, for both pedestrians and cyclists. By separating this from the main vehicular access point provides a safer access route to and from the site for both pedestrians and cyclists. Within the site 2m wide footpaths ⑬ provide separate, safe access to parking areas, cycle parking ⑭, refuse stores ⑮ and the entrance to each Unit ⑤.

Sheltered cycle storage exceeding the requirements of the London Plan is provided close to the entrances of both Units with 2 standard Sheffield stands for each Unit. There are also 2 stands for each unit to accommodate larger bikes such as cargo bikes or adapted bikes used by people with mobility difficulties.

Further details on access and circulation are given in the Transport Statement and Servicing Plan.



Fig.16A Access and circulation (extract from Drg. No. 122016 P1002 P03)

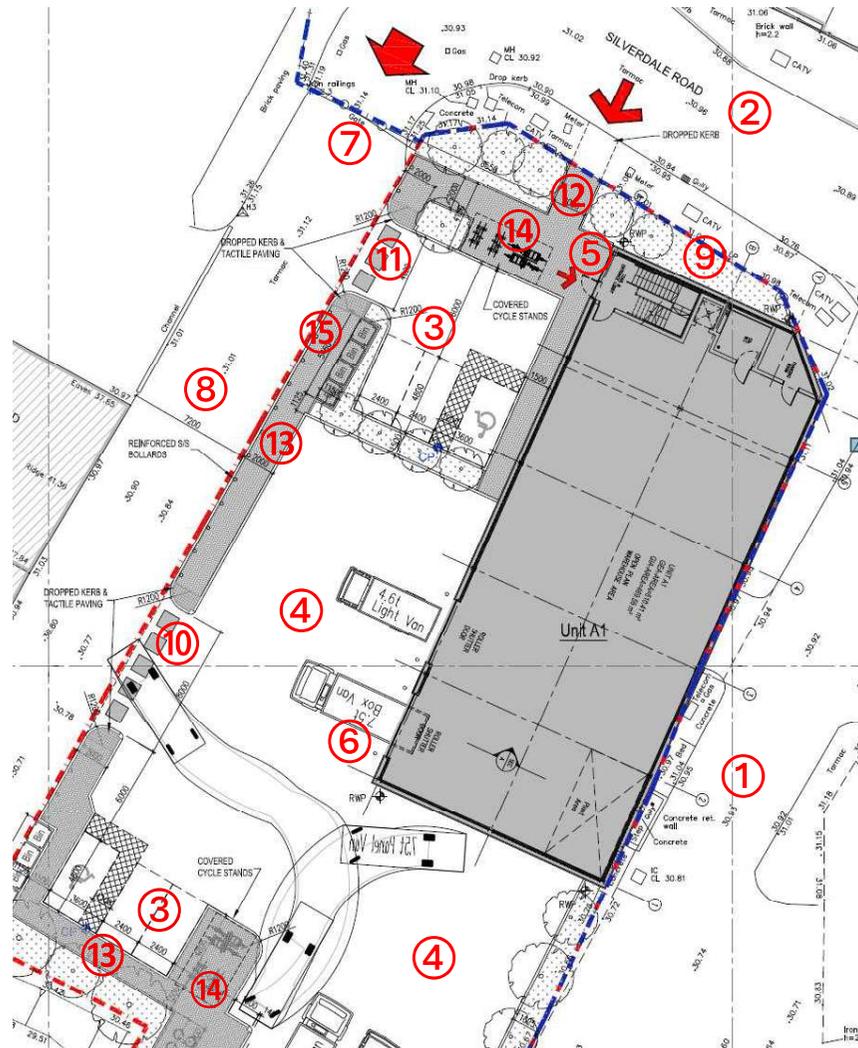


Fig.16B Access and circulation (extract from Drg. No. 122016 P1002 P03)

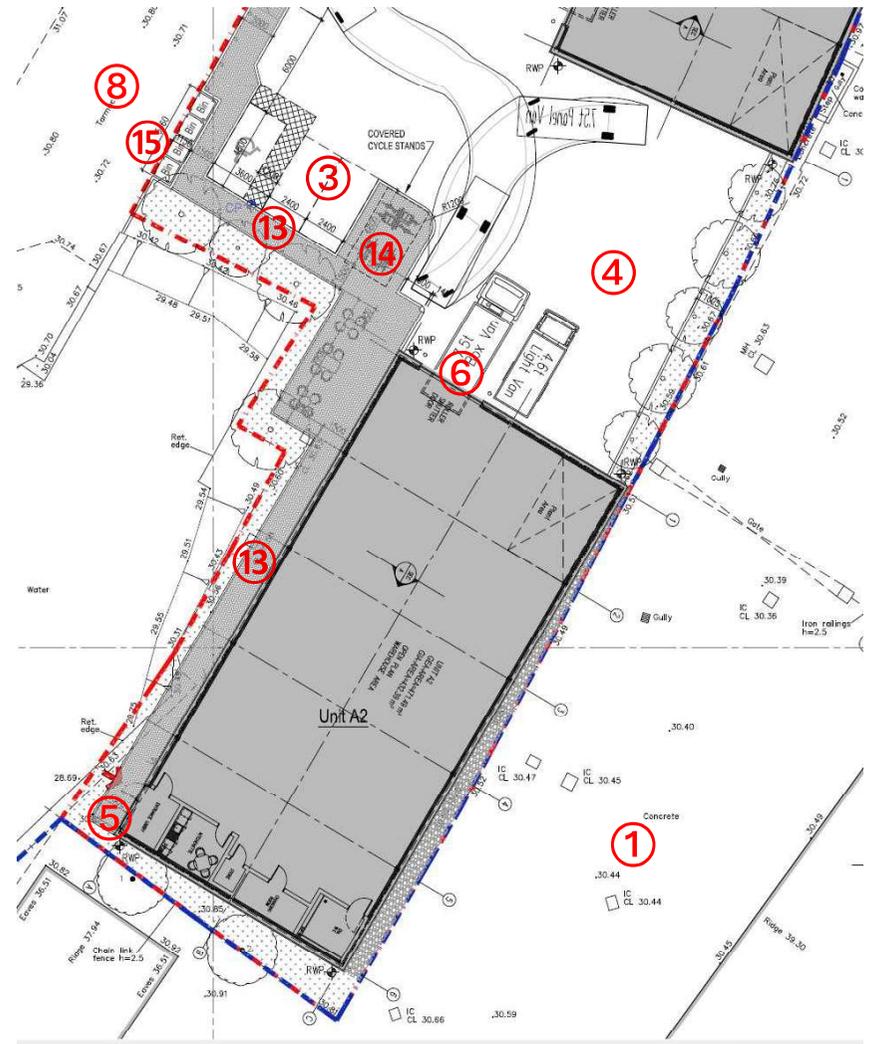


Fig.16C Access and circulation (extract from Drg. No. 122016 P1002 P03)

## 6.5 Other External Areas

Refuse storage is provided for each Unit next to the Estate access road with a 5.46 x 1.05m screened enclosure (4 euro-bins) providing storage for both general waste and recyclables ⑮.

Landscaping is provided in various parts of the site with a strategy to maximise and enhance the soft landscape where possible to provide biodiversity enhancement as well as a visual amenity.

The soft landscaping comprises trees, boundary planting, shrub and herbaceous plants, groundcover plants and climbing plants.

The hardscape has been designed as hardwearing with a mix of impermeable ⑯ and permeable ⑰ surfaces which clearly identify and delineate pedestrian and cycle routes, including zebra crossings ⑩ ⑪, and vehicle circulation and parking areas ③.

The boundaries to the site are formed either by the building enclosures or 2.4m high weld mesh fencing ⑱, and street furniture consists of both Sheffield stands and cargo bike stands ⑭, and bollards along the pavement edge to the Estate access road ⑲. Tables and chairs are provided for staff amenity next to Shackles Dock ⑳.

The existing entrance gates to Craufurd Industrial Estate on Silverdale Road which lead to the Estate access road for the wider site are to be retained ⑦.

The proposals have aimed to maximise the urban greening on site as far as possible with a UGF score of 0.23.

Further details of the landscape proposals are provided in the Landscape Design Report.

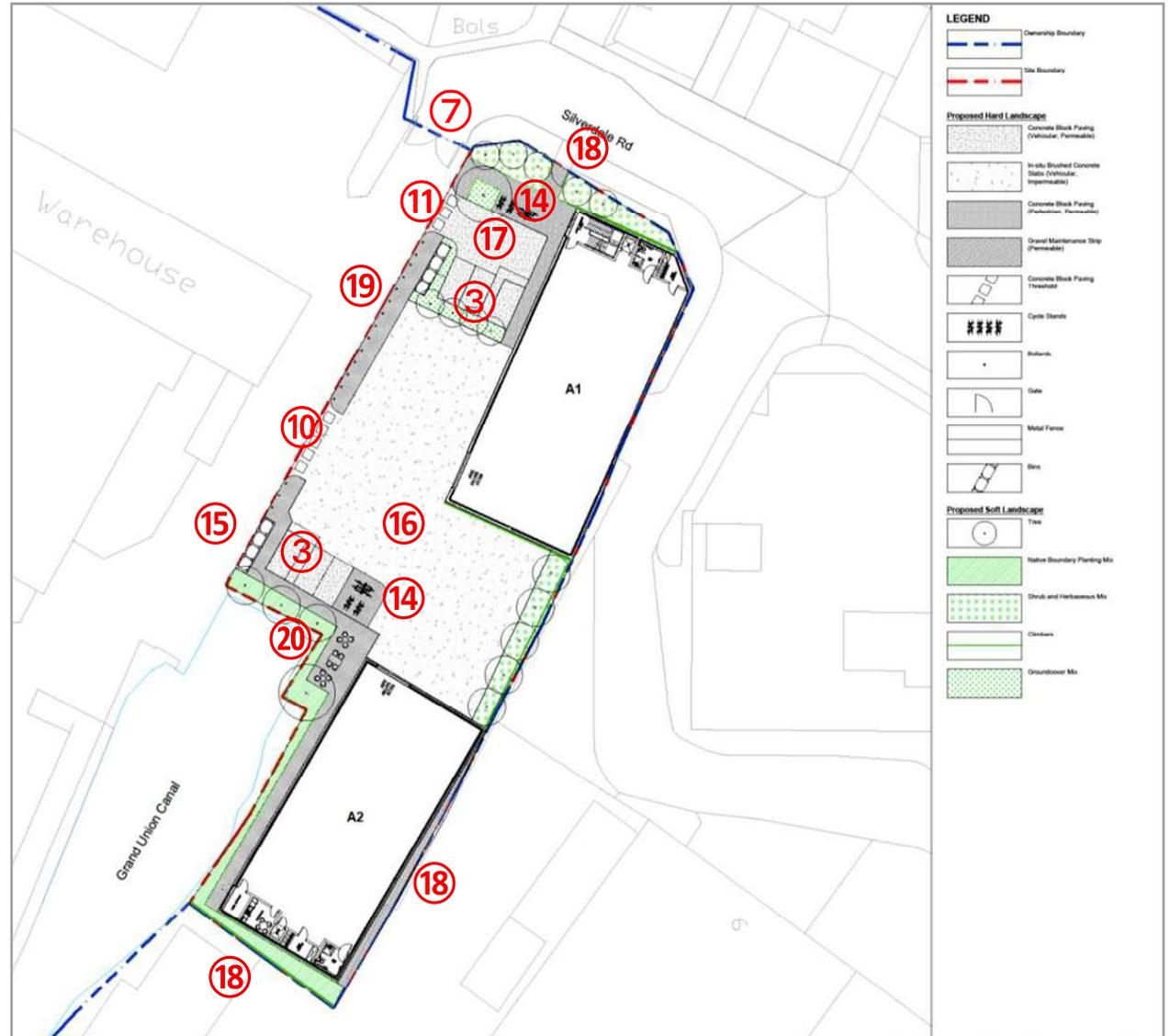


Fig.17 Other External Areas (Extract from Drg. No. LN00943 L-101)

## **6.6 Connectivity Through the Site**

Due to the existing industrial nature of the site and its use with lorry movements, security requirements and interface with the wider Crauford Industrial Estate, it is not considered appropriate for general public access to the site or links across the site. Also, it must be emphasised that there is currently no direct connection with the Grand Union Canal and therefore no established public pedestrian/cycling route between the site and the Grand Union Canal.

## 6.7 Height, Scale and Materiality

The proposed single storey buildings have an approximate internal eaves height of 8m ① and an approximate ridge height of 9m ② and have been designed to meet current industry requirements for this type of B2/B8 facility including 6m high loading bay doors ③ and the provision of a mezzanine area in Unit A1 for office space. As a result, the external parapet is 9.3m above external ground level ④ (Fig 18).

The elevation treatment and proportions (Fig.19) have been developed from the 6m high loading bay doors, creating a vertical pattern which extends across the elevation of each unit and consists of three visual elements:-

- the pedestrian entrance with spandrel panel, side panel and clerestory glazing above ⑤
- the loading bay overhead door with signage panel above ⑥
- the unit number identified with a large wall mounted letter/numeral ⑦

The use of horizontal profile cladding contrasts with the three visual elements on the front elevation, and by providing a visual and enclosing frame ⑧, the horizontal profile cladding reinforces the rhythm generated by the repetition of these elements on each unit as well as the overall scale and mass of the buildings. The combination of these features, along with the grey capping on the top of the parapet adds both articulation and visual interest to the buildings

In terms of historical context (Fig.20), and to respond positively to the Grade II Listed Benlow Works to the east of the site, the horizontal profile cladding is in a “copper beech” ⑨ colour as a visual reference to the Grade II Listed Benlow Works to the east of

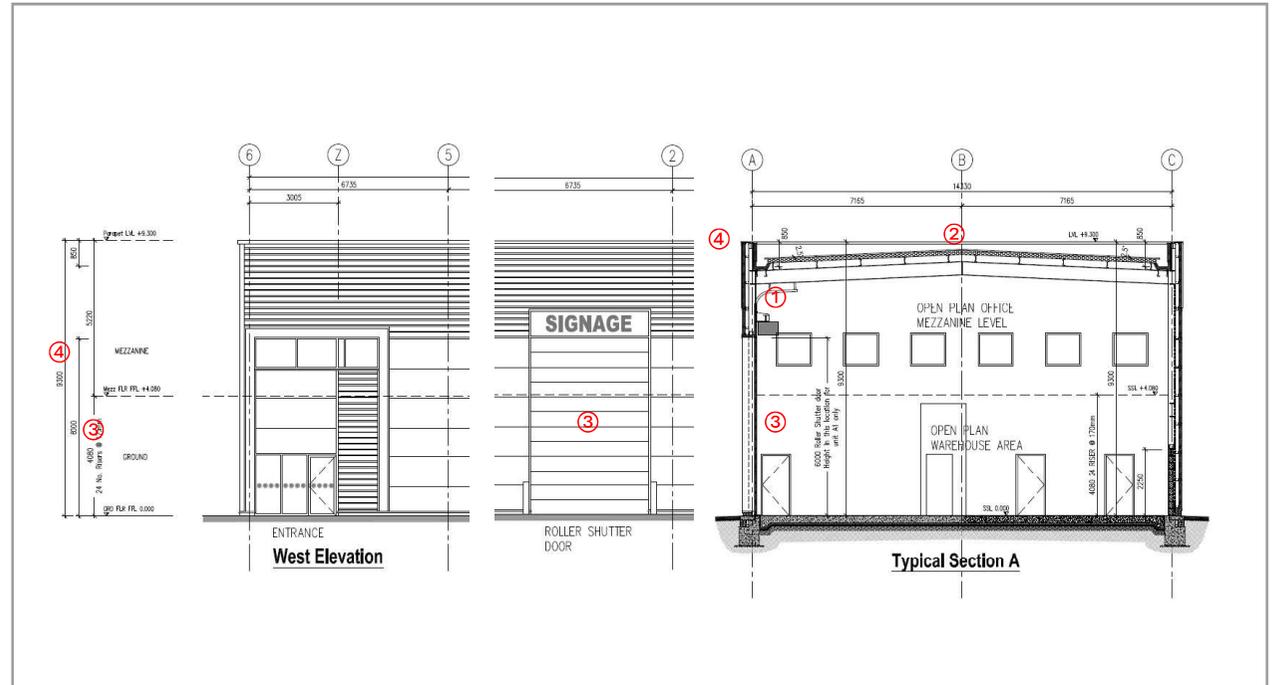


Fig.18 Building heights (extract from Drg. No. 122016 P102 - P03)

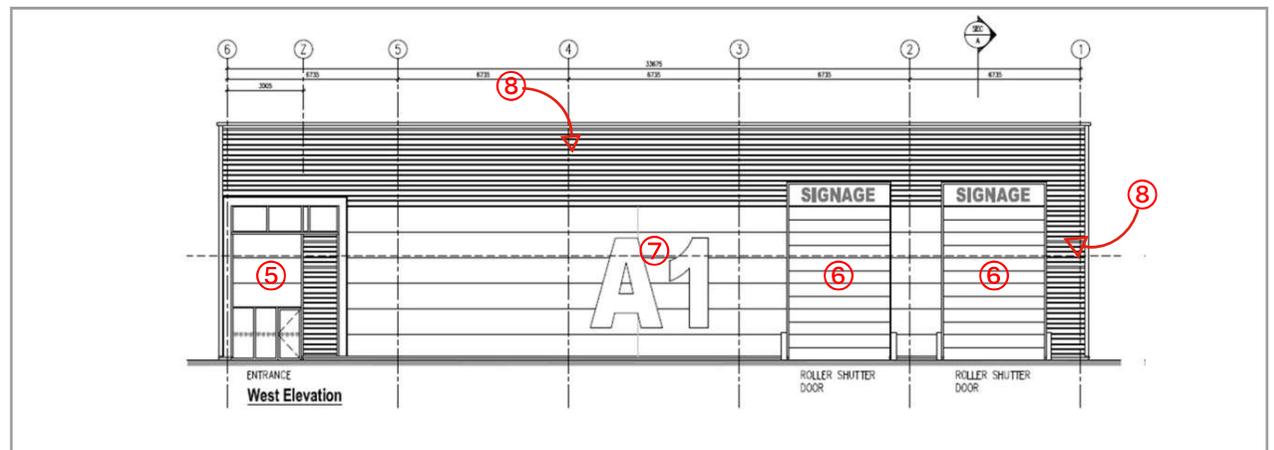


Fig.19 Elevational treatment and proportions (extract from Drg. No. 122016 P102 - P03)

the site. Combining it with flat panel cladding in “pure grey” ⑩ in a range of combinations provides a variety of elevations in the following ways:-

- A 3m deep “copper beech” cladding entablature running above the “pure grey” flat panels to reflect the same feature on the Benlow Works ⑪
- 6m tall “copper beech” cladding pilasters which provide the visual support to the entablature and reflect the buttressing between windows on the Benlow Works ⑫
- The use of “pure grey” flat panels to emphasise the active frontages to the Units with the entrances, loading bays and Unit numbers ⑬
- The use of “pure grey” flat panels to the rear elevations facing Benlow Works reflecting the spandrels between the brick pilasters ⑭
- Full height “copper beech” cladding to the three non-active gable elevation ⑮
- A grey capping on the top of the parapet to add a visual edge to the copper beech cladding ⑯

It must be emphasised that these proposals are for a temporary building as requested by Hillingdon Council and this is reflected in the use of façade materials with high quality, lightweight metal cladding which can be dismantled. It would not be possible to use a traditional masonry facade given their temporary nature.

With regard to external floorscape the service courtyard is finished in grey in-situ, brushed concrete slabs, and the parking and footpaths are a mixture of light buff and grey block paving.

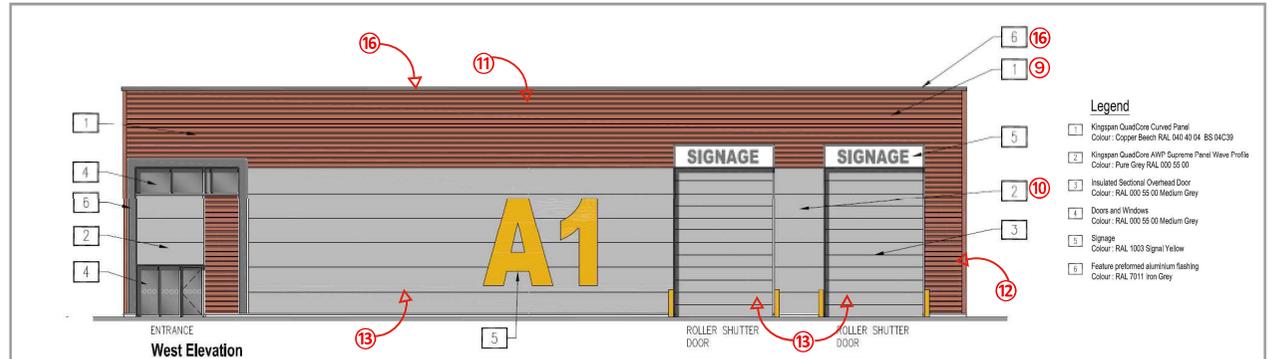


Fig.20A Elevational treatment - historical context (extract from Drg. No. 122016 P202-P01)

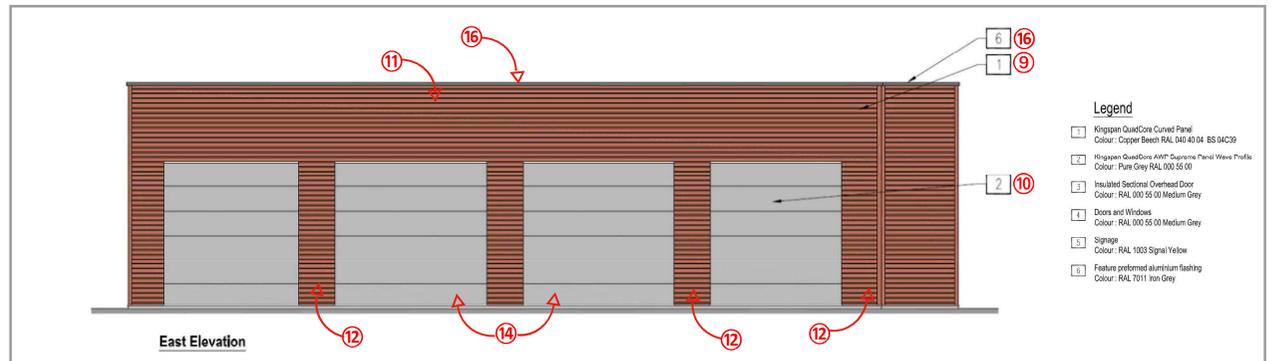


Fig.20B Elevational treatment - historical context (extract from Drg. No. 122016 P202-P01)

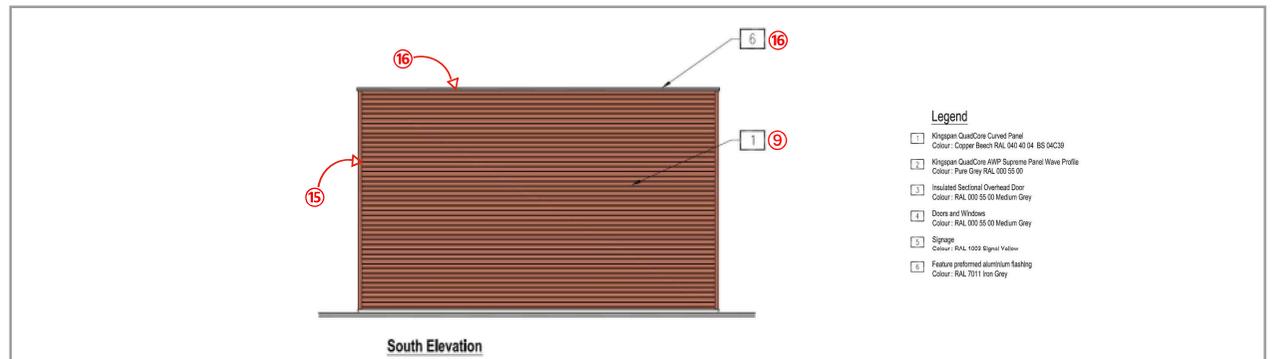


Fig.20C Elevational treatment - historical context (extract from Drg. No. 122016 P202-P01)



Fig.21B Current view

Fig.21A - Proposed view to east along Silverdale Road



Fig.22B Current view

Fig.22A - Proposed view to west along Silverdale Road

## 6.8 Amenity for Nearby Residents

There are 2 residential properties on the northern side of Silverdale Road, one directly opposite the development site (1 Chalfont Road), and the other north west of the site (2 Chalfont Road) which is approximately 32m from the closest point of Unit A1 (Fig.23). The existing site is an open-air works with some adhoc enclosures and can create noise, dust and odours for both properties. By contrast, the proposed industrial buildings will regularise the use and improve its relationship to surrounding residents and in particular to 1 & 2 Chalfont Road.

1 Chalfont Road has a stepped gable wall section and there is approximately a 1.3m step in the wall from ground floor to first floor and there is one window at first floor level (Fig.23).

The gable (north) elevation of Unit A1 as proposed, which runs in parallel with Silverdale Road and the northern boundary to the site, is set approximately 16.8m away from the ground floor of 1 Chalfont Road and has no windows. This dimension has been increased from 15m following feedback from Hillingdon Council planning team and has resulted in more substantial landscaping along the northern boundary including screening, climbing plants, trees and shrubs with herbaceous planting ① (Fig.24C).

As there are no windows in the north gable of Unit A1 (Fig.25) there is no impact on the privacy (i.e no overlooking) to surrounding residential properties, in particular No.1 Chalfont Road. This is a typical distance and relationship for properties across a street and given the reduced noise, dust and odour impacts from the existing use of the land, will result in an improved relationship to the residential properties.

Whilst the new “copper beach” clad building will provide a vastly improved outlook, in order to improve further the general amenity for local residents the northern boundary to the site will be landscaped behind a 2.4m high weld mesh boundary and security fence ② with approximately 1.5 - 4m deep planting/screening ① which will significantly enhance both the immediate aspect across Silverdale Road and the experience for passing pedestrians and cyclists.

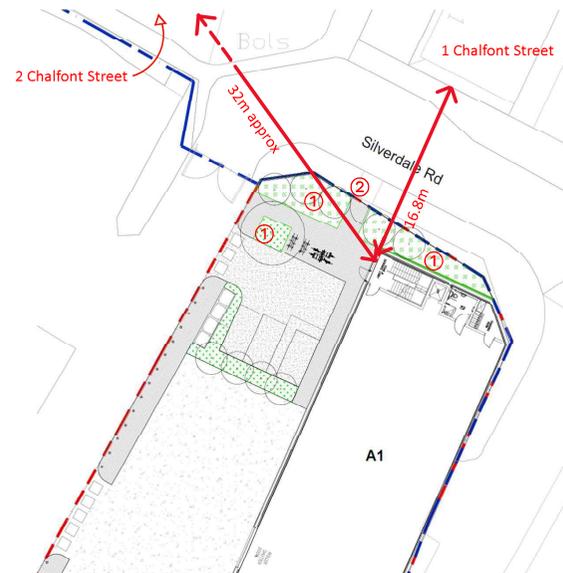


Fig.23 Proximity of Residential Properties (Extract from Drg. No. LN00943 L-101)



Fig.24A 1 Chalfont Street



Fig.24B Existing view to west along Silverdale Road



Fig.24C Proposed view to west along Silverdale Road

## 6.9 Daylight/Sunlight Impact on 1 Chalfont Road

The potential impact on daylight and sunlight was discussed at the pre-application meeting.

1 Chalfont Road which is directly opposite the development site (across Silverdale Road) (Fig.27) has a stepped gable wall section and there is approximately a 1.3m step in the wall from ground floor to first floor with one window (900mm wide x 1200mm high) at first floor level (Fig.28). The gable (north) elevation of Unit A1 which runs in parallel with Silverdale Road is 9.3m high and is set 16.831m away from the ground floor of 1 Chalfont Road. The gable to Unit A1 is offset from the perpendicular from the window.

Without an internal inspection of No.1 Chalfont Road for these purposes it is assumed the window is for a habitable room or space but not a living room due to its first floor location

The BRE guidance in BR209 covers both daylight and sunlight.

In terms of **daylighting** it states that its guidelines on existing buildings and new development relate to living rooms, kitchens, and bedrooms, but windows to bathrooms, toilets, storerooms and circulation areas need not be analysed. It also states that if the angle to the horizontal subtended by the new development at the level of the centre of the lowest window is less than 25° for the whole of the development then it is unlikely to have a substantial effect on the diffuse skylight enjoyed by the existing building (Fig.29).

In terms of **sunlighting** it states that its guidelines on existing buildings and new development relate to main living rooms, (kitchens and bedrooms are less



Fig.28 Gable Wall to 1 Chalfont Street

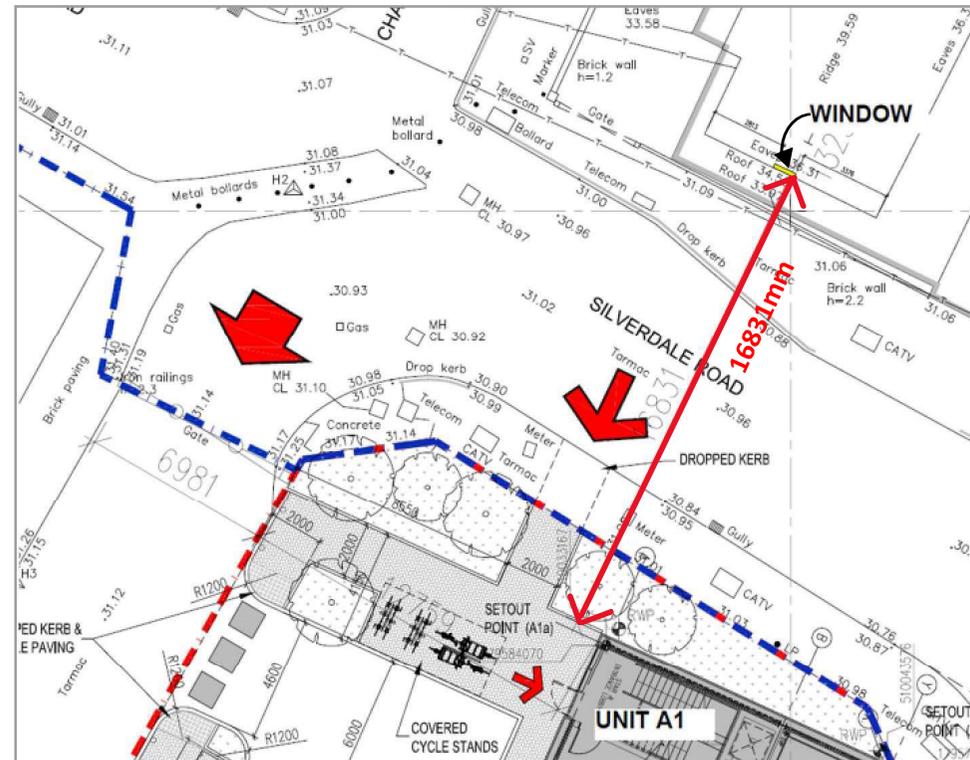


Fig.27 Proximity of 1 Chalfont Road (extract from Drg. No. 122016 P1002 P03)

important), and they have a window facing within 90° of due south. It also states (para 3.2.13) that the sunlighting of an existing dwelling may be adversely affected if a living room of an existing dwelling has a main window facing within 90° of due south, and any part of a new development subtends an angle of more than 25° to the horizontal measured from the centre of the window in a vertical section perpendicular to the window (Fig.29).

By using this '25° test' for both daylighting and sunlighting the proposed development (Unit A1) subtends an angle of 15° to the centre of the first floor window in 1 Chalfont Road (Fig.29). It is less than 25° and therefore the following conclusions can be made:-

- the new building is unlikely to have a substantial effect on the diffuse skylight (i.e. daylighting) enjoyed by the existing building
- it is unlikely the sunlighting of the existing dwelling will be adversely affected by the new building particularly as there appears to be no living room in the southern elevation of the existing property.

In conclusion, based on these assessments, the proposed development passes both the daylight and sunlight tests set out in the guidance in BRE document BR209.

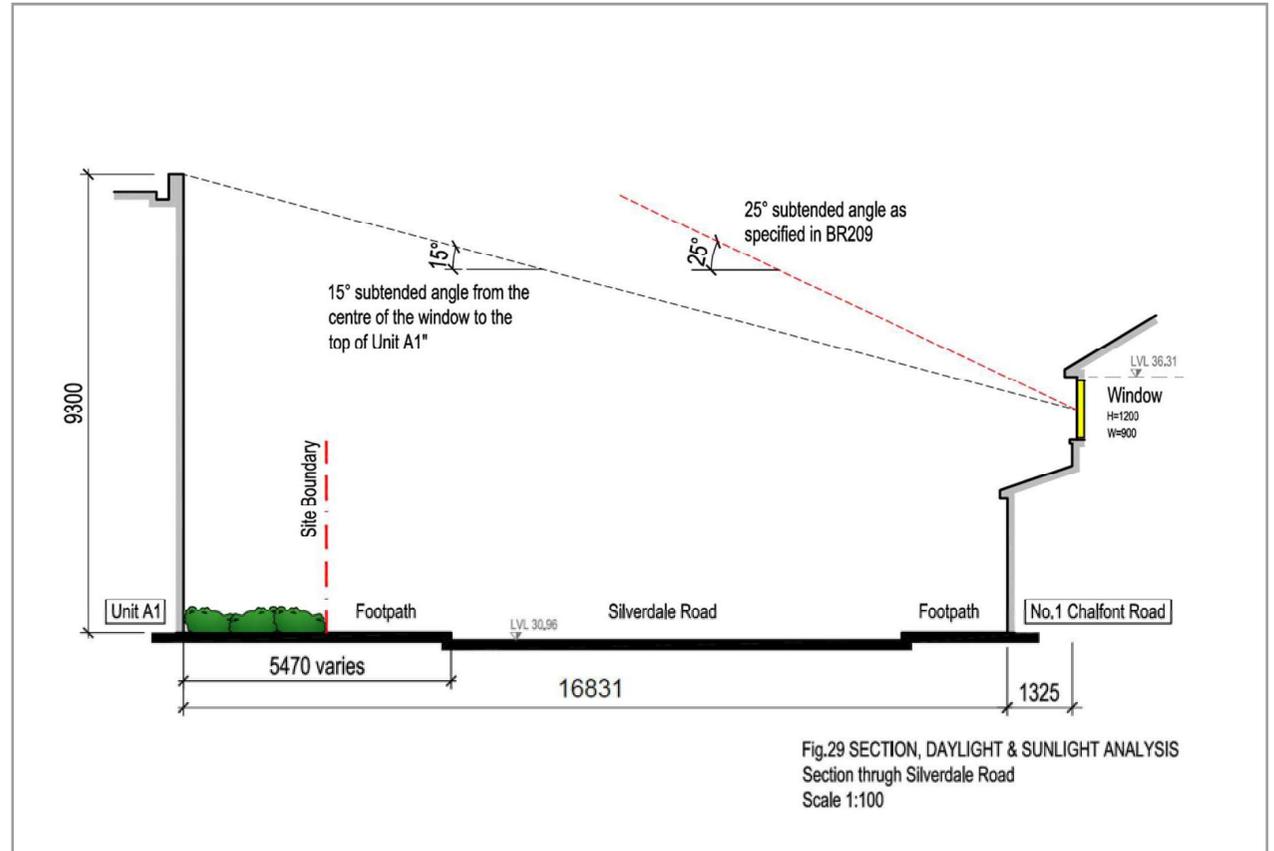


Fig.29 Section Analysis

## 6.10 Building Layouts

Both Units A1 and A2 provide simple industrial/storage space with pedestrian and loading bay access and an internal ceiling height of between 8.3m and 8.6m.

Unit A1, with a GIA of 544.74 m<sup>2</sup> and situated to the north of the service courtyard has its pedestrian entrance in the north west corner of the building ①. The two loading bays are located in the south west corner of the building ②.

The pedestrian access leads into a staircase and entrance lobby. At groundfloor level a disabled access toilet ③ and changing room ④ are provided alongside the main industrial/storgae space. There is a mezzanine level above, accessed via the stairs and a lift, which provides kitchenette ⑤ and office space ⑥.

Unit A2, with a GIA of 432.39 m<sup>2</sup> and situated to the south of the service courtyard has its pedestrian access in the south west corner of the building ①. The loading bay is located in the north west corner of the building ②.

The pedestrian access leads into an entrance lobby and on to a disabled access toilet ③, changing room ④ and kitchenette ⑤ which are provided alongside the main industrial/storage space.

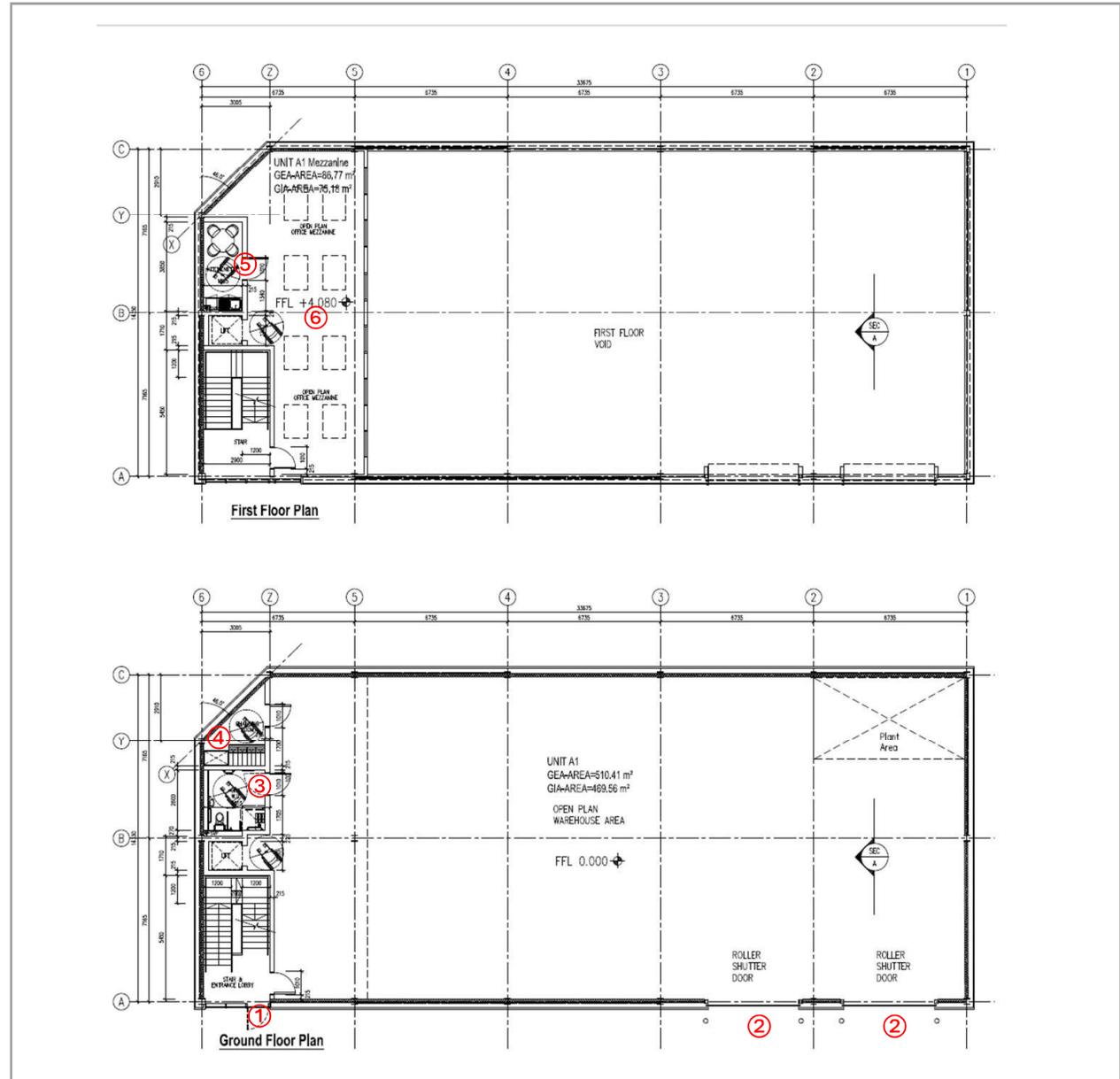


Fig.30A Unit A1 Floor plans (extract from Drg. No. 122016 P100 - P04)

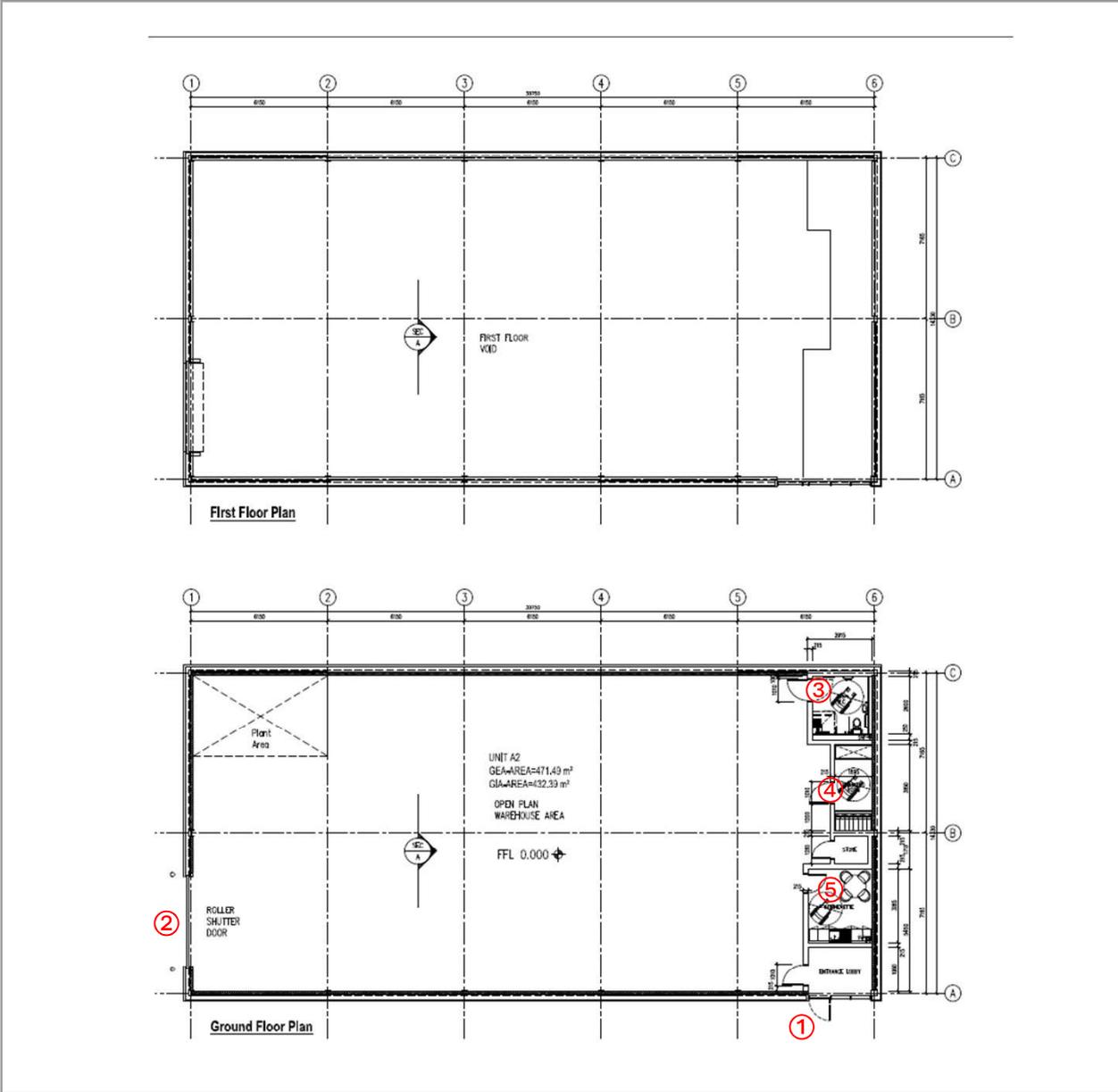


Fig.30B Unit A2 Floor plans (extract from Drg. No. 122016 P101 - P03)



Fig.31A Sketch view looking south east



Fig.31B Sketch view looking north east



Fig.31A Sketch view looking south west

## 6.11 Accommodation Schedule

Unit	Level	GEA (m2)	GIA (m2)
A1	Ground floor	510.41	469.56
	Mezzanine	86.77	75.18
A2	Ground floor	471.49	432.39
<b>TOTAL</b>		<b>1068.67</b>	<b>977.13</b>

## 6.12 Temporary Nature of the Buildings

As set out herein, the development proposal is for a temporary use (i.e. seeking temporary 'meanwhile' consent) as set out in the Pre-application Advice.

Therefore the buildings have been designed as simple steel structures with an insulated metal cladding which can be dismantled when their temporary use has expired, removed from site and, if appropriate, recycled for use elsewhere.

## 6.13 Conclusion

This Design and Access Statement clearly demonstrates that this application for temporary planning permission for B2/B8 buildings has been carefully considered by the Client's design team in close liaison with Hillingdon Council, and in doing so, that the proposed development will bring significant improvements and benefits.

The proposal revolves around regularising an existing industrial/storage use and, with that, reducing the impact on the local neighbourhood. It is the redevelopment of a site which is currently an open air works and storage facility with several adhoc enclosures and structures and with congested, heavy vehicle servicing.

By creating a high-quality design which provides contemporary buildings for industrial and storage purposes, and which will be an integral part of an extended Craufurd Industrial Estate, there are a number of clear improvements and benefits.

- An optimised site layout to current industrial/storage standards in terms of circulation, parking, servicing and access including the consolidated use of the existing vehicular service access to Craufurd Industrial Estate
- A site layout which carefully integrates vehicular circulation with safe access and circulation routes for pedestrians and cyclists, including separate access points to the site
- A layout which introduces landscape proposals which maximise and enhance the soft landscape, including 19 trees, and where possible to provide biodiversity enhancement as well as visual amenity
- A layout which also introduces a hard landscape strategy which has been developed to ensure clear legibility within the site and connectivity into its context for pedestrians and cyclists as well as vehicles
- Buildings which, despite being both contemporary and temporary, respect the surroundings and local heritage with the use of clear visual references in the design and good quality materials, and thereby providing an enhancement to the Grade 2 Listed Building nearby
- Buildings which are purposely designed in terms of layout, height and accommodation to suit the demands of the market locally
- A much-improved visual amenity and outlook for local residents with well-designed buildings and soft landscaping both on the perimeter of the development and within the site
- Replacing open storage/industrial with a controlled and managed building/operation which will reduce noise and dust emissions.