

**Design, Heritage and Access Statement for the
Conversion of upper floors to provide 2no. 2bed Maisonettes and 2No. 1bed Flats
at 42a Windsor Street, Uxbridge**

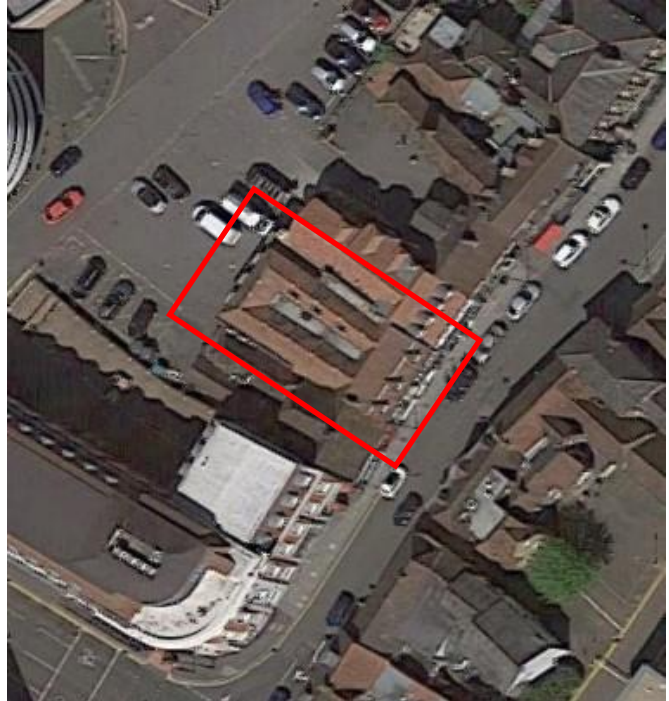
This document should be read in conjunction with proposal drawings ref 01 – 09 for the development analysis and explanation to the design and layout of the proposed conversion and also the historical importance of the existing building and the retention of significant historical details.

General Background

The original building fronting onto Windsor Street is a Grade II Listed early 17th Century timber framed building located within the Old Uxbridge/ Windsor Street Conservation Area.

It is an attractive building with a shop at ground floor and an access door to the first & second floors. The upper floors of the property have been used as offices for the past 25+ years, but due to the size and accessibility of the office unit, in comparison to new build out-of-town modern offices the building has been unable to attract a tenant for the past 5 years. Leaving the first and second floors susceptible to damage due to lack of occupancy and maintenance.

The first floor of the property is characterised by mock timber framing detail in-filled with white rough-cast rendered panels. There is a characteristic oriel window centrally positioned and wood dentil cornice detailing at eaves level. The original building is topped with a gambrel, plain clay tiled roof form with decorative ridge tiles and finials on the 3 dormers to the front. A chimney stack has been retained externally.



The property was extended in the 1980s with a large three storey (including mansard roof) extension to the rear removing large areas of the rear wall of the original listed building.

The submitted proposals retain the commercial premises on the ground floor and utilise the existing vacant and dilapidated first and second floors of the property for residential development whilst looking to overcome the issues raised by planning and conservation officers during the submitted applications in 2017 and the more recent refusal notice ref; 48534/APP/2019/3956.

Proposals + Responses to Previously Submitted Scheme Ref: 48534/APP/2019/3956

Taking into consideration the issues raised by the Planning Officer and Conservation Officer during the previous application Invent Architecture Ltd have re-assesses the proposed layouts and the potential to convert the upper floors of 42a Windsor Street to residential development whilst looking to overcome the following issues;

- Unsympathetic subdivisions and lack of a historical building survey / assessment
- Insufficient information has been provided as regards daylight/sunlight
- Useable external amenity space or compensatory measures
- To provide a suitable standard of residential accommodation by reason of actual and perceived overlooking between internal bedroom and staircase windows
- Siting of the windows and their proximity to the adjoining flats, would result in an oppressive environment and the provision of habitable rooms with very poor outlook that would result in a substandard form of living accommodation
- Failed to provide S106/Legal Agreement that would ensure that future residents do not have access to parking permits within the Controlled Parking Zone

Included within our designs and design statement we have answered the above comments which should satisfy the Planning Officer and Conservation Officer concerns.

The proposals submitted are for the conversion of the first + second floors of the building to provide 2No. 2 Bed maisonettes (a self-contained apartment split over two floors and having its own independent entrance) to the rear of the property and 2No. 1 Bed flats to the front of the property, within the existing historic building.

By the creating of 2No.1 Bed flats the front portion of the building is split between the proposed bedroom and kitchen / dining / living spaces utilising the existing beam partition layouts to avoid any unsympathetic subdivisions of the historic part of the building.

As noted above the existing internal finishes, within the original listed building footprint, are to be unchanged and retained as part of the proposals.

There is no intended works to the historic internal or external fabric of the building. During consultation in the previous application we agreed a planning condition to enforce this.

In order to overcome the Day / Sunlight / Outlook, we reviewed the existing building layouts and concluded that we needed to introduce an open-to-air courtyard in the centre of the building that would serve the upper floors of the maisonettes, Flats 1 + 2. By removing this portion of roof enabled to provide natural daylight / sunlight and an outlook to the smaller single bedrooms on the first floor of the maisonettes (existing second floor). Allowing the larger double bedrooms to be positioned on the external perimeter of the building benefitting from the existing windows and dormers with an outlook onto the rear public realm. We have also used this open courtyard to provide natural daylight to the corridor areas of the maisonettes. The proposed hallway window onto the courtyard (and the retained rooflights) will provide light over the landing areas and would introduce daylight onto the entrance level corridor of the maisonettes too. The two courtyards provided are private amenity areas accessed direct from the bedrooms of each flat. The courtyard will be divided by a 1.8m high screening therefore mitigating any overlooking / interlooking between units.

There are NO windows from Flats 3 + 4 looking into the courtyard.

In addition to the above a Daylight Calculation has been provided for the two internal bedrooms, facing the courtyard, which confirm that there will be a 3.1% daylight factor from the glazed doors leading to the courtyards. Which will provide a well lit bedroom space. In addition to this there is an additional rooflight provided to each of these roofs allowing for additional natural daylight and ventilation to enter these rooms.

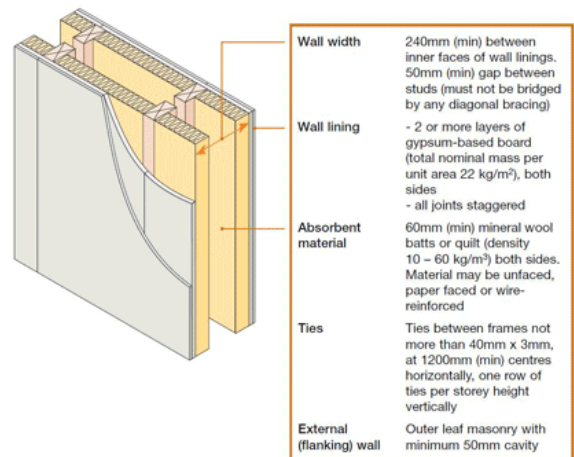
The entrance level of the maisonettes, Flats 1 + 2 has a central corridor with access to a family bathroom and utility cupboard, both of which do not required natural daylight under current regulations, the corridor continues to the open plan kitchen / dining / living spaces of maisonettes that utilise the existing first floor windows for their natural daylight / sunlight and outlook to these habitable rooms. The open plan kitchen / dining / living spaces have an average room depth (from external windows) of approx. 5.8m, which is no worse than an internal dimension in a modern build block of flats.

The entrances of the 1Bed Flats, Flats 3 + 4 are located off of the communal lobby. The new single staircase, leading to Flat 4, is positioned in the 1980s section of the building therefore no loss of any historic fabric. Both flats have been designed to orientate the living and sleeping accommodation to the front of the existing building benefitting from the existing windows and dormers onto Windsor Street. The bathrooms / store cupboards and utility areas are positioned to the rear of the these units as they do not require natural daylight under current regulations.

The communal entrance to each of the maisonettes will be via the existing communal corridor (fronting Windsor Street) and utilise the existing principle staircase from ground to first floor levels. The existing staircase from first to second

floor would be removed as part of the proposals. Each of the flats will benefit from a secure cycle store, utilising the existing rear garages and a communal bin and recycling store.

As the proposals are for maisonettes + flats the sound and fire separation will be required to the new party walls. The new party walls will be constructed of timber stud panels filled with insulation and doubled boarded (with a suitable Gypsum based plasterboard) to achieve the required fire resistance and Part E acoustic requirements. We have provided a detail of this system for clarity. The benefits from this timber panelled party wall system is that the affect to the existing buildings can be kept to a minimum. Where the new party walls abut an existing timber beam / panel a fire stop mastic can be used to avoid the cutting and removal of historic elements of the building.



Where the existing floor is to be upgraded to a new party floor (between Flats 3 + 4) the underside of the existing ceilings, where there are historic downstand beams and lath + plaster finishes a clear fire coating, paint applied, is to be provided over the affected areas to achieve a minimum 60 minute fire resistance. The sound proofing properties will be applied on top of existing floor boards providing an acoustic sound reduction in accordance with Part E.

We are therefore able to retain all of the historic timber detailing in the existing ceilings + walls.

As mentioned above Flats 1 + 2 will each have a private external amenity area in accordance with Policy DMHB 18. Although Flats 3 + 4 does not have any private external amenity space, and due the historical listing of the existing building, it is unlikely a balcony would be supported on the principal elevation of the building, we can have reviewed the local facilities and confirm that within 400m of the application site there is access to Rockingham Recreational Ground, Fassindge Park and Fassindge Allotments where public outdoor space can be enjoyed.

It is evident from our inspections that there are no historical elements or materials to the rear of the building where the 1980s extensions have been built.

The external appearance of the property is fully retained with the only alteration proposed would be the removal of the hybrid 1980s pitched and flat roof addition as noted on the plans.

On agreement that the submitted scheme is acceptable our client is willing to enter a legal agreement and sign a Section 106 / Legal Agreement that would ensure that future residents do not have access to parking permits within the Controlled Parking Zone.

Sustainability + Access

The proposed dwelling will incorporate high levels of energy efficiency through the use of energy efficient materials that will meet current standards required by the building regulations. The design includes the use of natural daylight which reduces the cost of artificial lighting within the development.

The building is positioned on the High Street which benefits from short walking distance to Uxbridge Station (metropolitan line) and there is regular bus service connecting with local towns.

Public car parking is available within walking distance of the building.

Cycle storage is provided within the building.

The principle access to the building is retained.

Listed Building + Heritage Statement

1. 5018 WINDSOR STREET

(North-West Side)

UXBRIDGE

Nos 41 & 42

TQ 0584 SW 18/450

II GV

2. Early C17 house of 2 storeys and attic, 3 windows. Tiled gambrel roof renewed with 3 modern dormers, wood dentil cornice. 1st floor roughcast with mock half timbering. Ground floor has plain C19 shop fronts.

Nos 39 to 48 (consec) form a group.

Listing NGR: TQ0544784030



Existing Front Elevation



Existing Rear Elevation

As noted above the existing internal finishes, within the original listed building footprint, are to be unchanged and retained as part of the proposals.

There is no intended works to the historic internal or external fabric of the building. During consultation in the previous application we agreed a planning condition to enforce this.

Conclusion

The scheme retains the commercial ground floor frontage to the High Street.

The scheme utilises redundant upper floor office spaces for residential accommodation providing homes in the town centre. Which is supported under the current National Policy Class O of Town and Country Planning (General Permitted Development) Order 2015 Schedule 2, Part 3 in respect of Change of Use from B1 to Dwellinghouse, with the exemption that this is a listed building.

The scheme now overcomes the reasons for refusal, application ref: 48534/APP/2019/3956.

The scheme retains the historic qualities associated to the listed building.

There is no impact in the design that would affect neighbouring properties.