

Heathrow Flightpath Car Park

784-B070271

Refuse Management Strategy

LPH UK 1 Ltd ('Lysara')

September 2025

Document prepared on behalf of Tetra Tech Environment Planning Transport Limited. Registered in England number: 03050297



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DRAWINGS

PROPOSED SITE PLAN - PHASE 2: DRAWING NO. 7935-SMR-00-ZZ-DR-A-2004-S3-P12

1.0 INTRODUCTION

- 1.0.1 This Refuse Management Strategy relates to the proposed development of an electric vehicle car park, at NCP Car Park (Heathrow Flightpath), off the A408, West Drayton, to the north of Heathrow Airport.
- 1.0.2 The 1.64-hectare application site is within the London Borough of Hillingdon (LBH) and is roughly bound by the M4, A4 and Sipson Way. A Site Layout Plan is appended showing both the site location and details of the nature of the development. Whilst principally comprising parking bays and access roads, the development includes some limited built development, which includes a food and beverage unit and welfare buildings for staff. The description of the development is provided below: -
- ‘Hybrid application consisting of full planning permission for the creation of a mixed use sustainable vehicle parking facility (Sui Generis) and food and beverage unit (Class E), alongside ancillary welfare and staff buildings, and other supporting infrastructure and site levelling, and outline planning permission for a future extension to the facility, with all associated matters reserved except for access (‘the Proposed Development’)’*
- 1.0.3 This Refuse Management Plan relates to the whole development site.
- 1.0.4 LBH include a ‘Refuse Management Plan’ in their Local Requirement List, which forms part of the February 2024 Local Planning Validation Checklist. This is a requirement for outline, full, and reserved matters applications. Hillingdon Local Plan Part 2 (2020) Policy DMHB 11: ‘Design of New Development’ requires space for recycling and suitable access for collection.
- 1.0.5 Policy EM11: ‘Sustainable Waste Management’ (Part 1 of the Local Plan, (2012)) requires waste management to be appropriately considered in development proposals.
- 1.0.6 There are also relevant requirements specified in London Plan 2021 (Policy SI 7 ‘Reducing waste and supporting the circular economy’), that have been considered in this document.
- 1.0.7 Whilst the development would not generate significant quantities of waste, the intention of this report is to provide details on how the generated waste would be managed. This includes the following details: -
- Estimated volumes and types of waste produced by the development.
 - Details of bin storage areas for both waste and recycling.

- Location of the proposed waste collection point and how the waste collection vehicle access the point.
- Details of how waste will be transferred between waste stores and the collection location and those persons responsible for waste management.
- Details of the number and capacity of bins proposed and collection frequency.

2.0 ESTIMATED TYPES OF WASTES, VOLUMES, AND PROVISION

2.0.1 The main source of waste is likely to arise from the food and beverage unit. The unit is 195m² and would be erected in the south of the site. Waste would also be generated by staff, with a 16m² staff unit and a 30.5m² welfare unit proposed. Customers of the car park may also bring waste into the site in their cars and look to deposit waste in the bins provided. The predicted waste types are anticipated to mainly comprise food and drink packaging. The waste types are displayed in the table below.

Table 1: Predicted Operational Waste Types

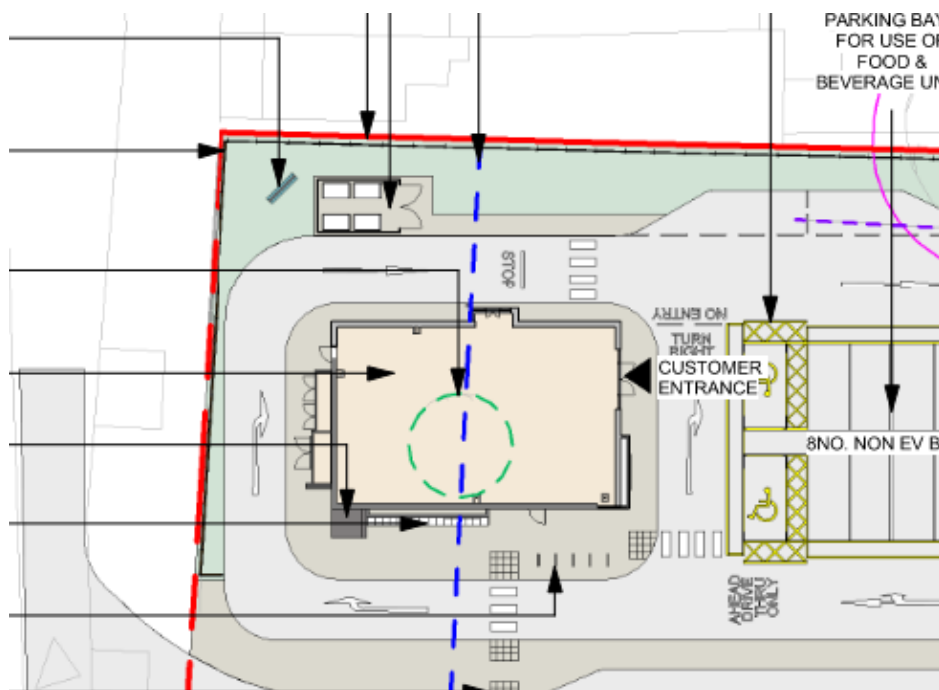
Waste Type	EW Code
Paper and cardboard	20 01 01
Glass	20 01 02
Biodegradable kitchen and canteen waste	20 01 08
Solvents	20 01 13* (hazardous)
Fluorescent tubes and other mercury containing waste	20 01 21* (hazardous)
Discarded equipment containing chlorofluorocarbons	20 01 23* (hazardous)
Edible oil and fat	20 01 25
Batteries and accumulators other than those mentioned in 20 01 33	20 01 34
Discarded electrical and electronic equipment other than those mentioned in 20 01 21 and 20 01 23 containing hazardous components	20 01 35* (hazardous)
Discarded electrical and electronic equipment other than those mentioned in 20 01 21, 20 01 23 and 20 01 35	20 01 36
Plastics	20 01 39
Metals	20 01 40
Biodegradable waste	20 02 01
Mixed municipal waste	20 03 01
Bulky waste	20 03 07

2.0.2 Many Local Authorities base waste storage provision on information produced by the Association of Directors of Environment, Economy, Planning and Transport (ADEPT), in their 2010, Making Space for Waste guidance document.

2.0.3 For foot outlets, ADEPT recommend 10,000 litres of waste storage space for every 1,000m². For the welfare and staff space, the metric for offices has been used, which recommends 2,600 litres of waste storage space for every 1,000m² of floor space.

- 2.0.4 On this basis the food and beverage unit would generate around 1,950 litres of waste per week, and the welfare/ staff space would generate around 121 litres of waste per week. Based on the above, around 2,070 litres of waste per week would be generated. It is considered the quantities of waste that would be brought into the site by customers of the car park would be negligible.
- 2.0.5 In total at the site, it is considered the allowance of space for a 1,100-litre euro bin for general waste, a 1,100-litre euro bin for recycling, and a 240-litre bin for food waste would provide sufficient waste storage space and would allow for recyclable materials to be separated out from residual waste.
- 2.0.6 The occupier of the food and beverage unit, a well renowned British business, also has their own specification for bin stores, which includes 24-7 vehicular access to 3 x 1,100 litre euro bins.
- 2.0.7 A bin store would be located to the west of the food and beverage unit, as shown in the top left corner of the Figure below. The store could accommodate at least 4 x 1,100 litre euro bins (or a higher number of smaller bins if required).

Figure 1 – Annotated Extract of Site Plan, Showing Bin Storage Location



- 2.0.8 This space has been designed to provide sufficient capacity for all uses at the site. If the occupier of the food and beverage unit was to require 3 x 1,100 litre euro bins, the other uses at the site would have the equivalent space of one further 1,100 litre bin (or a higher number of smaller bins). Based on the stated ADEPT guidance, this amount of space would be sufficient. It is also considered the development has been designed with sufficient space to enable the necessary segregation and collection of the waste streams generated by the development.
- 2.0.9 The bin storage areas would be demarcated by floor markings and/ or appropriate wall signs to ensure that the area is retained as a waste storage area.

- 2.0.10 The wheeled waste containers would be stored on an impermeable surface. This would ensure that the area can be routinely cleaned.
- 2.0.11 The waste storage area would be sufficiently lit to ensure that users are able to safely access the waste storage area during periods of darkness or low light levels.
- 2.0.12 No flammable materials would be stored within or close to the waste storage areas.

3.0 MOVEMENT & COLLECTION OF WASTE AND RESPONSIBILITIES

- 3.0.1 The waste bins will be collected by refuse collection vehicles (RCVs) from the internal road, immediately adjacent to the bins store. Vehicle tracking has been completed as part of the design of the site. The collection crews will take the bins directly from the store and will take the emptied bins back to the store.
- 3.0.2 A private waste management company would be hired to make the waste collections.
- 3.0.3 The management company at the site are responsible for ensuring standards at the site are maintained, and this would include routine monitoring of surfaces, i.e. for damage, to ensure the bins can easily be wheeled.

4.0 SUMMARY AND CONCLUSIONS

- 4.0.1 This Refuse Management Strategy relates to the proposed development of an electric vehicle car park, at NCP Car Park (Heathrow Flightpath), off the A408, West Drayton, to the north of Heathrow Airport.
- 4.0.2 Whilst the development would not generate significant quantities of waste, this document sets out how waste would be managed at the site.
- 4.0.3 Sufficient waste storage capacity has been designed into the development, and 4 x 1,100 litre euro bins could be accommodated (or a higher number of smaller bins). Sufficient space has been designed to allow for effective recycling.
- 4.0.4 A private waste management contractor would be hired to collect the waste from the site and return the emptied bins to the store. An on-site management company would be responsible for maintenance.
- 4.0.5 The details provided in this Strategy demonstrate that the sustainable management of waste has been given a high priority within the design of the development. It is considered sufficient space has been proposed within the development to allow for effective recycling. The development design would also allow for waste to be transported and collected in a safe and efficient manner.

DRAWINGS

PROPOSED SITE PLAN - PHASE 2: DRAWING NO. 7935-SMR-00-ZZ-DR-A-2004-S3-P12

BLUE LINE DENOTES EXISTING BOUNDARY FENCE AS PER TOPOGRAPHICAL SURVEY. REFER TO - 35225 Topographical Survey (December 2024).

BINS AREA. REFER TO REFUSE MANAGEMENT STRATEGY FOR FURTHER INFORMATION.

MAGENTA DENOTES TREE ROOT PROTECTION AREA

INDICATIVE LOCATION OF 9.05m HIGH TOTEM SIGNAGE UNDER SEPERATE APPLICATION. COLOUR AND MATERIAL TBC.

PROPOSED KNEE RAIL FENCE. REFER TO DRAWING 7935-SMR-00-ZZ-DR-A-2006.

PROPOSED REMOVAL/RELOCATION OF EXISTING TREES IN GREEN DASHED LINE. REFER TO - 35225 Topographical Survey (December 2024)

FOOD & BEVERAGE UNIT. DESIGN SUBJECT TO OCCUPIER SPEC TBC.

2NO. LONG STAY CYCLE PARKING SECURE LOCKERS

INDICATIVE E-COMMERCE PICK UP / DROP OFF SMART LOCKER - 95 OPENINGS

5NO. SHEFFIELD STANDS FOR CUSTOMER PARKING & E-BIKE BATTERY CHARGING UNIT.

DROPPED KERBS AND TACTILE PAVING.

INDICATIVE LOCATION OF BARRIERLESS ANPR.

INDICATIVE LOCATION OF WAYFINDING SIGNAGE. UNDER A SEPERATE SIGNAGE APPLICATION.

ACCESS JUNCTION AS PER S278 WORKS.

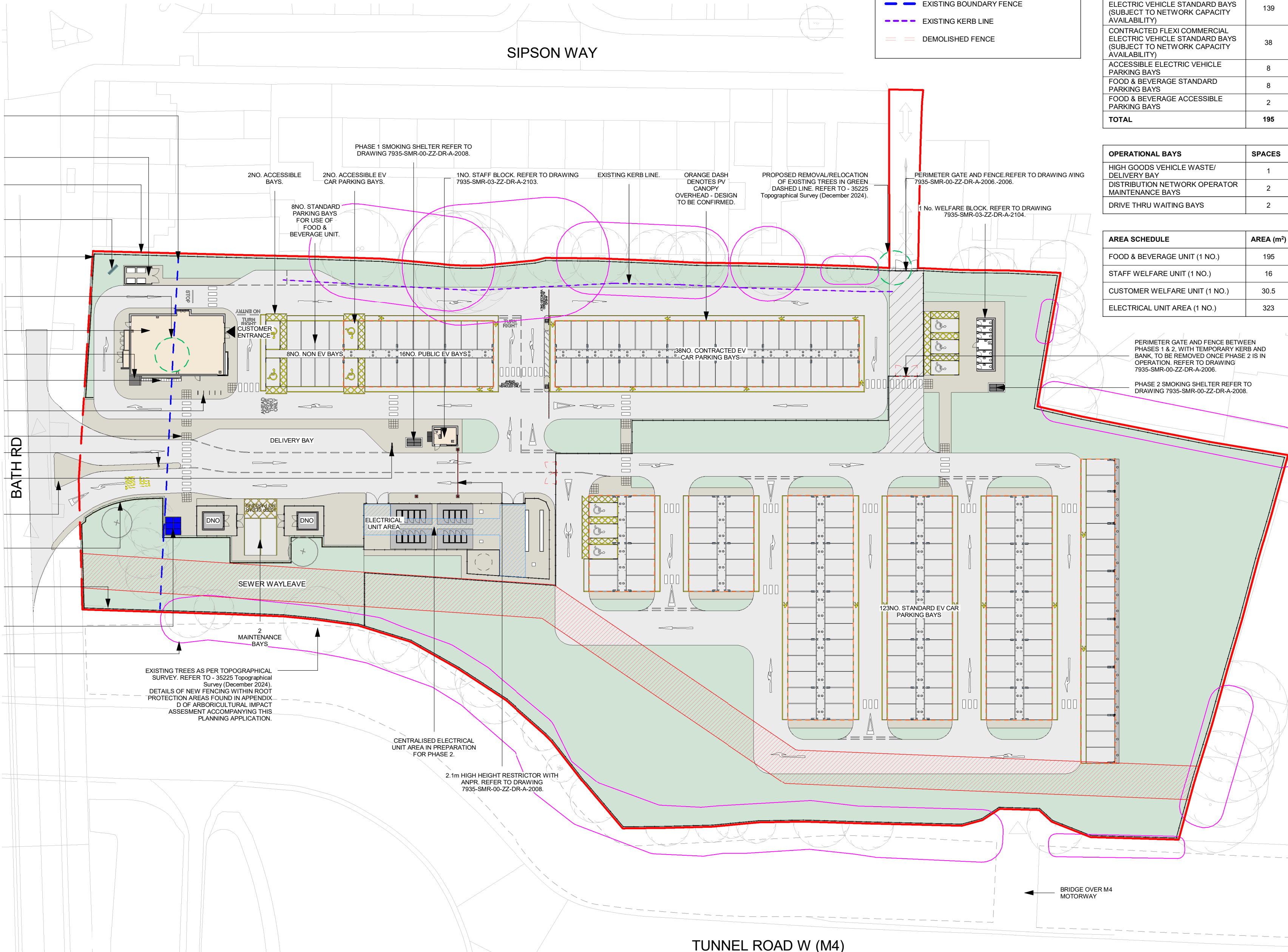
DRWG. REFERENCE: 0001 Rev P6 - FOR CONSTRUCTION BY MAYER BROWN.

PROPOSED KNEE RAIL FENCE. REFER TO DRAWING 7935-SMR-00-ZZ-DR-A-2006.

EXISTING FENCE TO BE REMOVED AND NEW FENCE TO BE ERECTED TO PERIMETER. REFER TO DRAWING 7935-SMR-00-ZZ-DR-A-2006.

CYCLE SHELTER REFER TO DRAWING 7935-SMR-00-ZZ-DR-A-2008.

MAGENTA DENOTES TREE ROOT PROTECTION AREA



LEGEND			
	SEWER WAYLEAVE		
	EMERGENCY ROUTE		
	ELECTRICAL UNIT AREA		
	EXTENT OF PHOTOVOLTAIC CANOPY		
	EXTENT OF ROOT PROTECTION AREAS		
	EXISTING BOUNDARY FENCE		
	EXISTING KERB LINE		
	DEMOLISHED FENCE		

SITE APPLICATION BOUNDARY - PHASE 2			
	AREA (sqm)	AREA (Hectare)	AREA (Acre)
SITE BOUNDARY	16389.50	1.639	4.05

SITE ACCOMMODATION	
CAR PARKING	SPACES
PUBLIC FLEXI COMMERCIAL ELECTRIC VEHICLE STANDARD BAYS (SUBJECT TO NETWORK CAPACITY AVAILABILITY)	139
CONTRACTED FLEXI COMMERCIAL ELECTRIC VEHICLE STANDARD BAYS (SUBJECT TO NETWORK CAPACITY AVAILABILITY)	38
ACCESSIBLE ELECTRIC VEHICLE PARKING BAYS	8
FOOD & BEVERAGE STANDARD PARKING BAYS	8
FOOD & BEVERAGE ACCESSIBLE PARKING BAYS	2
TOTAL	195

OPERATIONAL BAYS	SPACES
HIGH GOODS VEHICLE WASTE/ DELIVERY BAY	1
DISTRIBUTION NETWORK OPERATOR MAINTENANCE BAYS	2
DRIVE THRU WAITING BAYS	2

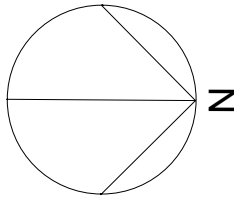
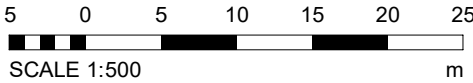
AREA SCHEDULE	AREA (m²)
FOOD & BEVERAGE UNIT (1 NO.)	195
STAFF WELFARE UNIT (1 NO.)	16
CUSTOMER WELFARE UNIT (1 NO.)	30.5
ELECTRICAL UNIT AREA (1 NO.)	323

PERIMETER GATE AND FENCE BETWEEN PHASES 1 & 2. WITH TEMPORARY KERBS AND BANK, TO BE REMOVED ONCE PHASE 2 IS IN OPERATION. REFER TO DRAWING 7935-SMR-00-ZZ-DR-A-2006.

PHASE 2 SMOKING SHELTER REFER TO DRAWING 7935-SMR-00-ZZ-DR-A-2008.

Architects are to be notified of any discrepancies. Contractors must check all dimensions on site. This drawing is subject to copyright laws and is for use on this project only. This drawing is to be used solely for the information as titled only. For other information refer to the latest revision of any cross referenced drawings. To be read in conjunction with relevant design standards/protocols

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P12	MINOR AMENDMENT TO BOUNDARY TREATMENTS & PLANNING.	29.08.25	AH	CV
P11	MINOR AMENDMENT TO ELECTRICAL UNIT AREA.	19.08.25	JC	CV
P10	PLANNING ISSUE IN LINE WITH DESIGN FREEZE 22.07.25.	30.07.25	JC	CV
P9	PLANNING ISSUE.	25.06.25	JC	CV
P8	ADDITION OF PHASE 2 FOOTPATH & REVISED WELFARE BLOCK	19.06.25	AH	CV
P7	F&B UNIT UPDATED, SOLAR CANOPY COLUMN POSITIONS INCLUDED IN PHASE 2. EV CHARGERS INCLUDED, AND OTHER MINOR AMENDMENTS.	06.06.25	AH	JC
P6	UPDATED IN LINE WITH DESIGN FREEZE 02.	23.05.25	JC	JM
P5	TREE ROOT PROTECTION OVERLAID, CYCLE ROUTE ADJUSTED ACCORDINGLY	10.04.25	HJ	JM
P4	SUBSTATION LAYOUT UPDATED. TRUCK PARKING POSITION REVISED. CYCLE PARKING AMENDED	25.03.25	TH	CV
P3	DRAWING UPDATED	10.03.25	TH	CV
P2	NOTES UPDATED	14.02.25	TH	CV
P1	FIRST ISSUE.	13.02.25	TH	CV
Rev	Description	Date	Drm	Ckd

Status

PLANNING

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Project

HEATHROW FLIGHTPATH CAR PARK, UB7 0DU

Client



Drawing Title

PROPOSED SITE PLAN - PHASE 2

Proj Ref	Origin	Zone	Level	Type	Role	Num	Status	Rev
7935 - SMR - 00 - ZZ - DR - A - 2004 - S3 - P12								
SMR Job Ref	Sheet	Scale	Drawn					
7935-00-2004	A2	1 : 500	AH					