

Heathrow Flightpath Car Park

784-B070271

Refuse Management Strategy

LPH UK 1 Ltd ('Lysara')

September 2025

Document prepared on behalf of Tetra Tech Environment Planning Transport Limited. Registered in England number: 03050297



TETRA TECH

Tetra Tech Cardiff, 5th Floor, Longcross Court, 47 Newport Road, Cardiff, United Kingdom, CF24 0AD

Tetra Tech Environment Planning Transport Limited. Registered in England number: 03050297
Registered Office: 3 Sovereign Square, Sovereign Street, Leeds, United Kingdom, LS1 4ER

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PROPOSED SITE PLAN - PHASE 2: DRAWING NO. 7935-SMR-00-ZZ-DR-A-2004-S3-P12

1.0 INTRODUCTION

- 1.0.1 This Refuse Management Strategy relates to the proposed development of an electric vehicle car park, at NCP Car Park (Heathrow Flightpath), off the A408, West Drayton, to the north of Heathrow Airport.
- 1.0.2 The 1.64-hectare application site is within the London Borough of Hillingdon (LBH) and is roughly bound by the M4, A4 and Sipson Way. A Site Layout Plan is appended showing both the site location and details of the nature of the development. Whilst principally comprising parking bays and access roads, the development includes some limited built development, which includes a food and beverage unit and welfare buildings for staff. The description of the development is provided below: -

'Hybrid application consisting of full planning permission for the creation of a mixed use sustainable vehicle parking facility (Sui Generis) and food and beverage unit (Class E), alongside ancillary welfare and staff buildings, and other supporting infrastructure and site levelling, and outline planning permission for a future extension to the facility, with all associated matters reserved except for access ('the Proposed Development')'

- 1.0.3 This Refuse Management Plan relates to the whole development site.
- 1.0.4 LBH include a 'Refuse Management Plan' in their Local Requirement List, which forms part of the February 2024 Local Planning Validation Checklist. This is a requirement for outline, full, and reserved matters applications. Hillingdon Local Plan Part 2 (2020) Policy DMHB 11: 'Design of New Development' requires space for recycling and suitable access for collection.
- 1.0.5 Policy EM11: 'Sustainable Waste Management' (Part 1 of the Local Plan, (2012)) requires waste management to be appropriately considered in development proposals.
- 1.0.6 There are also relevant requirements specified in London Plan 2021 (Policy SI 7 'Reducing waste and supporting the circular economy), that have been considered in this document.
- 1.0.7 Whilst the development would not generate significant quantities of waste, the intention of this report is to provide details on how the generated waste would be managed. This includes the following details: -
 - Estimated volumes and types of waste produced by the development.
 - Details of bin storage areas for both waste and recycling.

- Location of the proposed waste collection point and how the waste collection vehicle access the point.
- Details of how waste will be transferred between waste stores and the collection location and those persons responsible for waste management.
- Details of the number and capacity of bins proposed and collection frequency.

2.0 ESTIMATED TYPES OF WASTES, VOLUMES, AND PROVISION

2.0.1 The main source of waste is likely to arise from the food and beverage unit. The unit is 195m² and would be erected in the south of the site. Waste would also be generated by staff, with a 16m² staff unit and a 30.5m² welfare unit proposed. Customers of the car park may also bring waste into the site in their cars and look to deposit waste in the bins provided. The predicted waste types are anticipated to mainly comprise food and drink packaging. The waste types are displayed in the table below.

Table 1: Predicted Operational Waste Types

Waste Type	EWC Code
Paper and cardboard	20 01 01
Glass	20 01 02
Biodegradable kitchen and canteen waste	20 01 08
Solvents	20 01 13* (hazardous)
Fluorescent tubes and other mercury containing waste	20 01 21* (hazardous)
Discarded equipment containing chlorofluorocarbons	20 01 23* (hazardous)
Edible oil and fat	20 01 25
Batteries and accumulators other than those mentioned in 20 01 33	20 01 34
Discarded electrical and electronic equipment other than those mentioned in 20 01 21 and 20 01 23 containing hazardous components	20 01 35* (hazardous)
Discarded electrical and electronic equipment other than those mentioned in 20 01 21, 20 01 23 and 20 01 35	20 01 36
Plastics	20 01 39
Metals	20 01 40
Biodegradable waste	20 02 01
Mixed municipal waste	20 03 01
Bulky waste	20 03 07

2.0.2 Many Local Authorities base waste storage provision on information produced by the Association of Directors of Environment, Economy, Planning and Transport (ADEPT), in their 2010, Making Space for Waste guidance document.

2.0.3 For foot outlets, ADEPT recommend 10,000 litres of waste storage space for every 1,000m². For the welfare and staff space, the metric for offices has been used, which recommends 2,600 litres of waste storage space for every 1,000m² of floor space.

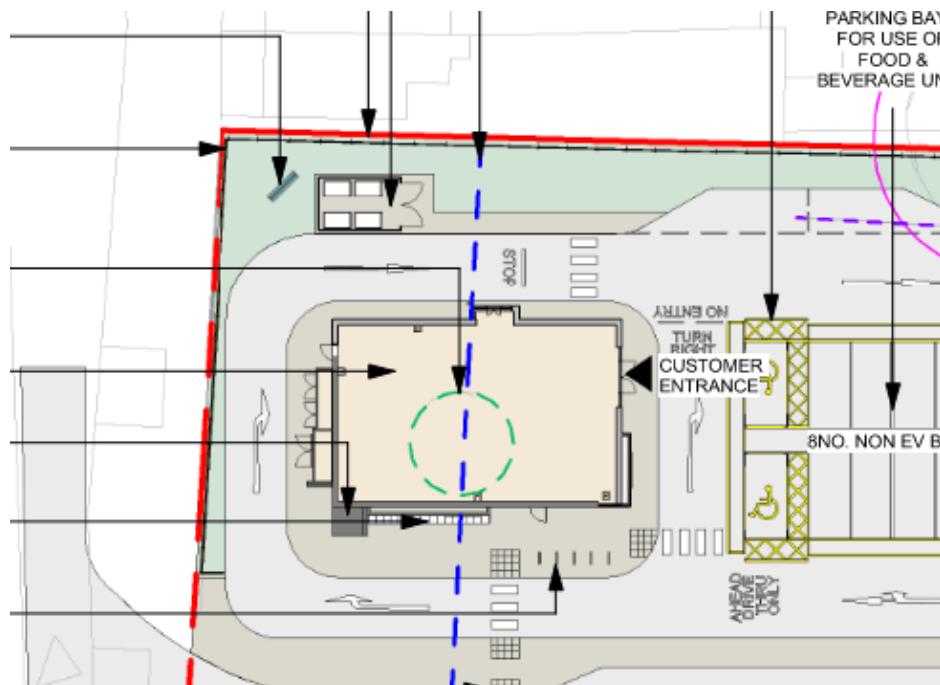
2.0.4 On this basis the food and beverage unit would generate around 1,950 litres of waste per week, and the welfare/ staff space would generate around 121 litres of waste per week. Based on the above, around 2,070 litres of waste per week would be generated. It is considered the quantities of waste that would be brought into the site by customers of the car park would be negligible.

2.0.5 In total at the site, it is considered the allowance of space for a 1,100-litre euro bin for general waste, a 1,100-litre euro bin for recycling, and a 240-litre bin for food waste would provide sufficient waste storage space and would allow for recyclable materials to be separated out from residual waste.

2.0.6 The occupier of the food and beverage unit, a well renowned British business, also has their own specification for bin stores, which includes 24-7 vehicular access to 3 x 1,100 litre euro bins.

2.0.7 A bin store would be located to the west of the food and beverage unit, as shown in the top left corner of the Figure below. The store could accommodate at least 4 x 1,100 litre euro bins (or a higher number of smaller bins if required).

Figure 1 – Annotated Extract of Site Plan, Showing Bin Storage Location



2.0.8 This space has been designed to provide sufficient capacity for all uses at the site. If the occupier of the food and beverage unit was to require 3 x 1,100 litre euro bins, the other uses at the site would have the equivalent space of one further 1,100 litre bin (or a higher number of smaller bins). Based on the stated ADEPT guidance, this amount of space would be sufficient. It is also considered the development has been designed with sufficient space to enable the necessary segregation and collection of the waste streams generated by the development.

2.0.9 The bin storage areas would be demarcated by floor markings and/ or appropriate wall signs to ensure that the area is retained as a waste storage area.

- 2.0.10 The wheeled waste containers would be stored on an impermeable surface. This would ensure that the area can be routinely cleaned.
- 2.0.11 The waste storage area would be sufficiently lit to ensure that users are able to safely access the waste storage area during periods of darkness or low light levels.
- 2.0.12 No flammable materials would be stored within or close to the waste storage areas.

3.0 MOVEMENT & COLLECTION OF WASTE AND RESPONSIBILITIES

- 3.0.1 The waste bins will be collected by refuse collection vehicles (RCVs) from the internal road, immediately adjacent to the bins store. Vehicle tracking has been completed as part of the design of the site. The collection crews will take the bins directly from the store and will take the emptied bins back to the store.
- 3.0.2 A private waste management company would be hired to make the waste collections.
- 3.0.3 The management company at the site are responsible for ensuring standards at the site are maintained, and this would include routine monitoring of surfaces, i.e. for damage, to ensure the bins can easily be wheeled.

4.0 SUMMARY AND CONCLUSIONS

- 4.0.1 This Refuse Management Strategy relates to the proposed development of an electric vehicle car park, at NCP Car Park (Heathrow Flightpath), off the A408, West Drayton, to the north of Heathrow Airport.
- 4.0.2 Whilst the development would not generate significant quantities of waste, this document sets out how waste would be managed at the site.
- 4.0.3 Sufficient waste storage capacity has been designed into the development, and 4 x 1,100 litre euro bins could be accommodated (or a higher number of smaller bins). Sufficient space has been designed to allow for effective recycling.
- 4.0.4 A private waste management contractor would be hired to collect the waste from the site and return the emptied bins to the store. An on-site management company would be responsible for maintenance.
- 4.0.5 The details provided in this Strategy demonstrate that the sustainable management of waste has been given a high priority within the design of the development. It is considered sufficient space has been proposed within the development to allow for effective recycling. The development design would also allow for waste to be transported and collected in a safe and efficient manner.

DRAWINGS

PROPOSED SITE PLAN - PHASE 2: DRAWING NO. 7935-SMR-00-ZZ-DR-A-2004-S3-P12

