

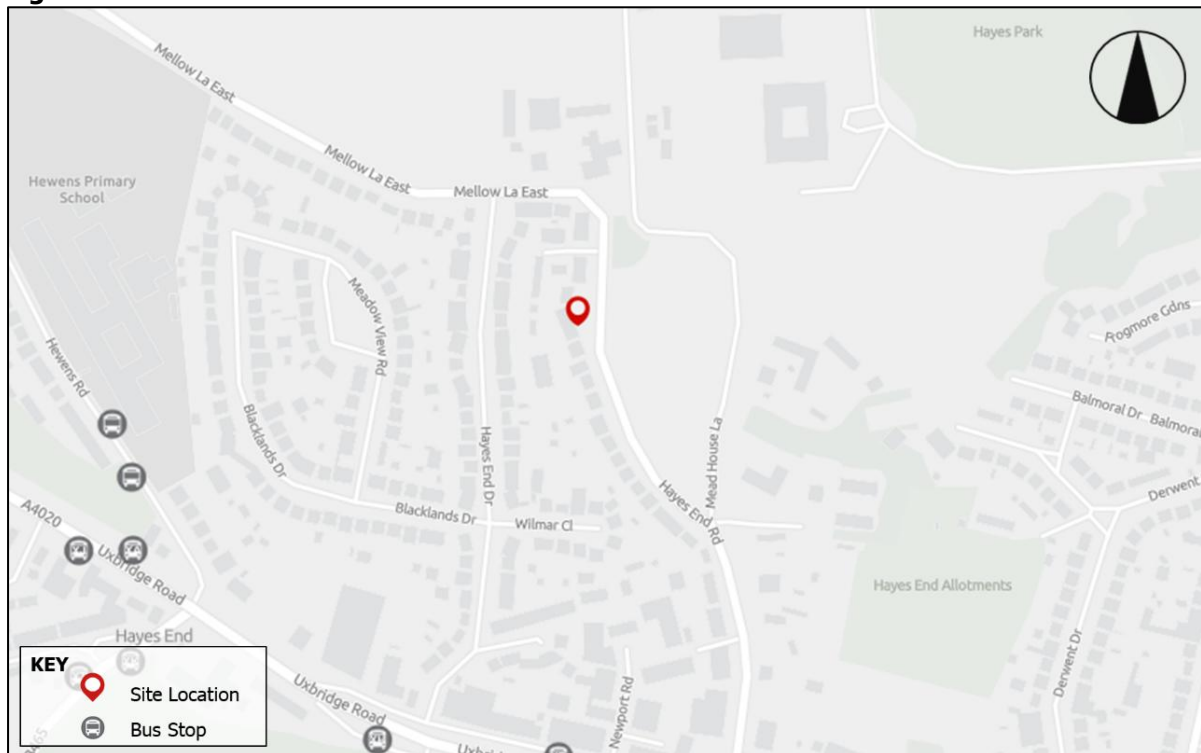
Job No: 2026-5660
File Ref: N01-AC-Parking Survey Results (260519)
Date: May 2026
Job Title: 105 Hayes End Road, Hillingdon

Subject: Parking Note

Introduction

1. TTP Consulting has been appointed to provide highways advice associated with the proposals for 105 Hayes End Road in Hillingdon.
2. This note sets out the findings of the parking survey undertaken on the 12th and 13th of May 2026 and should be read in conjunction with the Transport Statement submitted with the planning application.
3. The site comprises a semi-detached property with parking to the front, which has permission to be converted to create 2 apartments albeit is currently operating as two HMOs with five rooms in each. It is proposed to convert the building into a single large nine-room HMO. The site location is shown in **Figure 1**.

Figure 1: Site Location



Highway Network

4. The site fronts onto Hayes End Road, a single two-way carriageway subject to a 30-mph speed limit, running broadly north to south between the A4020 Uxbridge Road and Mellow Lane East. Hayes End Road is not within a controlled parking zone (CPZ), and there are no on-street restrictions, such as double yellow lines, prohibiting parking in the vicinity of the site.
5. Uxbridge Road lies to the south of the site and is the nearest part of the strategic road network. It provides connections between Shepherd's Bush, Westfield London and M40 Junction 1 (the Denham Interchange).

Parking Survey

6. Surveys were carried out in accordance with a methodology derived by the London Borough of Lambeth, which is commonly used throughout London, to determine the existing level of on-street parking demand, with surveys covering an area broadly within a 200-metre walk of a site on two weekday nights between 12:30 am and 5:30 am when demand for residential parking is usually highest. The surveys were undertaken on Tuesday 12th and Wednesday 13th May 2026.
7. **Table 1** provides a summary of the results, with a copy included at **ANNEX A**.

Street	12 th May 2026 (04:20)			13 th May 2026 (04:10)		
	Parked	Space	%	Parked	Space	%
Dalton Close	12	1	92%	12	1	92%
Hayes End Road	25	37	44%	25	37	44%
Mellow Lane East	10	7	46%	11	7	46%
Hayes End Drive	5	1	80%	6	1	80%
Total	52	46	53%	54	46	54%

8. The survey recorded 52 and 54 cars parked on the two nights with 46 spaces identified as available for parking. The surveys showed that most vehicles were parked in unrestricted areas, although a small number were parked across dropped kerbs. It is assumed that these vehicles are associated with adjacent properties and do not materially affect the survey results. Furthermore, it should be noted that the results include the demand associated with the Site.
9. The survey also recorded the number of cars parked on private driveways and across front gardens. The results which are summarised in **Table 2** recorded a total of 71 cars parked on the Tuesday and 72 cars on the Wednesday.

Table 2: Summary of Driveway Parking (By Street)						
Street	12th May 2026 (04:20)			13th May 2026 (04:10)		
	Parked	Total Spaces*	%	Parked	Total Spaces*	%
Dalton Close	2	2	100%	2	2	100%
Hayes End Road (West)	45	62	73%	45	62	73%
Mellow Lane East	14	16	88%	15	16	94%
Hayes End Drive	10	13	77%	10	13	77%
Total	71			72		

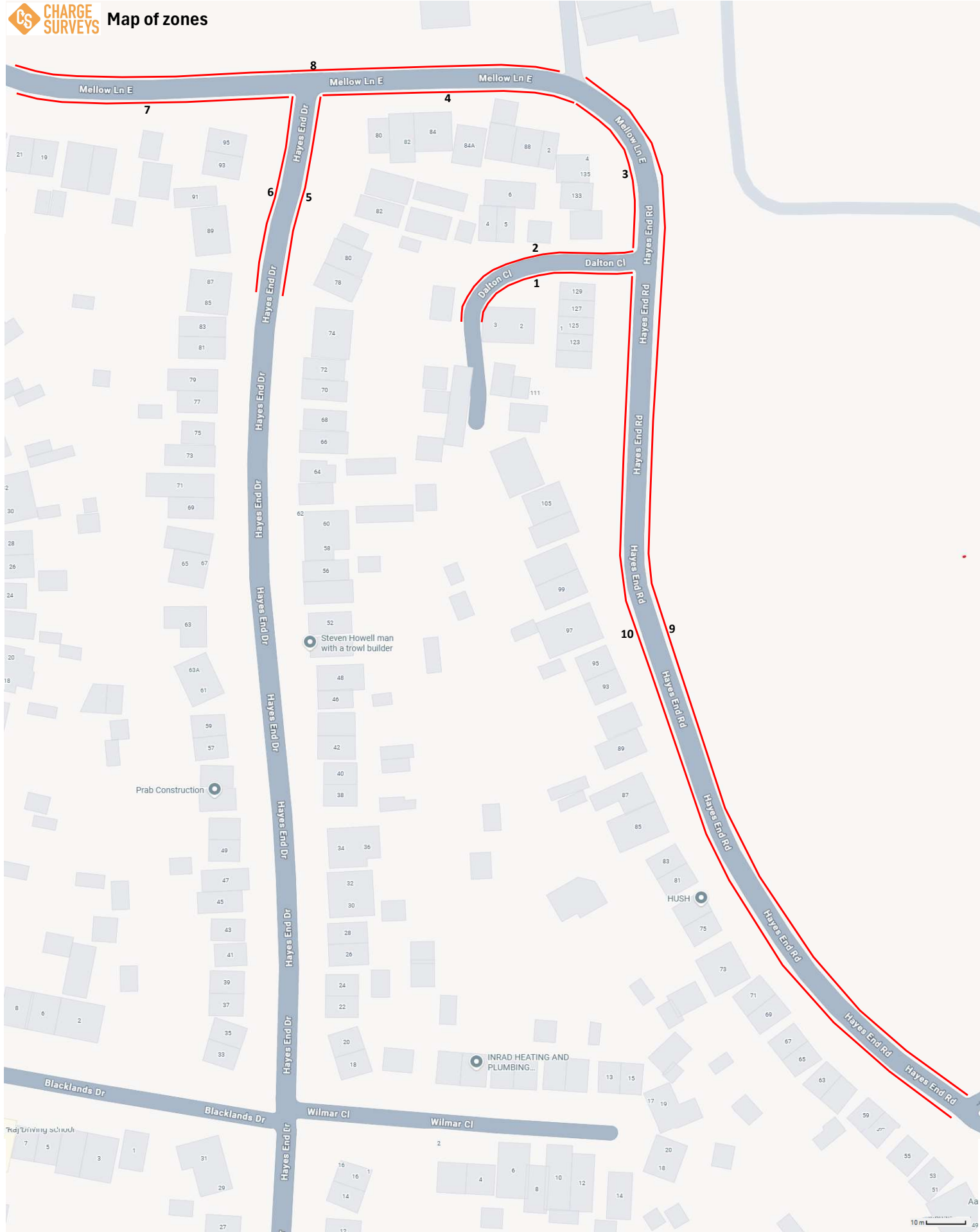
**Approximate count of total available spaces*

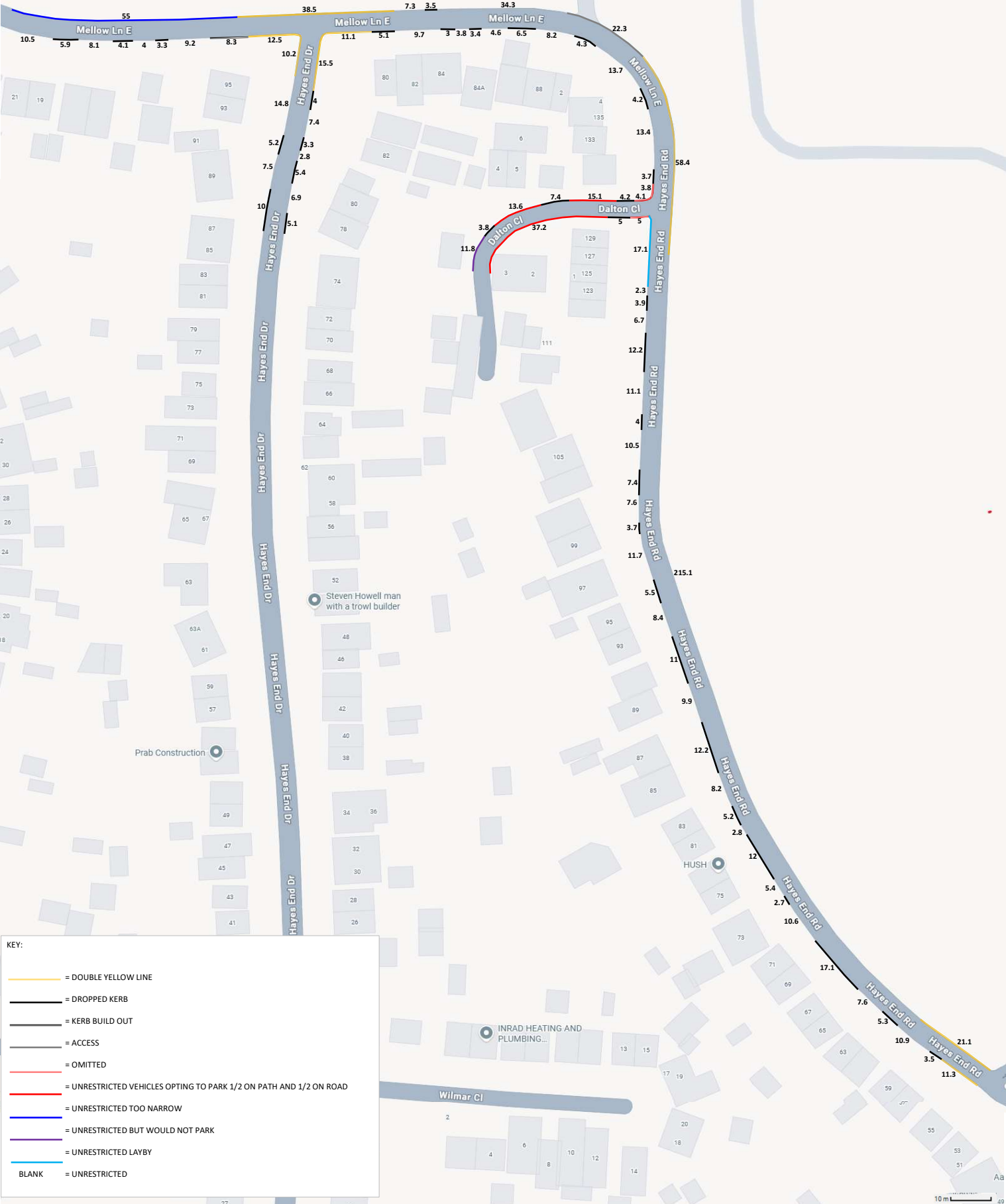
10. The survey results indicate that most properties capable of accommodating driveway or front garden parking make use of these spaces.

Summary

11. The site is a semi-detached property which has permission to be converted to 2 apartments albeit is currently operating as two HMO with a total of 10 bedrooms.
12. Parking surveys were conducted on the 12th and 13th of May 2026 in accordance with the Lambeth methodology to understand existing parking demand overnight. The results which recorded cars parked on driveways in addition to those parked on-street would have included the demand associated with the existing 10 bedroom HMO revealed 46 on-street spaces on both nights.
13. Setting aside that the proposed 9 bedroom HMO would have a lesser demand than that associated with the existing 10 bedroom HMO rooms, there would be sufficient on-street capacity to accommodate additional cars. Furthermore, parking stress levels are considered low at less than 60% overall and less than 50% along Hayes End Road.
14. As such, it can be concluded that the parking associated with the existing (and proposed HMO) rooms does and would not result in any unacceptable impact on the local highway network.

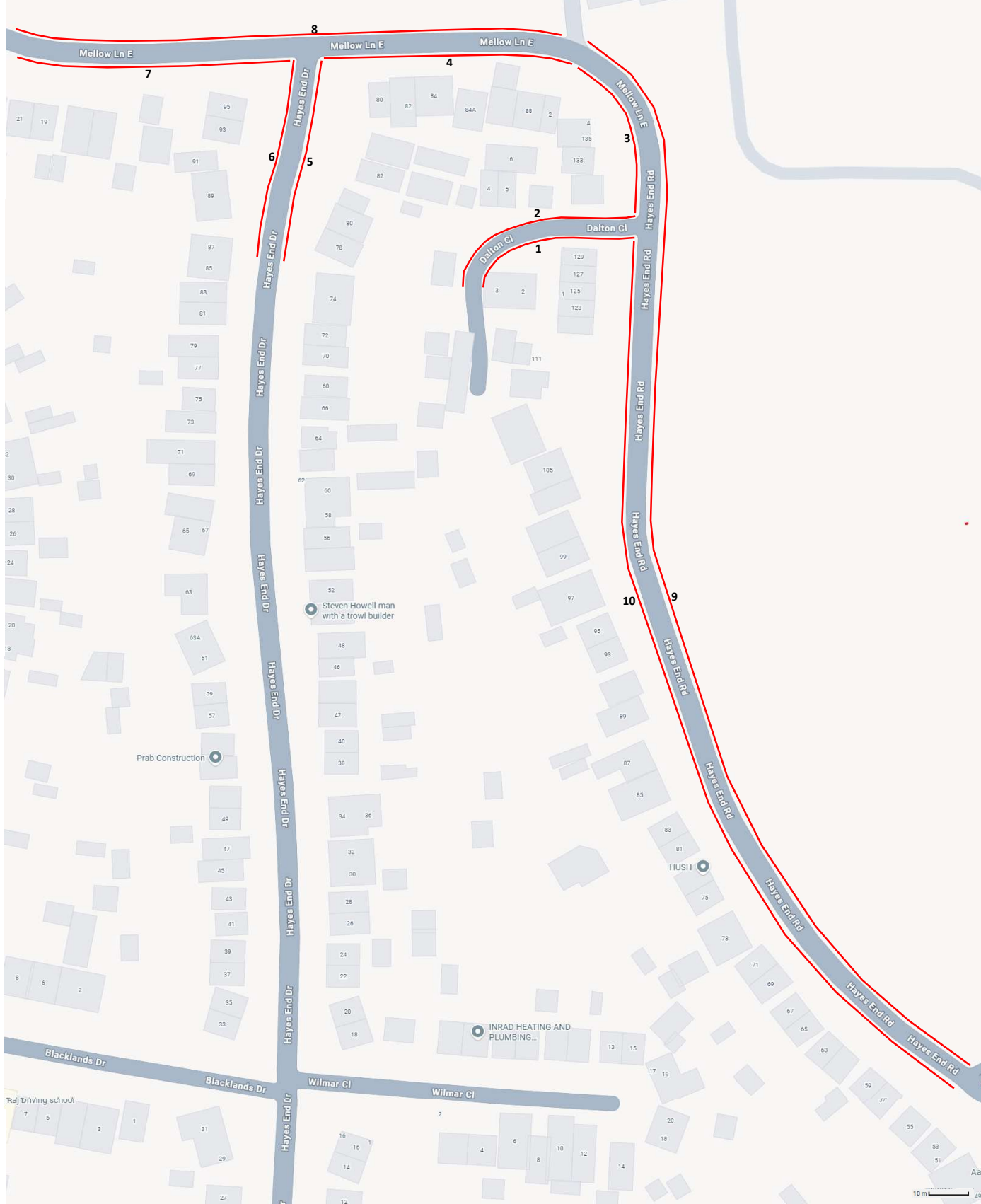
ANNEX A





KEY:

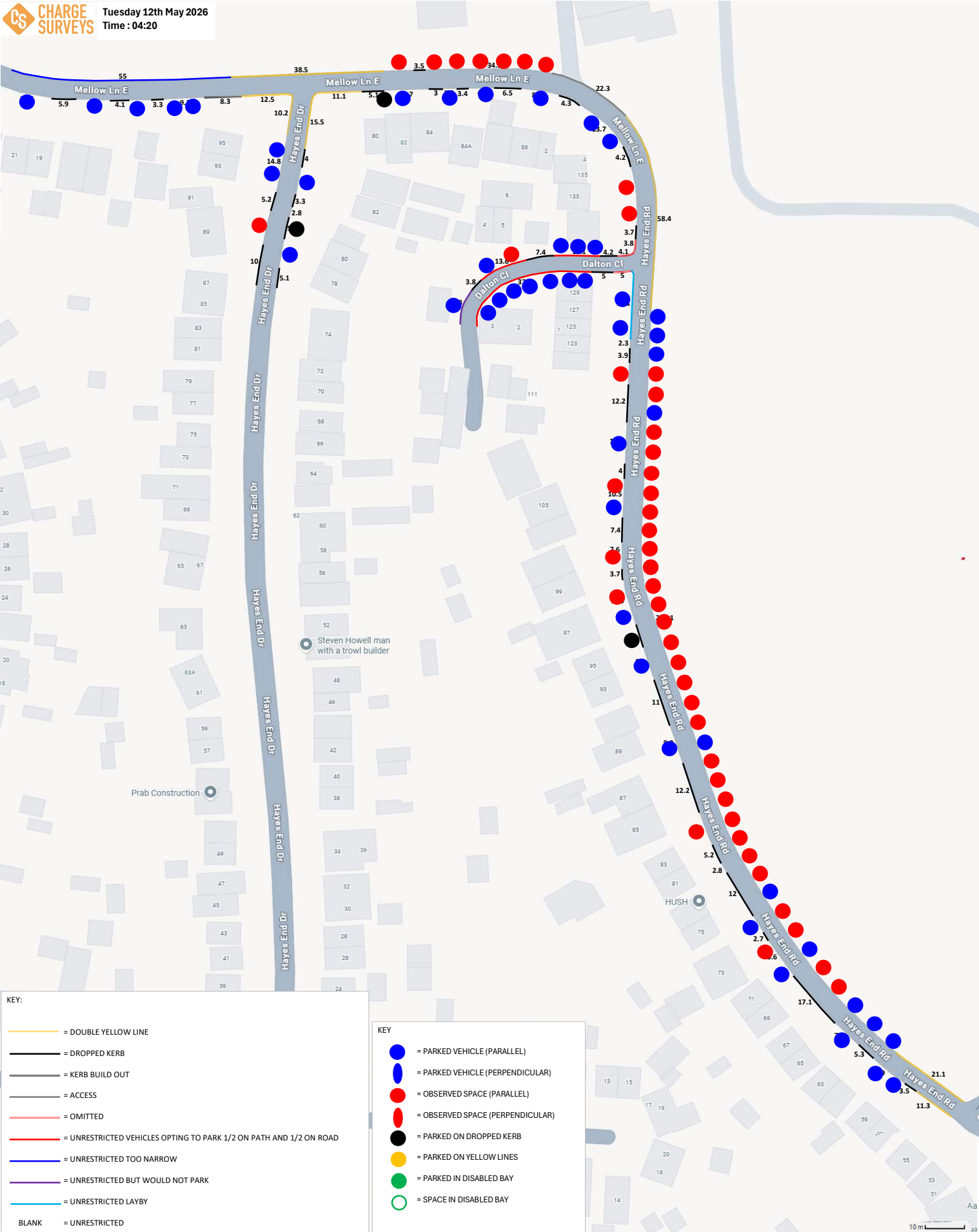
- = DOUBLE YELLOW LINE
- = DROPPED KERB
- = KERB BUILD OUT
- = ACCESS
- = OMITTED
- = UNRESTRICTED VEHICLES OPTING TO PARK 1/2 ON PATH AND 1/2 ON ROAD
- = UNRESTRICTED TOO NARROW
- = UNRESTRICTED BUT WOULD NOT PARK
- = UNRESTRICTED LAYBY
- = UNRESTRICTED



Tuesday 12th May 2026	
ZONE	NUMBER OF VEHICLES PARKED ON PRIVATE DRIVES / FRONT GARDENS
1	none
2	2
3	3
4	8
5	7
6	3
7	6
8	none
9	1
10	41

Wednesday 13th May 2026	
ZONE	NUMBER OF VEHICLES PARKED ON PRIVATE DRIVES / FRONT GARDENS
1	none
2	2
3	2
4	8
5	7
6	3
7	7
8	none
9	1
10	42

Vehicles parked on drives

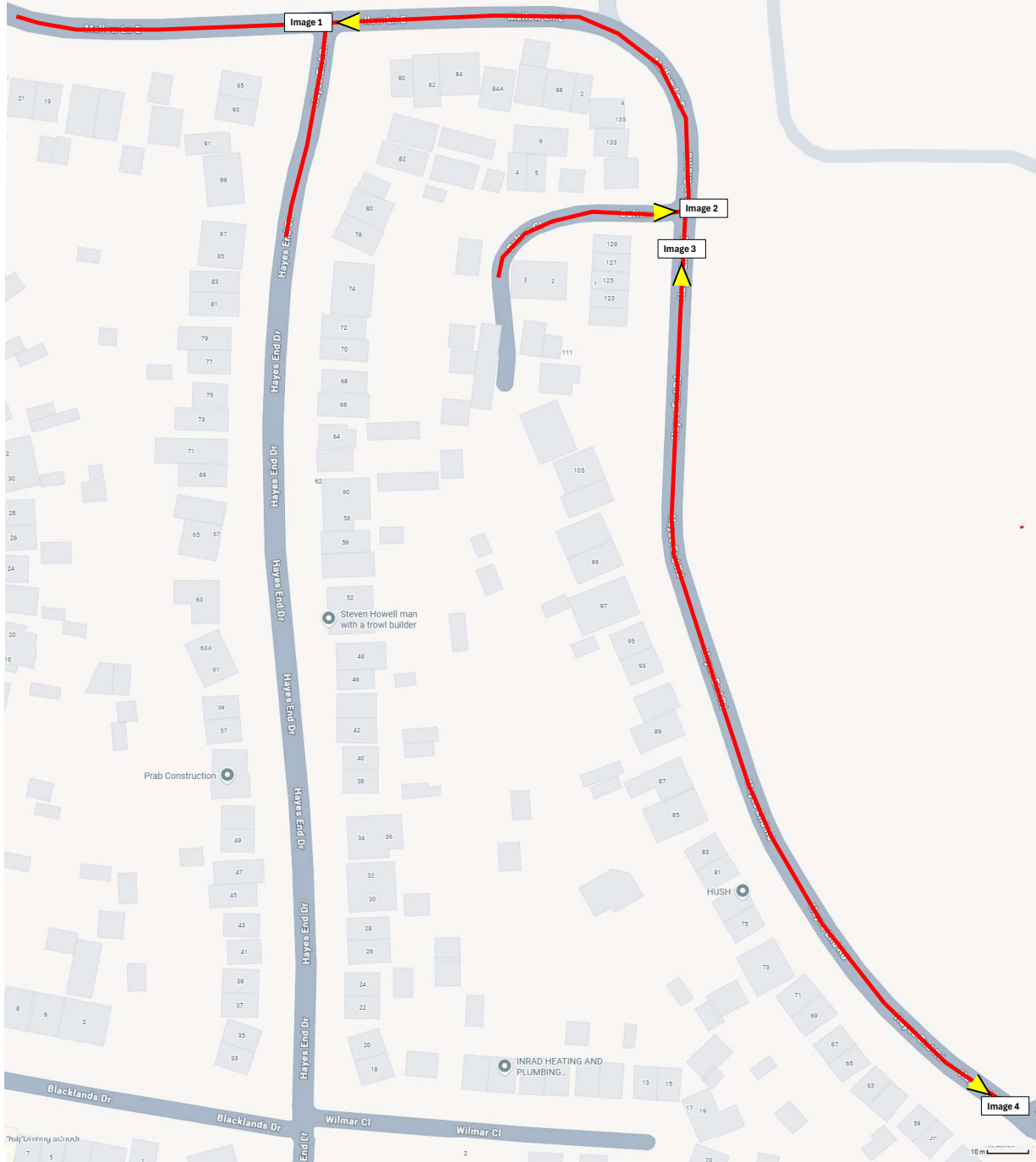


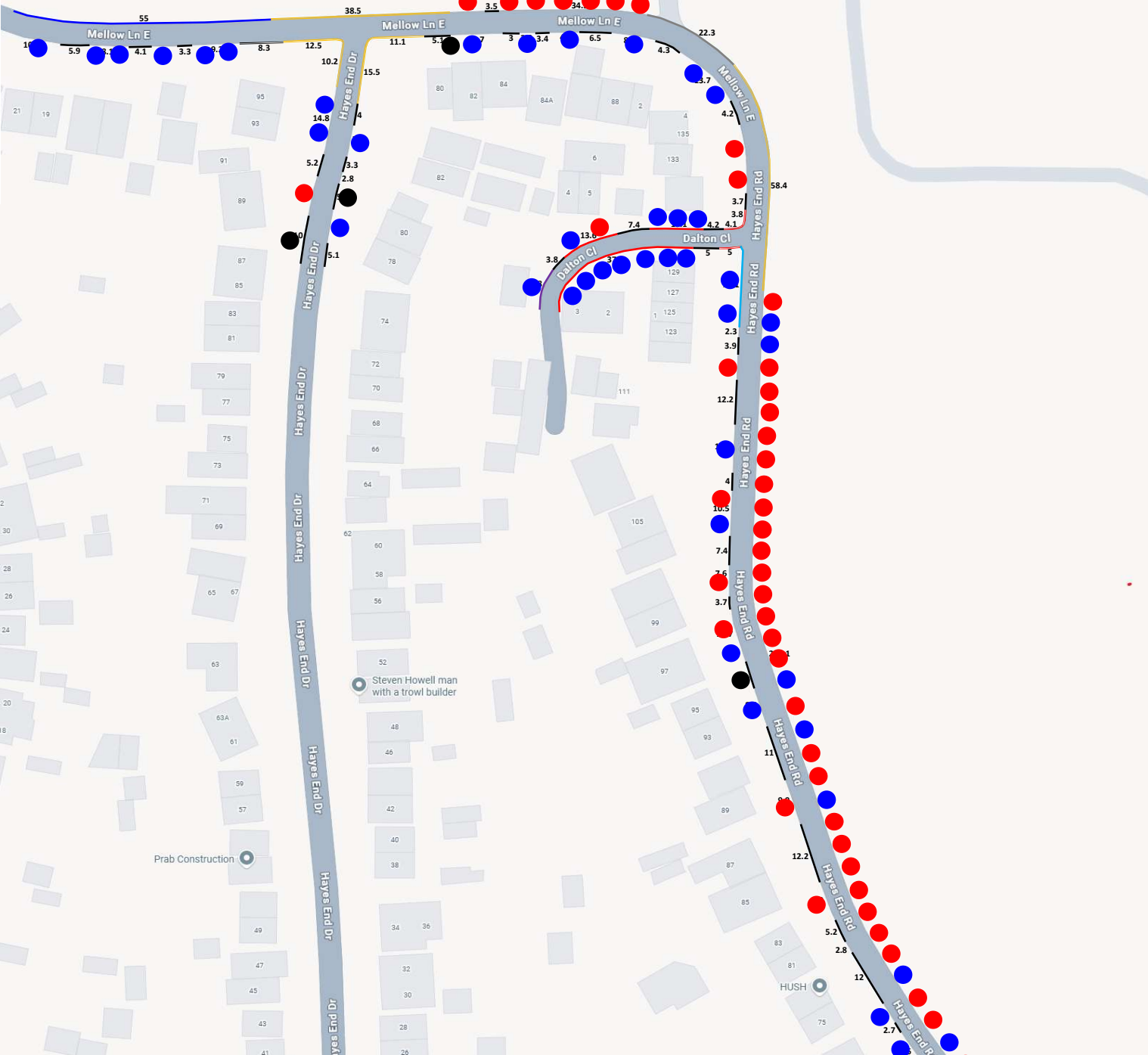
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- BLANK = UNRESTRICTED

KEY

- = PARKED VEHICLE (PARALLEL)
- = PARKED VEHICLE (PERPENDICULAR)
- = OBSERVED SPACE (PARALLEL)
- = OBSERVED SPACE (PERPENDICULAR)
- = PARKED ON DROPPED KERB
- = PARKED ON YELLOW LINES
- = PARKED IN DISABLED BAY
- = SPACE IN DISABLED BAY





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