

DESIGN & ACCESS STATEMENT ADDENDUM 2

Bridge Retail Park, Hayes

Full Planning Application

Document Prepared for:



GRAFTONGATE

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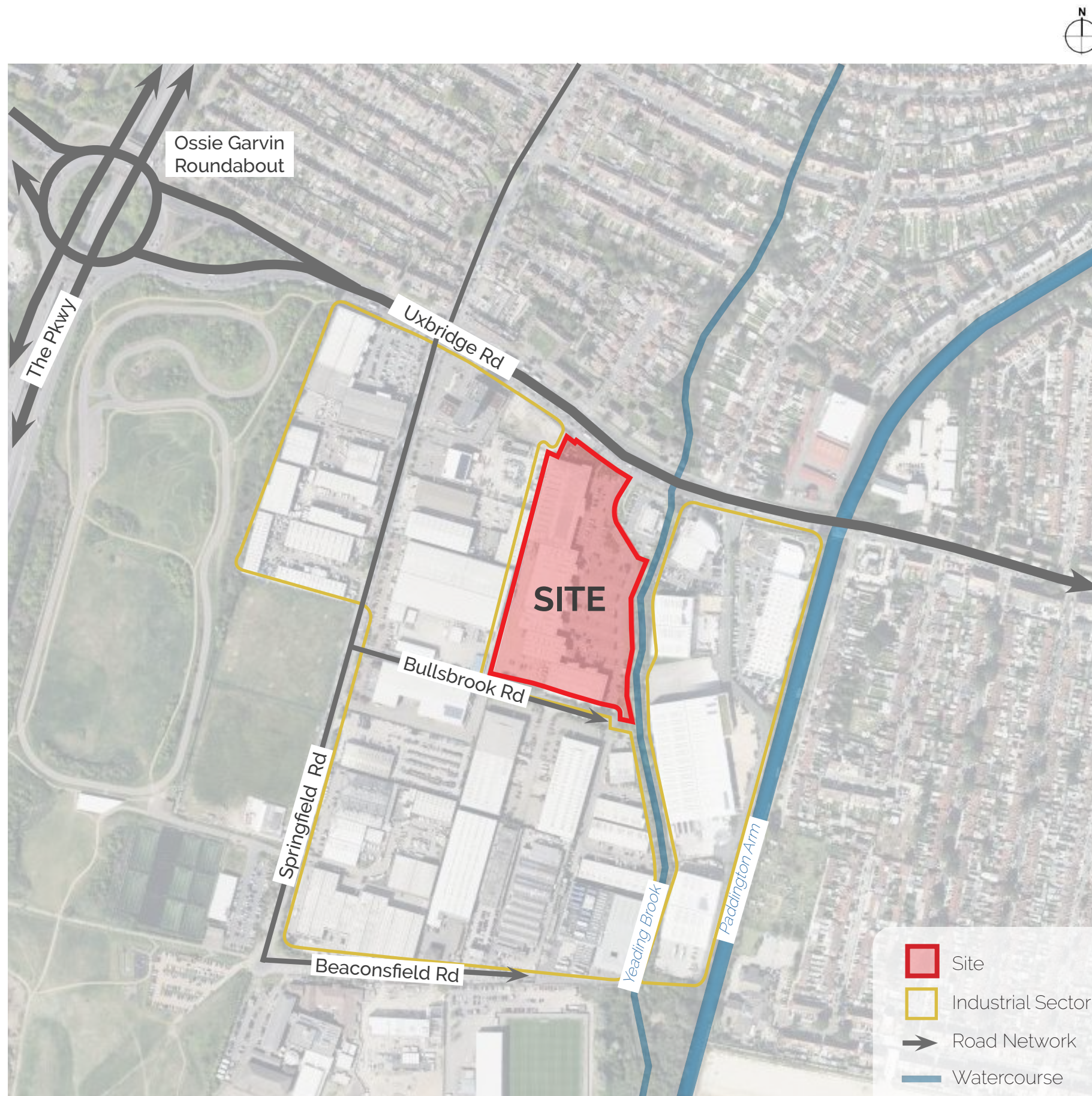
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01 INTRODUCTION



01 INTRODUCTION



Site Location - Immediate Context

01.1 STATEMENT OVERVIEW

This Design & Access Statement Addendum has been prepared by UMC Architects on behalf of OXW Hayes S.à.r.l., in support of the submission of a Full Planning Application for the development of flexible employment space.

This will comprise of warehousing, associated parking, pedestrian and vehicular access arrangements, landscaping and associated infrastructure.

Application Site: Bridge Retail Park, Uxbridge Road, Hayes, UB4

Local Authority: Hillingdon Council

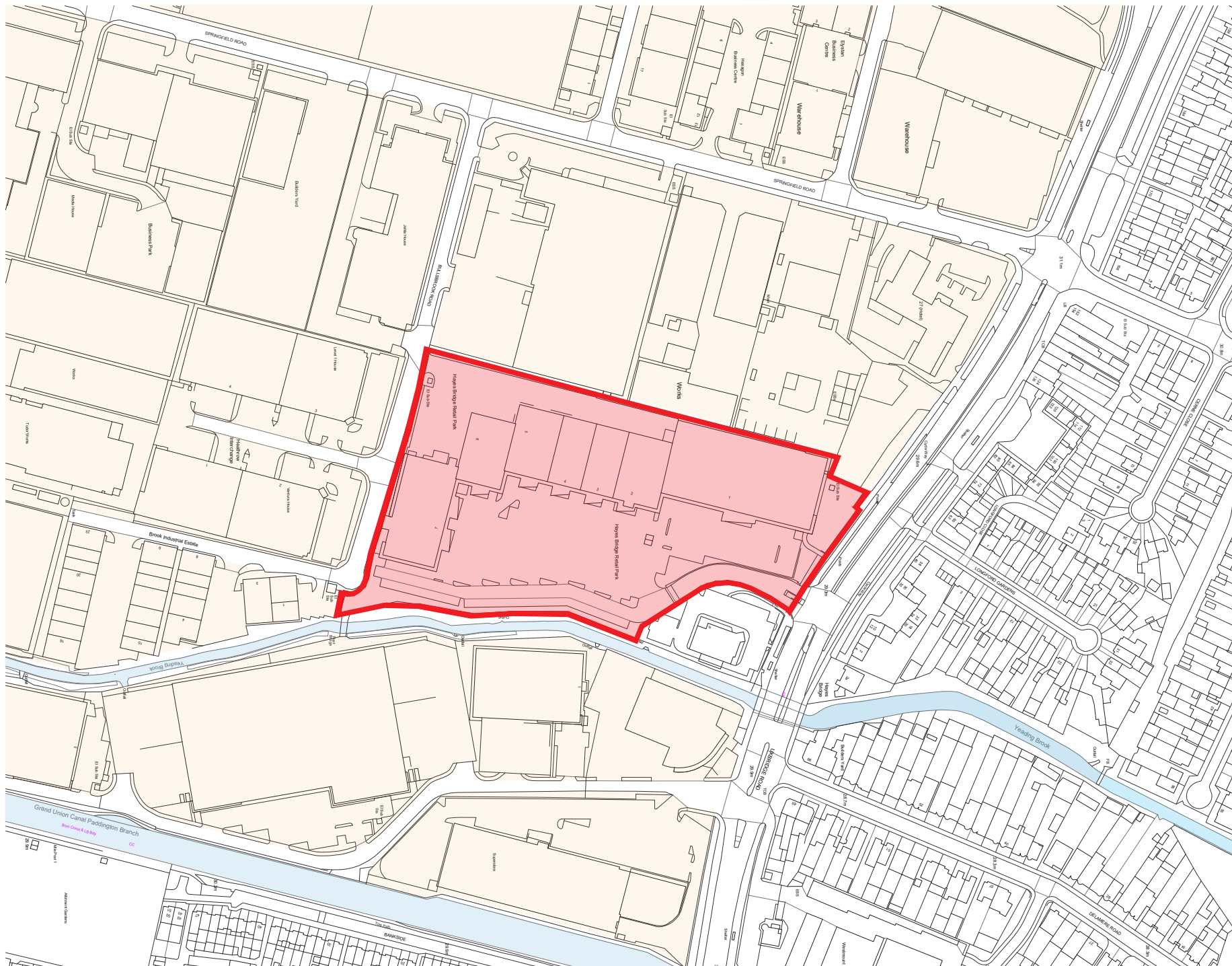
Site Area: 2.88 hectares (7.11 acres)

Proposed Use: E(g)(iii)(industrial) and B8 (Storage or distribution) to form extension to existing industrial area along with ancillary office space, parking and associated works.

Solutions have been implemented to fulfil urban greening ambitions and adhere to institutional standards. The proposal ensures a high level of architectural design which responds to and enhances the local character of Hayes.

This Addendum should be read in conjunction with the Design and Access Statement and not as a replacement to it.

01 INTRODUCTION



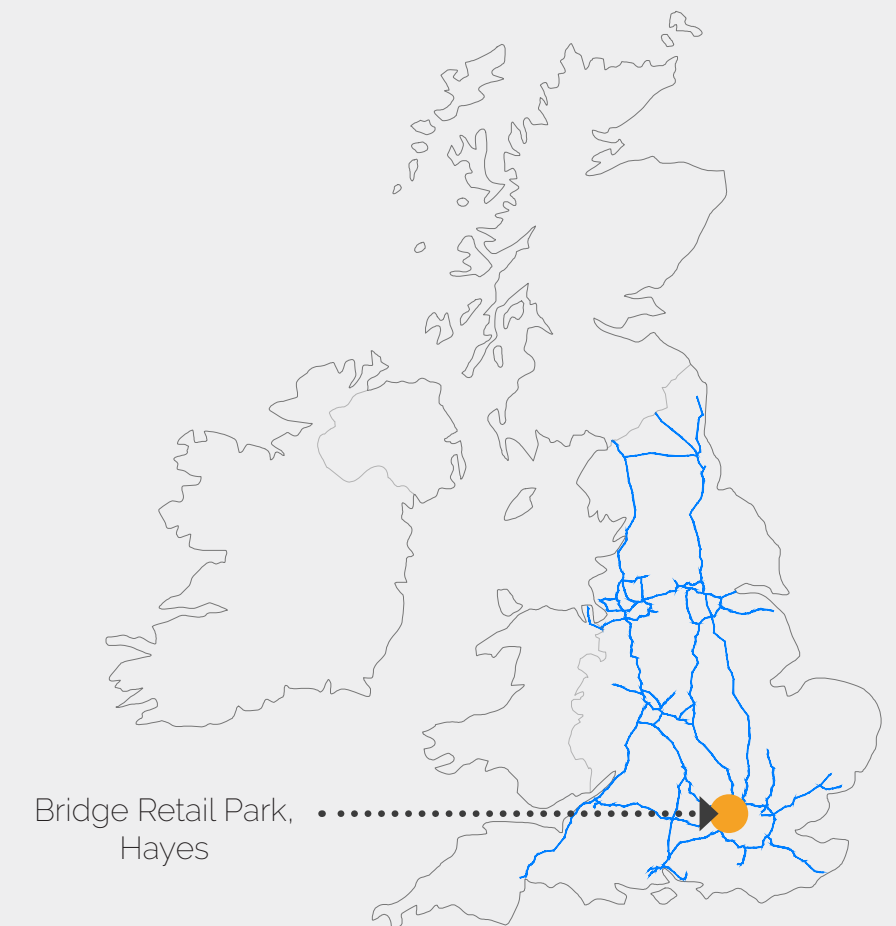
Site Location - Existing Context

01.2 REPORT CONTENT & STRUCTURE

This document aims to explain the underlying design principles that have shaped the progression of the scheme. These principles have led to the development of a proposal that not only aligns with its intended purpose, but also enhances both its immediate and broader context.

Previously, a Design & Access Statement and an Addendum were submitted for this scheme. This second addendum presents a consolidation of elements from both designs, resulting in a cohesive and well-justified design approach.

The addendum will highlight key design changes, addressing both the site layout and architectural aesthetic, with the aim of creating a modern employment space with a welcoming environment.



Site Location - National Context

01 INTRODUCTION



Site Layout Proposal

01.3 EXECUTIVE SUMMARY (SITE LAYOUT)

Following proactive engagement with Hillingdon Council, the proposed master plan (shown), strives to align with the corridor enhancement strategy, urban greening ambitions and place making, whilst ensuring compulsory institutional standards are met.

Key design elements:

- 1 No loss of HGV parking**
Yard area and HGV parking numbers are maintained.
- 2 Office reorientation**
Bridged office design omitted and office reorientated to follow the building line.
- 3 Northern elevation**
Northern elevation extended 3.3m closer to the road.
- 4 Urban greening**
The total green area percentage has increased from **19%** to **21.7%** through the retention and increased addition of landscaping and green roofs.
- 5 Retaining existing green areas**
In order to fulfill urban greening targets, existing greenery is preserved rather than re-purposed for additional parking spaces.
- 6 Car parking re-configuration**
Dead ends have been omitted and the parking quantum is maintained at 91 No. Permeable paving is utilised to improve SuDS provision.
- 7 Worker break provision**
Provision provided through green roof access above the main office, and landscaping maintained adjacent to Uxbridge Road for external seating opportunities.
- 8 Active frontage**
Active frontage spanning the width of the northern elevation creates a welcoming presence along Uxbridge Road, inviting interaction without overwhelming the surrounding residential streetscape.

01 INTRODUCTION



Entrance View Proposal



Entrance View Proposal

01.4 EXECUTIVE SUMMARY (DESIGN)

The proposed elevational treatment offers a cohesive design, responsive to its setting, while enhancing the character of Uxbridge Road.

The office element fronts Uxbridge Road expressing an active modern elevation, while the larger warehouse element is nestled to the rear of the site with a roofscape that manifests and enforces the industrial character of the use and setting.

- 1 Active engagement**
Significant amendments have been made to the building footprint resulting in a layout that actively engages with Uxbridge Road.
- 2 Architectural form**
The architecture is split into **two complimentary styles**: The office element is expressive and modern, referencing architectural styles of adjacent landmark buildings, while the warehouse offers a familiar historic form.

The architectural form, reminiscent of a north light structure, has been kept to articulate the elevation and break down its longitudinal mass.
- 3 'North light' style parapet**
The saw-tooth north light parapet is maintained to warehouse only creating a distinction between warehouse and office provision.
- 4 Roof design**
A standard hipped roof is maintained in the design, The roof light specification is suitable for a building of this orientation and scale.
- 5 Physical appearance**
The green walls on the office have been excluded and substituted with a contemporary metallic cladding feature. The GLA proposed the removal of green wall, citing that urban greening goals can effectively be achieved through dedicated landscaping efforts instead.

02 SITE LAYOUT



02 SITE LAYOUT

02.1 ILLUSTRATIVE MASTER PLAN EVOLUTION - PREVIOUS APPLICATIONS

The scheme has undergone continuous development across previous applications in order to identify the best possible solution for the site. Design features have been thoroughly evaluated, including unit size, orientation and landscaping. The progression of the illustrative proposal's can be monitored using the traffic light system provided below.



Application 1

1



Application 2

2



The Proposal

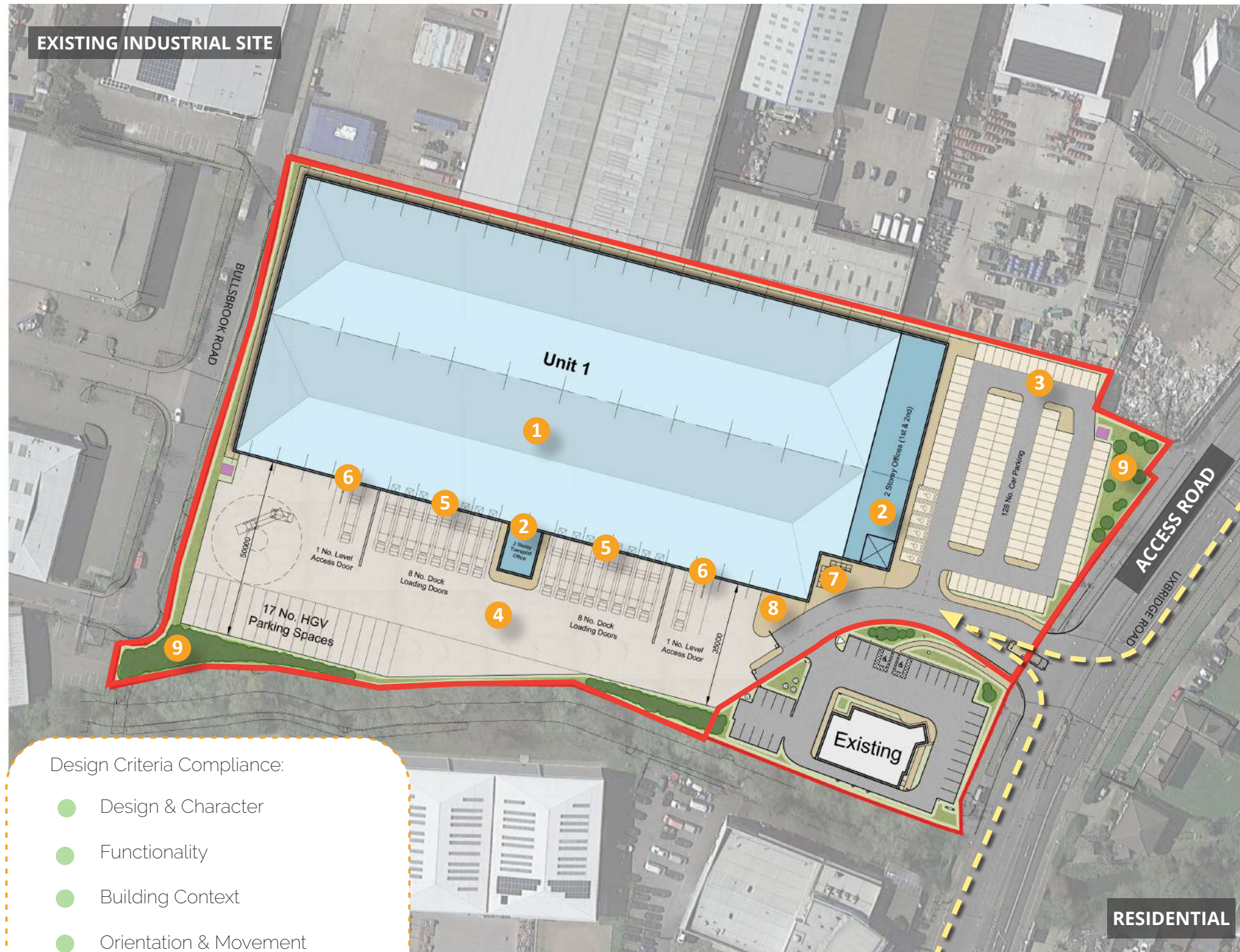
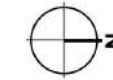
3

- Design & Character
- Functionality
- Building Context
- Orientation & Movement
- Quality of Landscape
- Health & Wellbeing
- Arrival Aesthetic
- Road Presence
- Efficient Car Parking Arrangement

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02 SITE LAYOUT



EXISTING INDUSTRIAL SITE

Design Criteria Compliance:

- Design & Character
- Functionality
- Building Context
- Orientation & Movement
- Quality of Landscape
- Health & Wellbeing
- Arrival Aesthetic
- Road Presence
- Efficient Car Parking Arrangement

Illustrative Plan: Planning Application 1

02.2 SITE LAYOUT (APPLICATION 1)

The initial proposal for this development consisted of a single unit, Unit 1 - **174,030 sq ft (16,168 sq m)**.

The unit is set back to the South-West boundary of the site for privacy and less intrusion to the neighbouring residential. Loading bays, car and cycle parking, ancillary office space and landscaping has been provided. The service yard to all the units are directed into the centre of the development and shielded from view. The frontage is set back for privacy to the neighbouring residential with access only provided from Uxbridge Road.

Existing landscaping is preserved and additional landscaping implemented to front Uxbridge Road.

Key

- | | |
|--------------------------|--------------------------|
| 1 Warehouse | ➡ HGV/Car Primary Access |
| 2 Ancillary Offices | 7 Cycle Shelter |
| 3 Car Parking | 8 Waste Storage |
| 4 Service Yard | 9 Landscaping |
| 5 HGV Loading Dock Doors | |
| 6 Level Access Doors | |

02 SITE LAYOUT



Design Criteria Compliance:

- Design & Character
- Functionality
- Building Context
- Orientation & Movement
- Quality of Landscape
- Health & Wellbeing
- Arrival Aesthetic
- Road Presence
- Efficient Car Parking Arrangement

Illustrative Plan: Planning Application 2

02.3 SITE LAYOUT PROPOSAL (APPLICATION 2)

The proposal for this development site consists of a single unit, Unit 1 - **178,529 sq ft (16,585 sq m)**.

The design evolved based on the feedback received, with the aim of establishing a stronger road presence that fosters engagement, while remaining considerate to the nearby residential buildings.

The design included an extension of offices, with a bridge link connecting the office and warehouse, fronting Uxbridge Road. This established road presence, and provided areas of landscaping to create breaks within the Uxbridge Road streetscape. Offices were positioned within close proximity to the car parking and internal cycling parking for ease of access to building office.

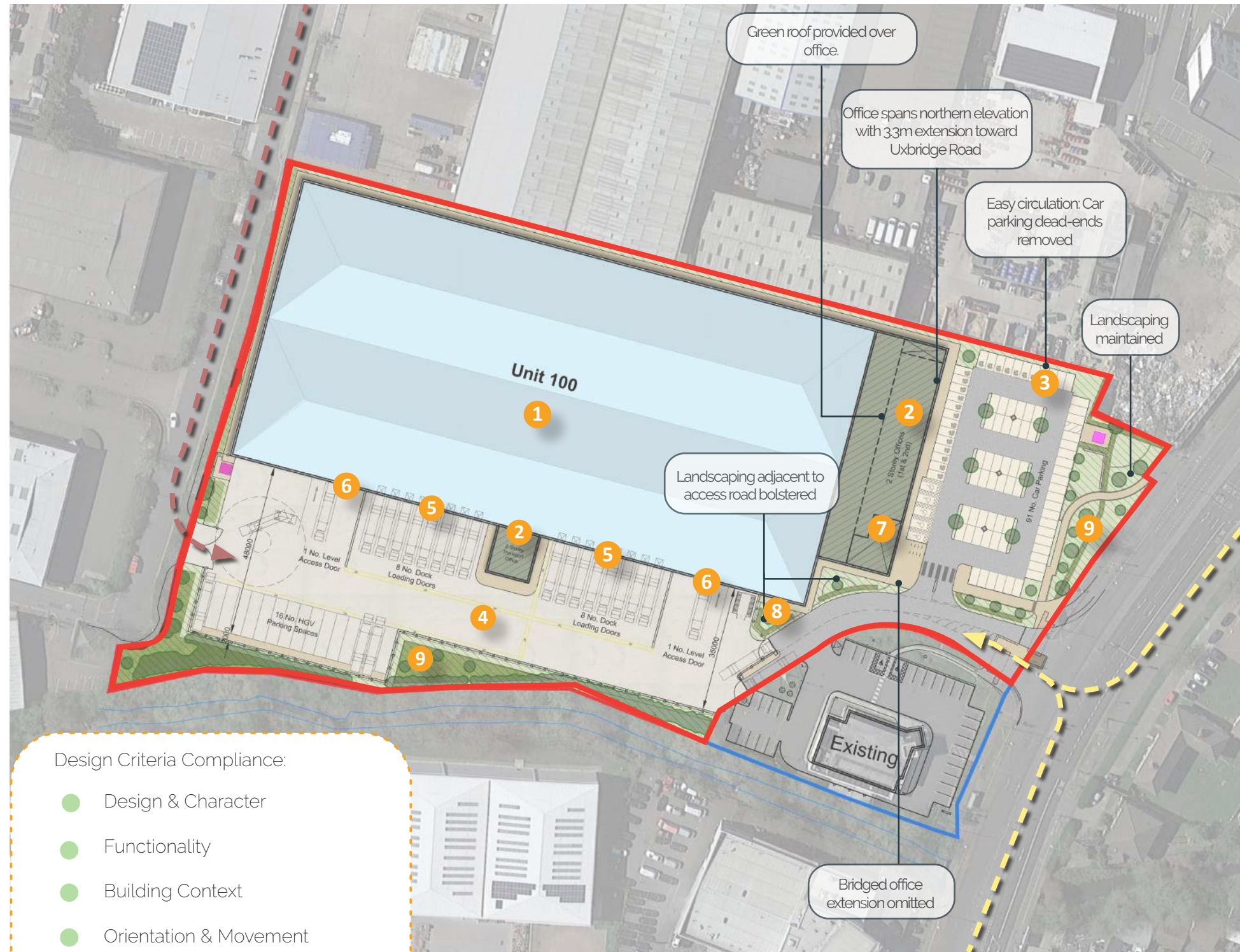
The access road led cars and cyclists underneath the link bridge towards the proposed parking utilities. HGV's have separated access to parking and loading bays, via both Uxbridge Road and Bullsbrook Road, with sufficient circulation space.

Additional landscaping was implemented in gaps where there are no buildings or areas of hard standing. Green walls and green roof features were introduced, increasing the total green area percentage from **4% to 19%**. The parking arrangement was modified to accommodate.

Key

- | | |
|--------------------------|------------------------|
| 1 Warehouse | Design Changes |
| 2 Ancillary Offices | HGV/Car Primary Access |
| 3 Car Parking | HGV Secondary Access |
| 4 Service Yard | 7 Cycle Shelter |
| 5 HGV Loading Dock Doors | 8 Waste Storage |
| 6 Level Access Doors | 9 Landscaping |

02 SITE LAYOUT



Design Criteria Compliance:

- Design & Character
- Functionality
- Building Context
- Orientation & Movement
- Quality of Landscape
- Health & Wellbeing
- Arrival Aesthetic
- Road Presence
- Efficient Car Parking Arrangement

Illustrative Plan: The Proposal

02.4 SITE LAYOUT - THE PROPOSAL

The proposal for this development site will consist of a single unit, Unit 1 - **173,521 sq ft (16,122 sqm)**, illustrated on UMC drawing "21048_P0014[J]_Site Layout"

The site layout has undergone significant changes from the previous proposal, bearing similarities to the first proposal in certain respects.

Notably, the bridged extension of offices fronting Uxbridge Road has been removed and the office provision currently spans the width of the northern facade, increasing its active elevation. Road presence has been addressed through extending the northern elevation forward by 3.3 meters. This design maintains a welcoming presence that encourages interaction, without overwhelming the residential streetscape.

Offices are positioned within close proximity to the car parking and internal cycling parking for ease of access to building office. The revised car parking arrangement has eliminated dead-ends, allowing for smoother circulation and reduced traffic. Separated access to parking and loading bays remain, via Uxbridge Road and Bullsbrook Road.

Additional greenery has been integrated through landscaping and green roof. Landscaping adjacent to the access road has been reinforced and bolstered. This has resulted in the total green area percentage increasing from **19% to 22%**.

Key

- | | |
|--------------------------|------------------------|
| 1 Warehouse | Design Changes |
| 2 Ancillary Offices | HGV/Car Primary Access |
| 3 Car Parking | HGV Secondary Access |
| 4 Service Yard | 7 Cycle Shelter |
| 5 HGV Loading Dock Doors | 8 Waste Storage |
| 6 Level Access Doors | 9 Landscaping |

03 DESIGN

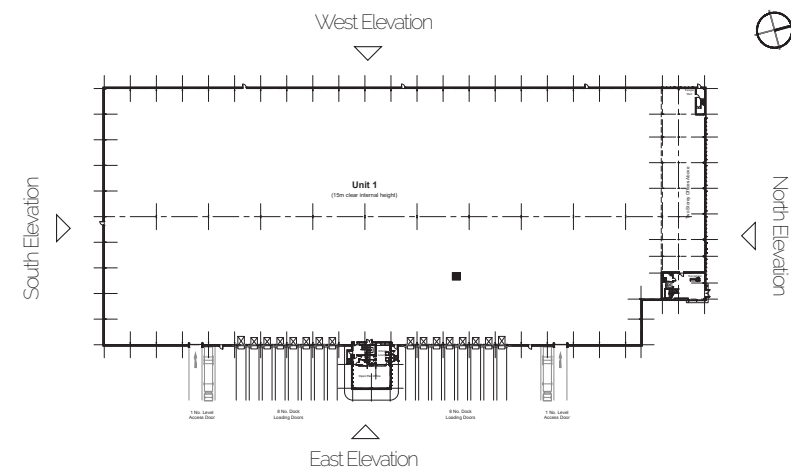


03 DESIGN

PLANNING APPLICATION 1



Proposed North Elevation Scale 1:250

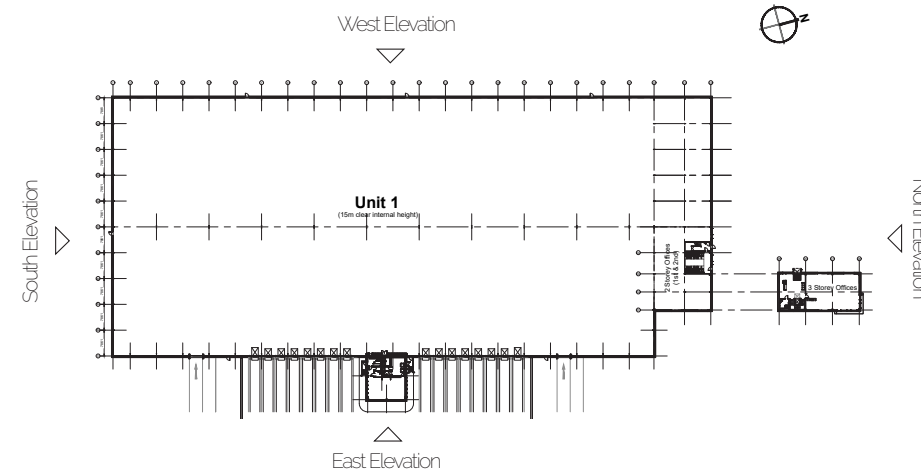


Proposed East Elevation Scale 1:250

PLANNING APPLICATION 2



Proposed North Elevation Scale 1:300



Proposed East Elevation Scale 1:300

03.1 ELEVATIONS - PREVIOUS APPLICATIONS

The elevations on the left indicate the architectural treatment of previous design iterations and how these have developed into the proposal.

APPLICATION 1

This design implements a simple industrial façade with bookends to strategically break up areas of uninterrupted cladding. The vertical hierarchy of cladding colour enhances the building linearity and reduces its perceived scale.

The feedback received suggested the darker colour palette emphasises the heaviness of the form, and cladding lacked visual appeal. In response, the building form evolved to create visual interest and the colour scheme transitioned towards a lighter palette.

APPLICATION 2

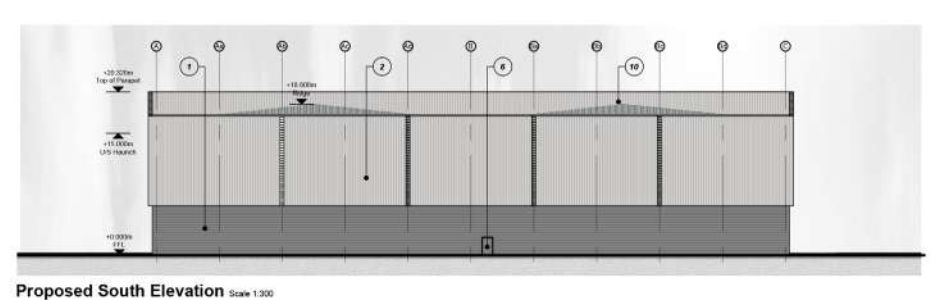
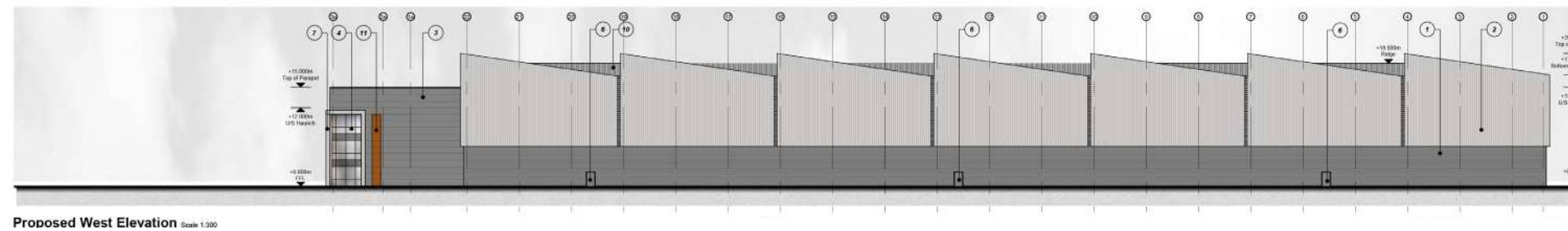
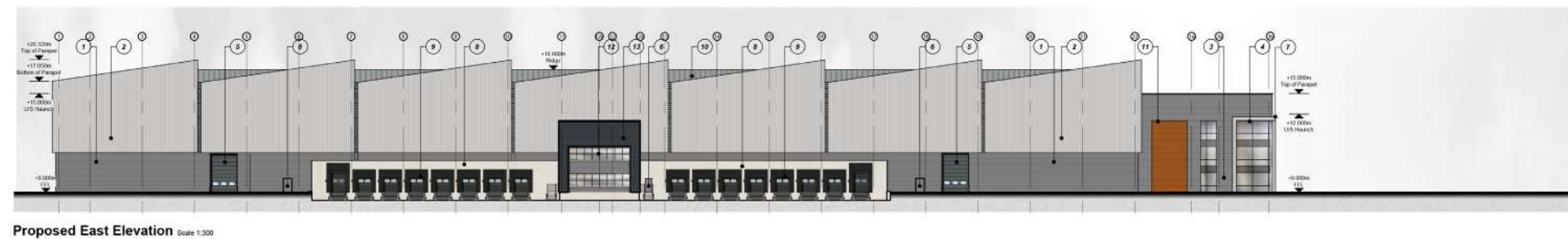
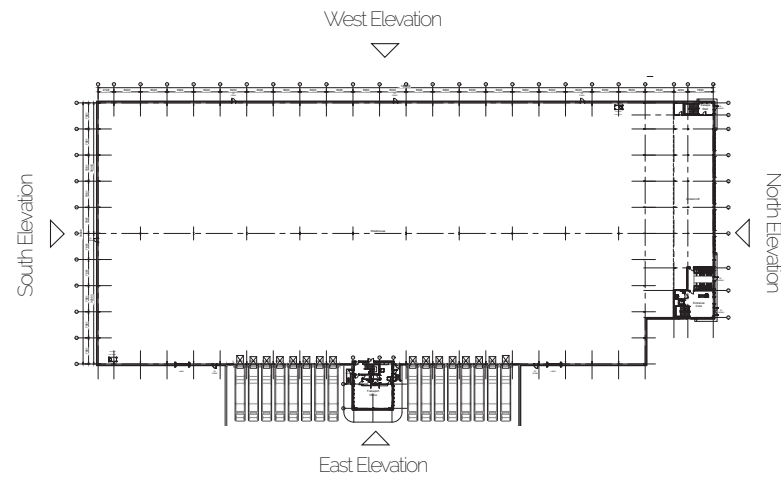
The architecture comprises two complementary styles: the warehouse section features a familiar hipped roof design, while the saw-tooth parapet introduces a modern and distinctive presence within the industrial site.

The bridged offices facing Uxbridge Road create a prominent roadside presence and adds architectural interest.

The elevational treatment has been designed to enhance aesthetic quality, whilst offering excellent longevity and durability. Various cladding profiles and colours has been introduced, along with full-height glazing, to break down the overall uniformity of the elevation. The saw-tooth profile serves to punctuate the longitudinal mass of the elevation into visually comprehensible elements, with a 500mm projection to add further depth to the form.

By incorporating low-level, darker trapezoidal cladding panels, a vertical hierarchy of mass is introduced. This articulates a human-scale dimension to the building's form, resulting in a sense of order, scale, and presence around the site.

03 DESIGN



Key

- | | | | |
|---|--|----|--|
| 1 | Horizontally laid trapezoidal built up wall cladding
Tata Steel Prisma
Colour: Slate Grey | 7 | Feature Entrance
Colour: Oyster (RAL 7035) |
| 2 | Vertically laid trapezoidal built up wall cladding
Tata Steel Prisma
Colour: Oyster | 8 | Pre-cast insulated dock pro-wall system |
| 3 | Horizontally laid colour coated cladding system
with microrib finish
Tata Steel Prisma
Colour: Slate Grey | 9 | Overhead sectional doors with dock leveller
Colour: Anthracite (RAL 7016) |
| 4 | Aluminium curtain walling system
Glazing: Grey tinted
Frame Colour: Anthracite | 10 | Built up roof cladding
Tata Steel HPS200
Colour: Goosewing Grey (RAL 7036) |
| 5 | Overhead sectional doors with level access
Colour: Slate Grey | 11 | Horizontally laid colour coated cladding system
with microrib finish
Tata Steel Prisma
Colour: Seren Copper |
| 6 | Security door & frame.
Colour: to match adjacent cladding | 12 | Aluminium window/door system
Glazing: Grey tinted
Frame Colour: Anthracite |
| | | 13 | Horizontally laid trapezoidal built up wall cladding
Tata Steel Prisma
Colour: Anthracite |
| | | 14 | Horizontally laid trapezoidal built up wall cladding
Colour: Orion |

03.2 ELEVATIONS - THE PROPOSAL

The proposed elevation design evolved in conjunction with the site layout modifications, integrating elements from both prior proposals.

The saw-tooth parapet has been maintained, whilst the bridged office extension has been omitted. The parapet ends at the warehouse with the offices reoriented with the building line and set lower.

The architectural principle is clear: the warehouse presents as a unified form, while the office embodies its own distinct form. Even as these two elements meet, they continue to express their own architectural language.

The office green walls featured in the previous proposal have been omitted. Despite this omission, the urban greening percentage has increased through the addition of strategic landscaping. Instead, the green walls will be replaced by a metallic feature wall.

03 DESIGN

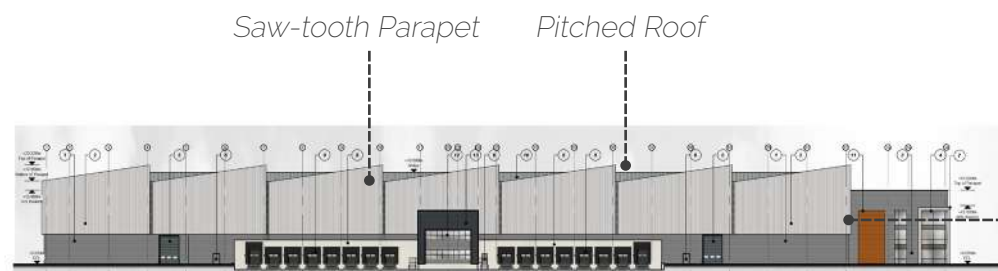


JLR Manufacturing Centre

ROOFSCAPE

The JLR Manufacturing Centre articulates a saw-tooth roofscape supporting a north light solution.

Historically, many industrial units implemented a north light solution to reduce the energy demand for the building. A true north-light solution has been explored. While this approach may be suitable for certain warehouses, this particular scheme features a saw-tooth parapet profile combined with a conventional pitched roof, which would function equally effectively.



The Proposal - East Elevation



JLR Manufacturing Centre Roofscape



The Proposal - 500mm Fin Extension

03.3 ELEVATIONS - PRECEDENT STUDY

The proposed elevational design has drawn inspiration from architectural precedents, such as the Jaguar Land Rover (JLR) Manufacturing Centre.

JAGUAR LAND ROVER MANUFACTURING CENTRE

LOCATION: i54 Business Park | Innovation Drive | Wolverhampton | WV9 5GB

ARCHITECTS: Arup Associates

PROJECT YEAR: 2018

MASSING

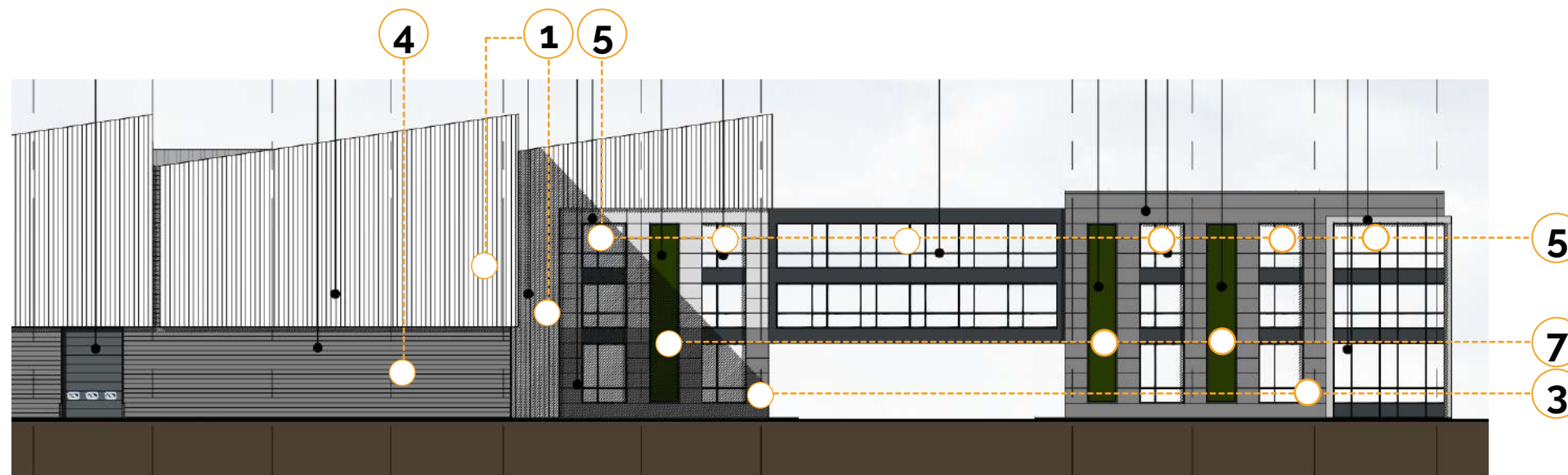
The JLR Manufacturing Centre breaks down the mass of the building by manufacturing spaces into visually comprehensible elements. The darker cladding separating the fins expresses each module as a distinct element. This strategy creates a sense of order, scale and presence around and within the site.

Similarly, the aim for the proposal is to break down the warehouse form into legible elements through its architectural treatment. A vertical hierarchy has been expressed on the façade through low-level darker trapezoidal cladding panels, which articulates a human-scale and connection to the building.

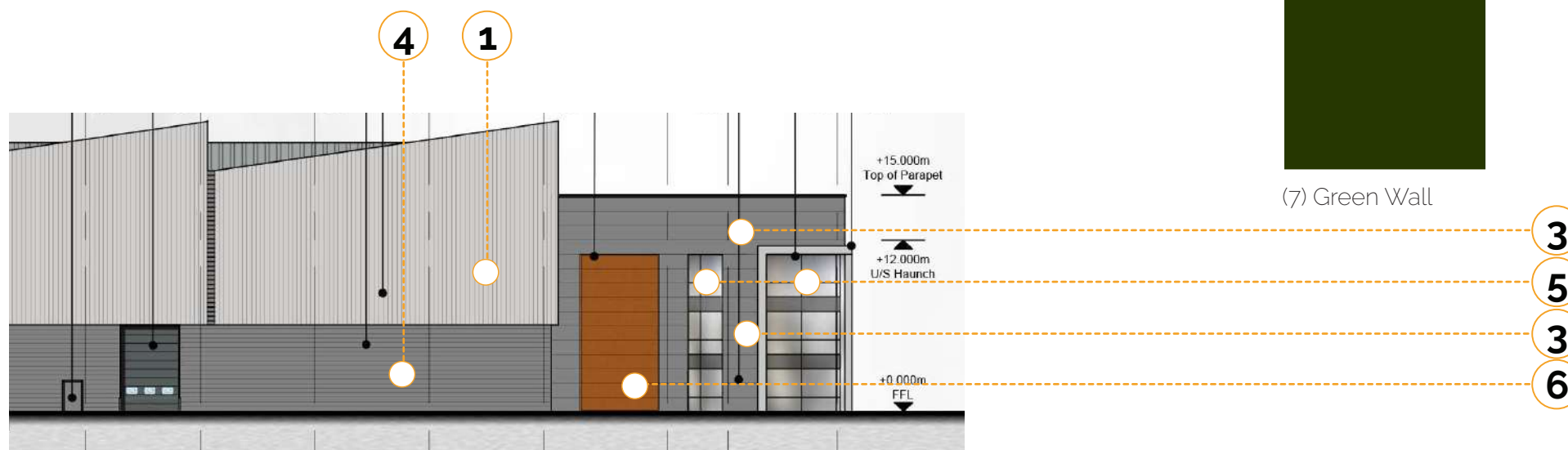
DEPTH OF FORM

The contrast between the light vertical and dark horizontal trapezoidal cladding punctuates the long elevation. Additionally, each fin extends out of the warehouse volume by 500mm to create depth to the form and provide further presence within the site.

03 DESIGN



Previous Proposal

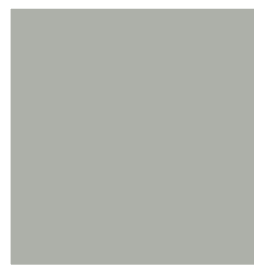


The Proposal

Colour Palette



(1) Vertically laid trapezoidal built up wall cladding
Colour: Oyster



(2) Horizontally laid colour coated cladding system with microrib finish
Colour: Orion



(3) Horizontally laid colour coated cladding system with microrib finish
Colour: Slate Grey



(4) Horizontally laid trapezoidal built up wall cladding
Colour: Slate Grey



(5) Aluminium curtain walling system
Glazing: Clear
Frame Colour: Anthracite,
Spandrel Colour: Anthracite



(6) Tata Steel metallic feature cladding
Colour: Seren Copper

03.4 UNIT APPEARANCE

The office element is expressive and modern, referencing architectural styles of adjacent landmark buildings. A goal post feature detail will highlight the office entrance from the rest of the building to indicate where the access is.

A clear differentiation between the office and warehouse elements is achieved through the saw-tooth parapet ending at the warehouse, with colours and cladding choices further emphasising this distinction.

The GLA proposed the removal of green wall, citing that urban greening goals can effectively be achieved through dedicated landscaping efforts instead.

The offices will be constrained by the floor to ceiling levels, with openings throughout to provide adequate lighting.

03.5 MATERIALS

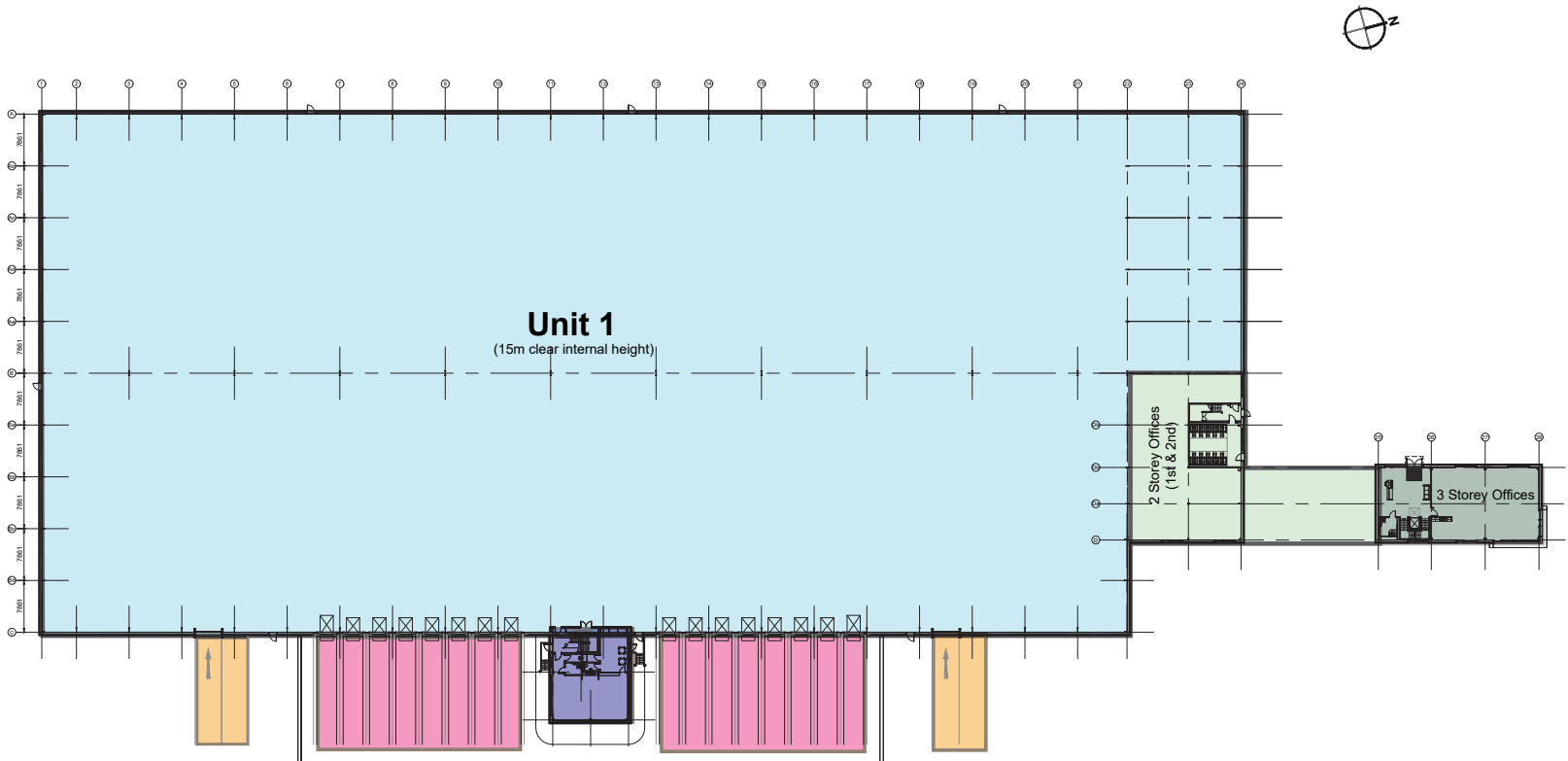
All materials are high quality products for business or industrial applications:

- Silver, metallic or grey colour cladding
- Profiled or flat metal cladding
- Large format window and curtain walling systems
- Feature building elements in strong colours or accent materials
- Translucent glazing, polycarbonates or membrane technology
- Green wall panels

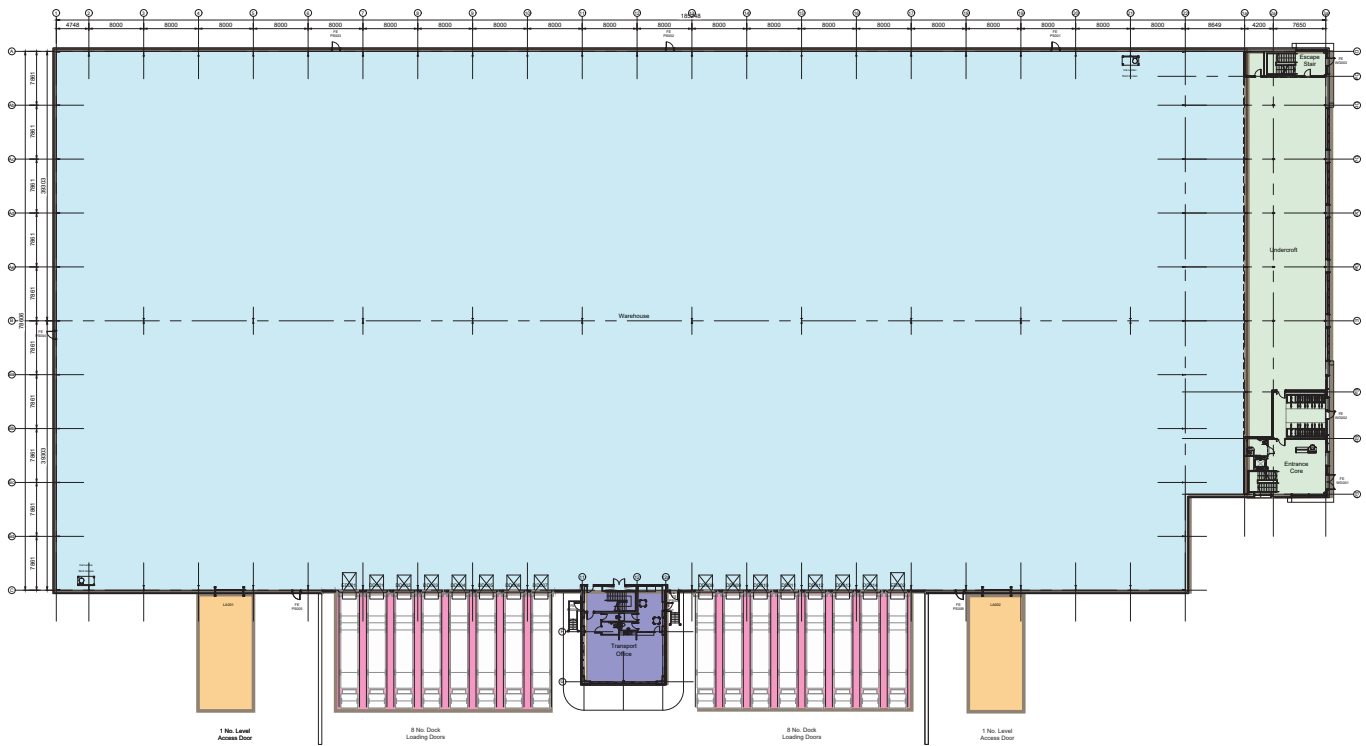
03.6 COLOUR & TEXTURE

The colour chart indicated to the left demonstrates a grey palette varying in hue from white to anthracite. This aims to break up the elevation with contrasting cladding colours. The use of corporate / stronger accent colours will be discouraged to avoid conflict in building appearance, although strong signage zones will be encouraged to promote end user identity.

03 DESIGN



Previous Building Layout Proposal



Building Layout Proposal

03.7 SCHEDULE OF ACCOMMODATION (GIA) PREVIOUS PROPOSAL

The proposal comprises a steel-framed warehouse with 2 storey offices and an extension of 3 storey offices.

Warehouse Area	-	150,553 ft ² - 13,987 m ²
2 Storey Office	-	15,187 ft ² - 1,411 m ²
3 Storey Office	-	8,666 ft ² - 805 m ²
Transport Office	-	3,433 ft ² - 319 m ²
Loading Docks	-	16 (incl. 2 Euro Docks)
Level Access Doors	-	2
Total Building Area (GIA)		177,839 ft ² (16,522m ²)

03.8 SCHEDULE OF ACCOMMODATION (GIA) - THE PROPOSAL

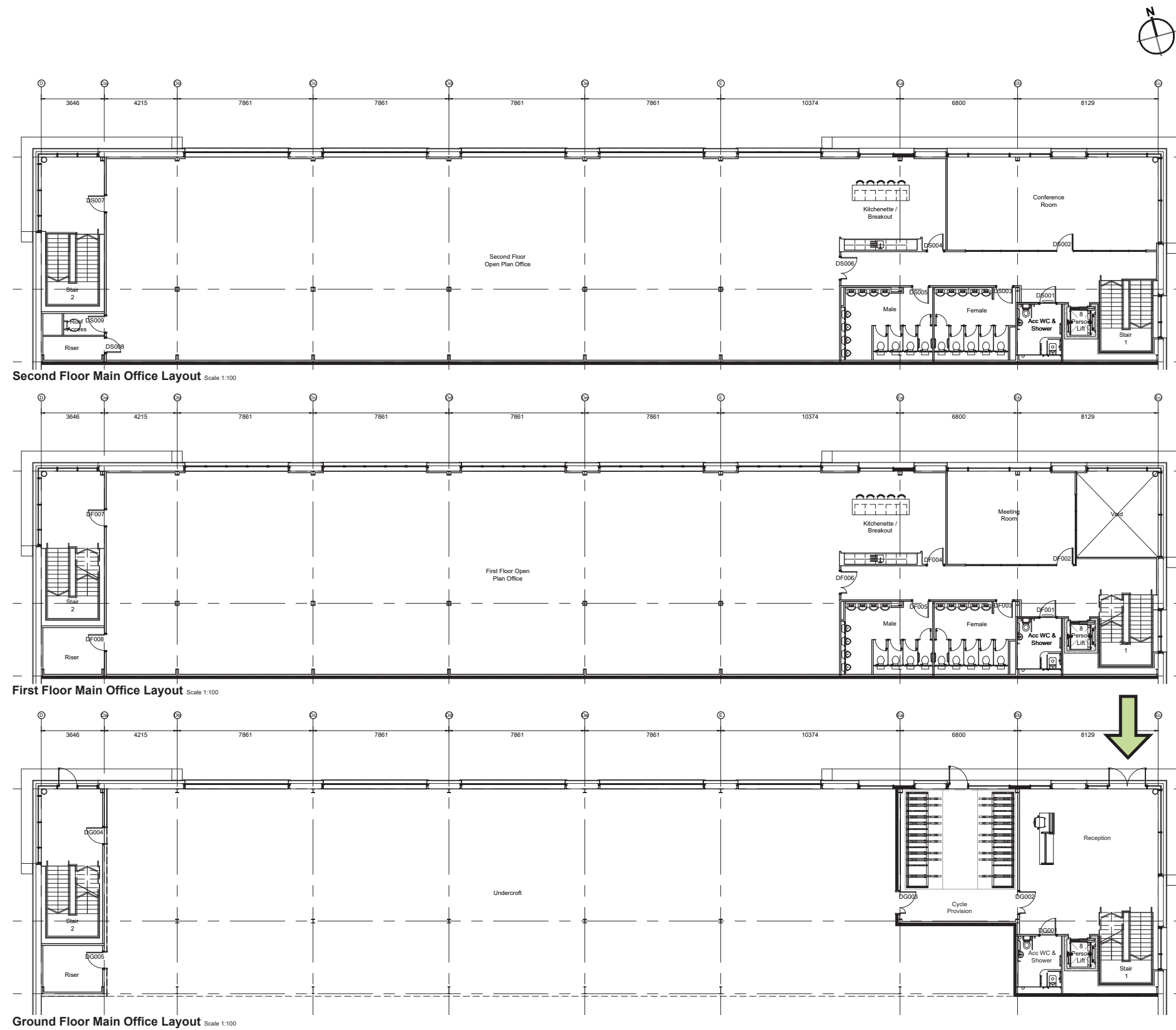
The proposal comprises a steel-framed warehouse with 2 storey offices fronting Uxbridge Road. This is sized to suit the operational requirements of the occupier. The application seeks to provide 14,095m² of warehousing internal floor area, in addition to 1,731m² of associated office and welfare accommodation.

The HGV yard is dimensioned to accommodate modern articulated vehicles and their turning circles.

The proposed site layout submitted as part of this application confirms the proposed floor areas as listed in the schedule of accommodation below:

Warehouse Area	-	151,712 ft ² - 14,095m ²
2 Storey Office	-	18,628 ft ² - 1,731 m ²
Transport Office	-	3,181 ft ² - 296 m ²
Loading Docks	-	16 (incl. 2 Euro Docks)
Level Access Doors	-	2
Total Building Area (GIA)		173,521 ft ² (16,122m ²)

03 DESIGN



Proposal Office Layout

Internal layouts for information only

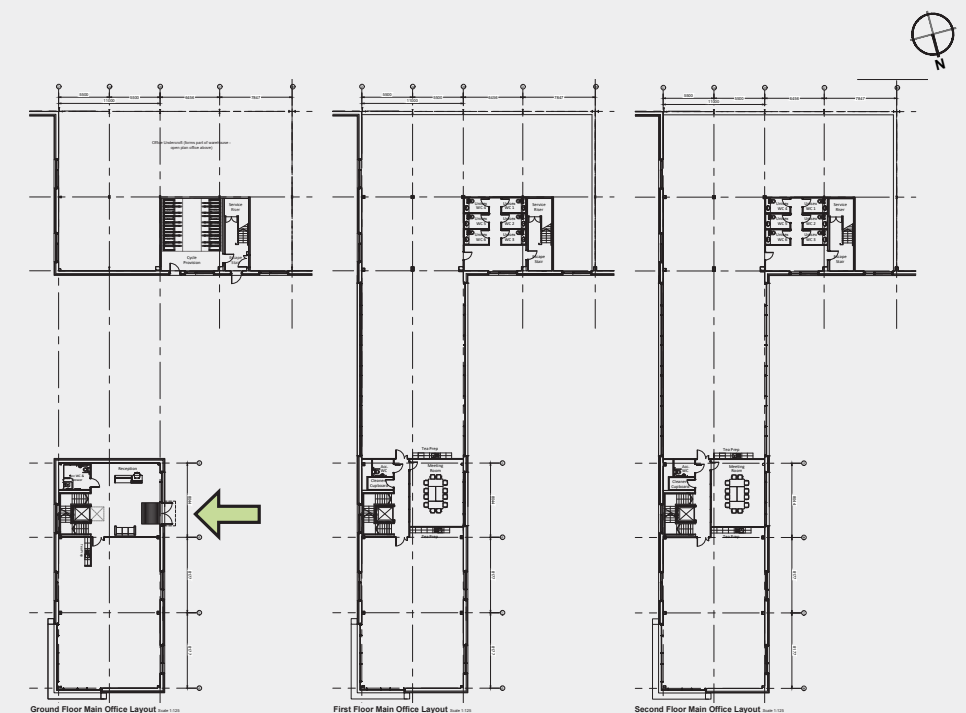
03.9 ANCILLARY OFFICE LAYOUTS

The office layouts have been rearranged and adjusted to suit the new building footprint. The office core is positioned at the corner and easily accessible from all areas around the site.

Provisions for unisex and accessible WC's, cleaner stores, tea prep areas and circulation space have been provided. The sizes of the areas are derived from the needs of the end user.

A conference room at first and second floor level has been provided within the core, with window partitions to allow abundant light to filter through to the circulation space.

Cycle parking provision is to be provided internally - with a two tier pull out cycle rack system.



Previous Proposal Office Layout

04 SUMMARY



04 SUMMARY

04.1 SUMMARY

This addendum demonstrates a well designed, high-quality scheme that will provide desirable employment and become an asset to the local area. It is anticipated that the scheme will act as a catalyst for new job opportunities and bring valuable investment to the community. The development supports employment growth, contributing to economic regeneration in line with regional and local aspirations.

Significant amendments have been proposed to the overall site design and elevational treatment, striving to create a cohesive justified scheme that actively engages with Uxbridge Road.

The proposal aligns with the corridor enhancement strategy, urban greening objectives, and placemaking principles, while ensuring compliance with mandatory institutional standards. The previous proposal offered 19% site green area (including, green walls, green roof and landscaping). Through active engagement with Hillingdon Council and continuous design refinement, this percentage has increased to 22%. This was facilitated by the implementation of additional landscaping and green roofing.

A revised architectural form which reorientates the offices to follow the building line is proposed. The saw - tooth parapet design has remained and aims to articulate the modern elevation from both near and distant perspectives. This profile aims to break down the length of the building to provide a legible roofscape that enforces its industrial character and setting.

The architectural principle is clear; the warehouse reads as one form and the office another. Although they meet, they express their own language.

The design aims to minimise impact on surroundings, offering an appealing, well-connected, modern design that serves as a functional and safe space for all users.

All design proposals contained within this report have been designed to meet the required design and sustainability policies of Hillingdon Council.

04 SUMMARY



Entrance View Proposal

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