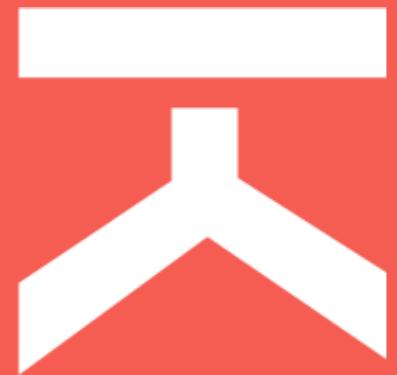


Design and Access Statement



LOCATION

Paddington Packet Boat
High Road, Uxbridge, UB8
2HT

CLIENT

Paddington Packet Boat
Developments Ltd

PROJECT NUMBER

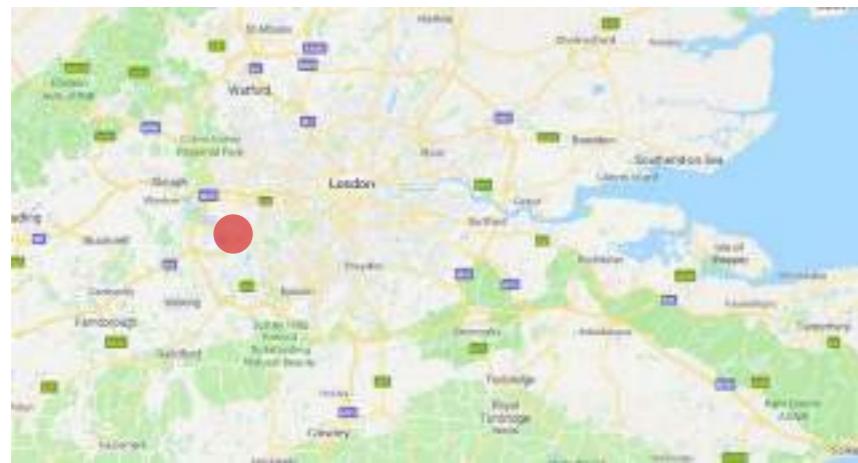
105

DOCUMENT NUMBER

01

SITE ADDRESS:

Paddington Packet Boat
High Road,
Uxbridge, London, UB8 2HT



This Report should be read in conjunction with the following:

- Application Drawings
- Planning Statement
- Transport Assessment
- Travel Plan
- Viability Statement
- Heritage Statement
- Archaeology Statement
- Energy Statement
- Sustainability Statement
- Air Quality Assessment
- Noise Impact Assessment
- Drainage Statement
- Land Contamination Desktop Study
- Sunlight and Daylight Assessment

Client:
Paddington Packet Boat Developments Ltd
Ground House, 2-3 Little Burrow, Welwyn Garden City,
AL7 4SP

Architect:
Benchmark Architects
90 Dunstable Street, Ampthill, Bedfordshire, MK45 2JR

Consultant Team:

Planning Consultant - Nexus Planning
Holmes House, 4 Pear Place, London SE1 8BT

Transport & Drainage - Cotswold Transport Planning
13 Orchard Street, Bristol, BS1 5EH

Energy & Construction Logistics - Waterman Group
Pickfords Wharf, Clink Street, London, SE1 9DG

Heritage - Corrie Newell Historic Buildings Consultancy
90 Highfields, Great Yeldham, Halstead, Essex, CO9 4QH

Air Quality - Air Quality Consultants
North West House, 119 Marylebone Road, London, NW1
5PU

Land contamination - HydroGeo
Waddington House, Llanover Business Centre, Llanover,
Abergavenny, NP7 9HA

Noise Impact - Cole Jarman
John Cree House 24b High Street, Addlestone Surrey,
KT15 1TN

Viability -James R Brown & Co Ltd
38 The Dene Sevenoaks Kent TN13 1PB

Sunlight Daylight - Schroeders Begg
Vox Studio, Unit 410, 1-45 Durham Street, London SE11
5JH

Fire strategy - Bureau Veritas
Bureau Veritas UK | Atlantic House, Atlas Business Park,
Wythenshawe, Manchester, M22 5PR

Author:
Anish Mistry / Lindsey Mcilvaney

Checker:
Mark Doohan

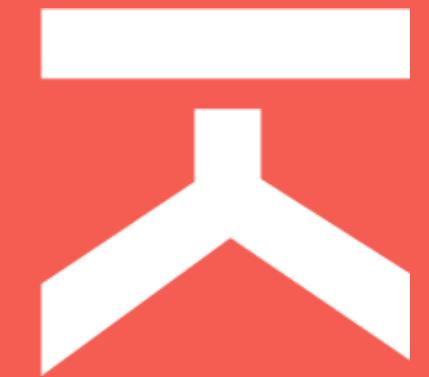
105 -Design and Access Statement

First Issue:	9th November 2018
REV A:	6th December 2018
REV B:	19th December 2018
REV C:	16th July 2021
REV D:	20th July 2021
REV E:	22th July 2021
REV F:	26th August 2021
REV G:	16th February 2022
REV H:	16th May 2022
REV J:	06th October 2022
REV K:	28th February 2024
REV L:	27th March 2024

CONTENTS

1.0	INTRODUCTION	
1.1	Introduction & purpose of document	5.4 Proposed Elevations
1.2	Site wide context	5.5 Proposed Sections
1.3	Site location	6.0 APPEARANCE
1.4	Development parameters	6.1 Facade breakdown
2.0	SITE ANALYSIS	6.2 Materials Palette
2.1	Site Heritage: Conservation Area	6.3 Visuals
2.2	Surrounding building context	7.0 LANDSCAPING
2.3	Existing site context	7.1 Boundary treatments
2.4	Existing context: Storey heights	7.2 Amenity area & front landscaping
2.5	Site constraints & opportunities	7.3 Urban Greening Factor
2.6	Building Heritage: Report Summary	8.0 AMOUNT
2.7	University & local amenities	9.0 ACCESS
3.0	PLANNING CONTEXT & HISTORY	
3.1	Planning History	9.1 Site Access
3.2	Approved scheme	9.2 Cycle Parking
4.0	DESIGN STRATEGY AND PROCESS	9.3 Waste and recycling
4.1	Proposed Use & The Student Room	9.4 Inclusive access
4.2	Plan development	10.0 BUILDING ORGANISATION & FIRE
4.3	Building precedents	11.0 SUSTAINABILITY STRATEGY
4.4	Massing	12.0 SUMMARY AND CONCLUSION
5.0	DESIGN PROPOSALS, LAYOUT & SCALE	
5.1	Proposed Site Layout	
5.2	Proposed Floor Plans	
5.3	Proposed Typologies & Space Standards:	
	a. Communal areas layout	
	b. Typical Studio Layout	
	c. Studio typologies	
	d. Space Standards	

1.0 INTRODUCTION



1.0 INTRODUCTION

1.1 INTRODUCTION & PURPOSE OF DOCUMENT

Benchmark Architects have prepared this Design & Access Statement in support of the Planning Application brought forward by Paddington Packet Boat Developments Limited.

This Design and Access statement outlines the design proposals for the redevelopment of the site currently occupied by the Paddington Packet Boat public house.

The proposals outlined in this document show the scheme that has already been granted planning permission on the site. Permission was granted on 8/9/23 for a purpose-built student accommodation ('PBSA') scheme comprising 61 studios: planning reference 1058/APP/2021/3423.

Following the grant of approval and S106 agreement further viability reports have been produced. The financial viability assessment has concluded the scheme including S106 agreement and affordable housing provision is not viable even though the S.106 was signed quite recently (i.e. 18/8/23).

The viability report outlines 4 scenarios for consideration. The viability report should be read in conjunction with this Statement.

The purpose of this statement is to provide the following information relating to the planning application:

- Location - site features and local character, constraints and opportunities.
- Use and amount of development - proposed use and number of dwellings
- Layout and scale - provide details of the building layout and design process/rationale and proposed heights in relation to the existing context.
- Appearance - the proposed materials and massing
- Access - within and around the scheme

- Landscaping - provide details of hard landscape materials, planting areas and define the treatment of private amenity space

The proposals have been carefully considered in consultation with a professional design team including Nexus Planning. This document is intended to be read in conjunction with Nexus's planning statement.



CGI View of the Development

1.2 SITE WIDE CONTEXT

The site is located to the west of London in the south of Uxbridge, and is identified in red.

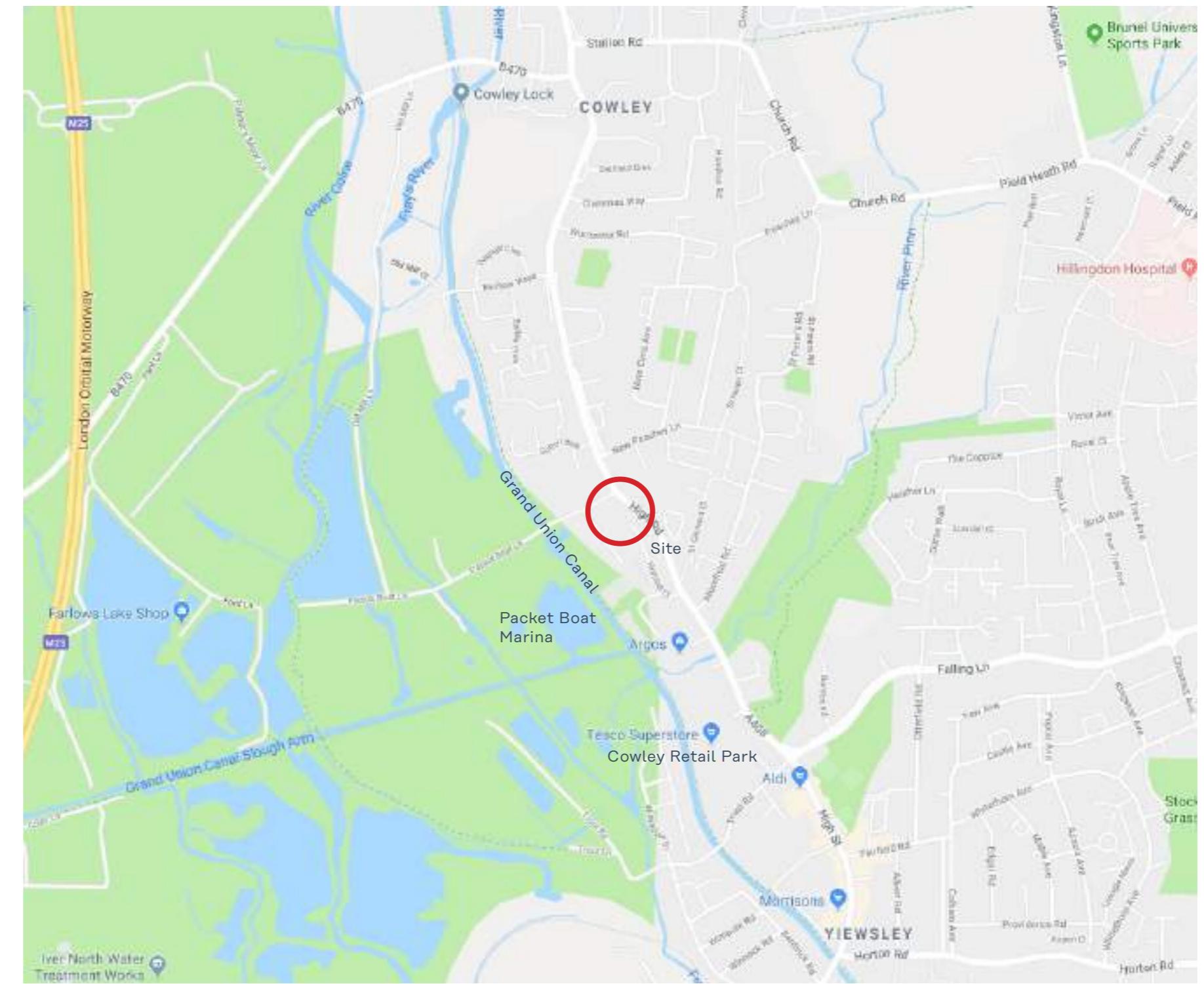
The site lies between the Grand Union Canal, Fray's River and River Colne to the west and the River Pinn to the east. The site sits between Cowley to the north and Yiewsley to the south. Hillingdon Hospital is to the east of the site.

The site has good road transport links with the M25 to the west of site and M4 to the south.

The site has good public transport links and falls within PTAL 1b. Bus stops are located within 1 minutes' walk of the site providing access to a 24-hour bus service, which interchanges at West Drayton Railway Station. West Drayton Station, soon to become a key Crossrail link is the nearest train station only a 19 minute walk or a 5 minute cycle ride from the site (approximately 1.5km).



Location Map (wider context)



Location Map

1.3 SITE LOCATION

The site is located on the corner of High Road and Packet Boat Lane, Cowley Peachy.

The site is at the junction of a residential and industrial warehouse area adjacent to the canal system and packet boat marina. The site is classified as a brownfield site as there is already an existing building (a public house) and a large car park area currently occupying the site.

To the north of the site is Cowley Recreation Ground with Cowley retail park just south of the site.

1.4 DEVELOPMENT PARAMETERS

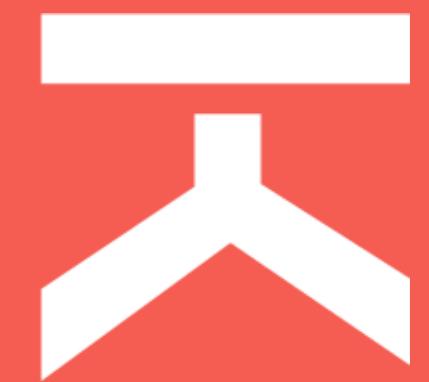
The proposed development seeks full planning permission for the demolition of the vacant Paddington Packet Boat public house and construction of 61 purpose built student accommodation units with associated internal and outdoor amenity space, refuse areas, cycle storage and supporting plant room facilities.

Site Area:

948.3 m²
0.09483 Hectares
0.2342 Acres



2.0 SITE ANALYSIS

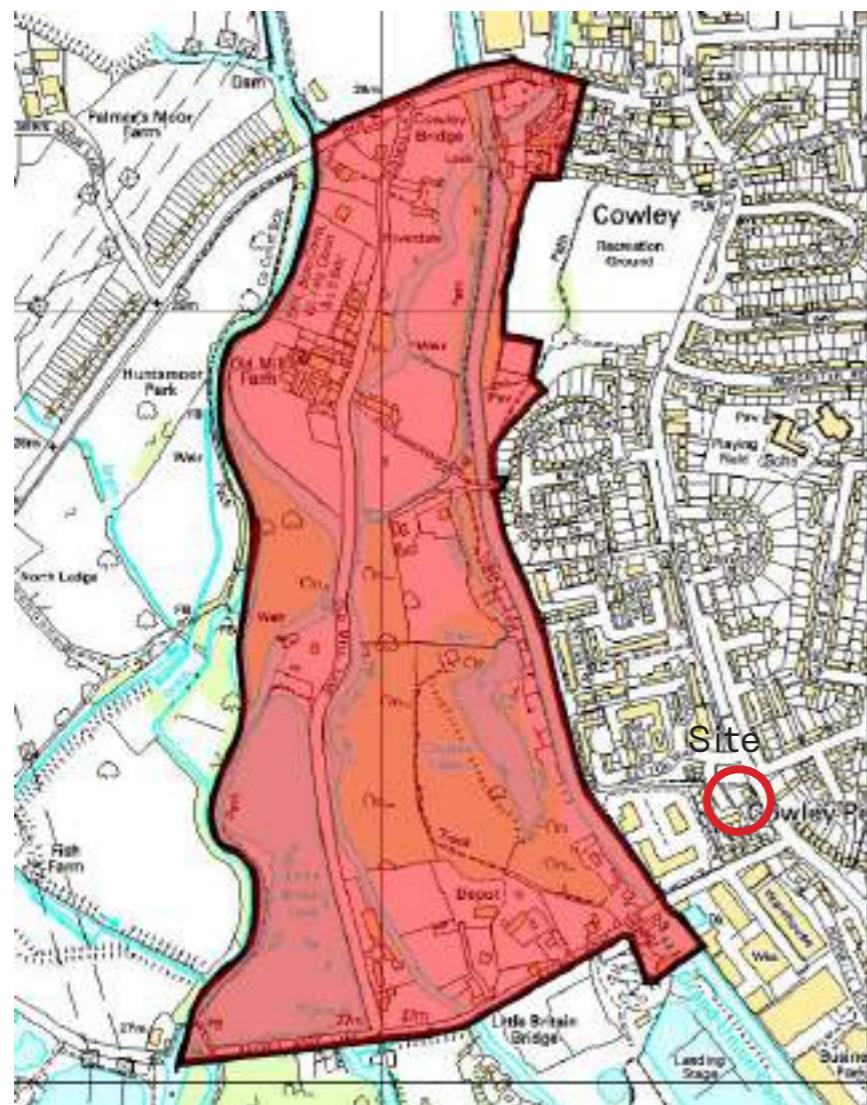


2.0 SITE ANALYSIS

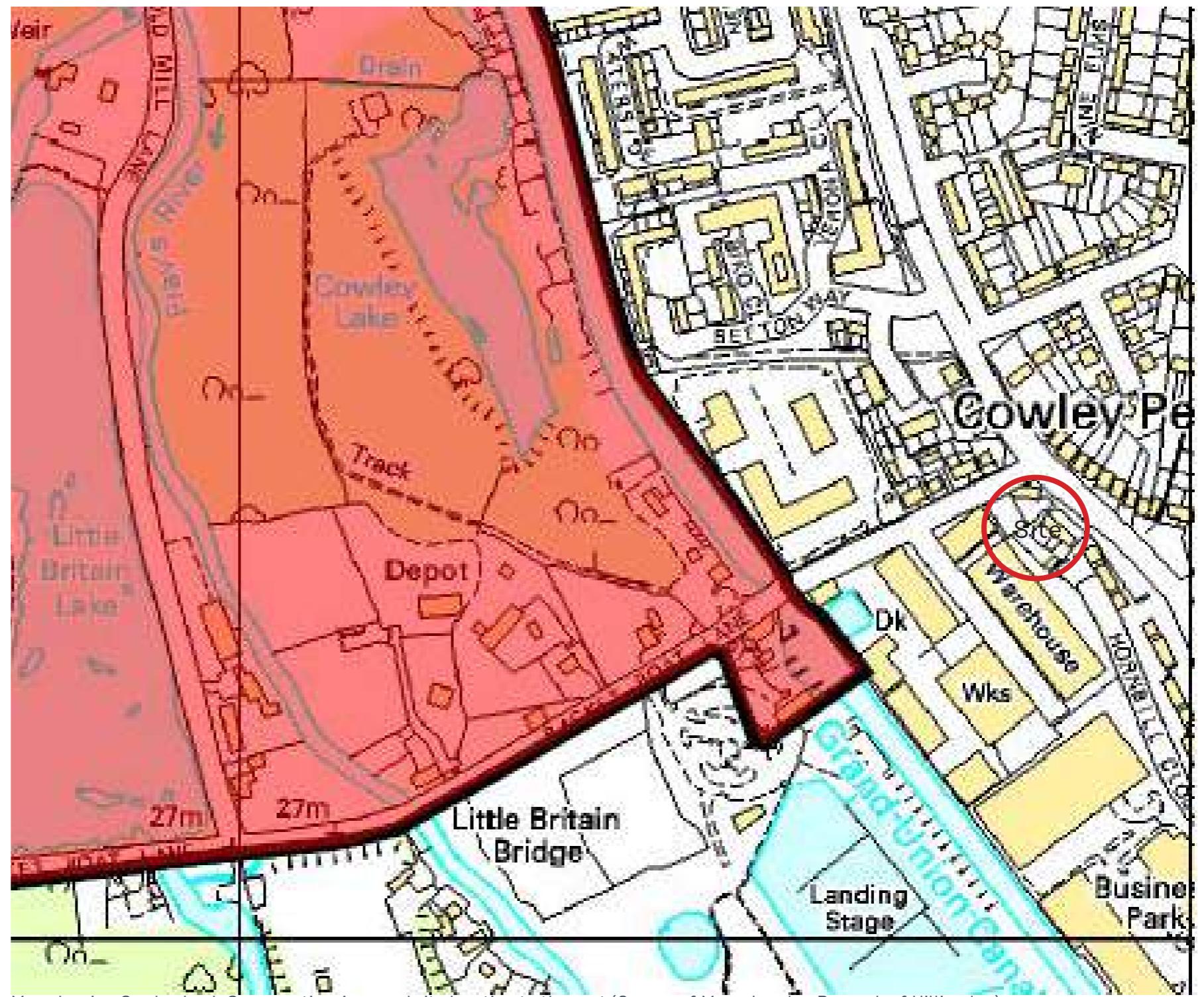
2.1 SITE HERITAGE : CONSERVATION AREA

The site of the Paddington Packet Boat public house is located in the London Borough of Hillingdon.

The plan below shows the extent of the Cowley Lock Conservation Area which is located to the west of the site. From the plan it is clear, that the site falls outside of this Conservation Area.



Map showing extent of Cowley Lock Conservation Area (Source of Map - London Borough of Hillingdon)



Map showing Cowley Lock Conservation Area and site location to the east (Source of Map - London Borough of Hillingdon)

2.0 SITE ANALYSIS

2.2 SURROUNDING BUILDING CONTEXT

The immediate surrounding area east of High Road is a mix of apartments, terraced, semi-detached and detached residential properties.

There is no common style, mass or material to the surrounding building context with a mix of brick and render façades and hipped, gable and flat roofs.

The adjacent photographs show some of the surrounding context.



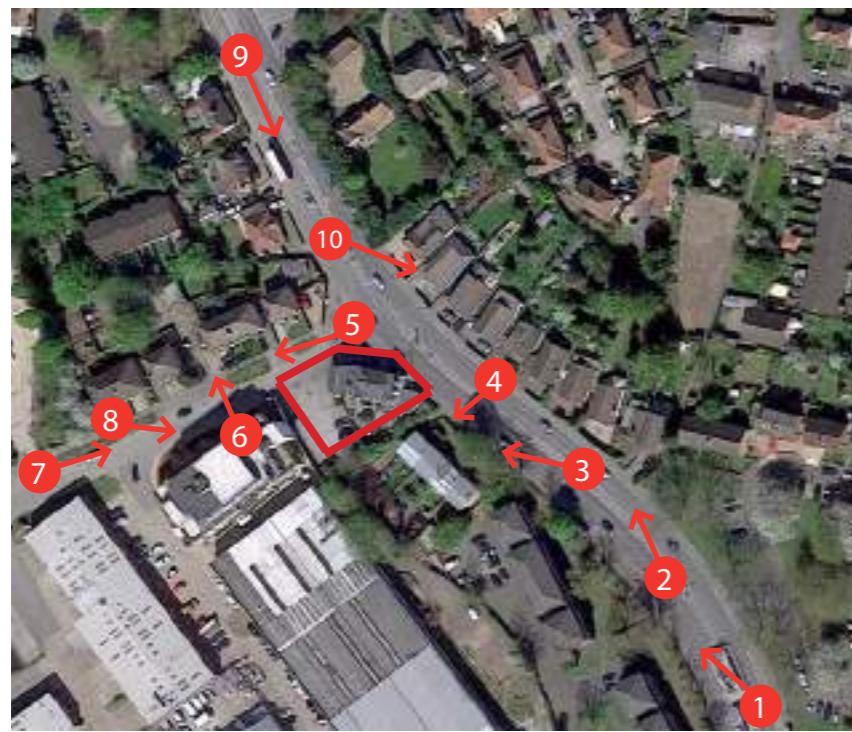
1. View looking north along High Road



2. View looking north along High Road to detached two storey dwellings & bungalows



4. Two storey dwellings to the immediate south of the site



Location key - aerial site photograph



3. View looking north west to 2 storey dwellings south of site

2.0 SITE ANALYSIS



5. View looking west at the junction of High Road and Packet Boat Lane



6. View of two storey dwellings opposite site on Packet Boat Lane



7. View looking east long Packet Boat Lane



8. View of five storey apartments on Packet Boat Lane



9. View looking south along High Road



10. View looking east to bungalows opposite the site

2.0 SITE ANALYSIS

2.2 SURROUNDING BUILDING CONTEXT (CONTINUED)

A new five storey studio, 1, 2 and 3 bedroom apartment development to the west of the site adjacent to the Grand Union Canal was completed in 2019.

The adjacent images show the CGI's of the Union Park Development (source of images Grand Union Park website). Further images and details of the completed development can be found online.



2.0 SITE ANALYSIS

2.3 EXISTING SITE CONTEXT

The proposed site is currently occupied by a closed public house whose lease has now expired. The premises are boarded up and is non viable as a public house - it has been closed for a number of years and is not trading. The photos opposite show the building. It is vacant. The Paddington Packet Boat public house is a two-storey white render building with tiled gabled roof. The building is positioned at an angle on the junction of High Road and Packet Boat Lane. There is a small seating area to the south of the building away from the road and a parking area to the west of the site. Access to the parking area is from Packet Boat Lane. The site gained approval in September 2023 for the demolition of the existing building and erection of a 4 storey 61no. student accommodation block.

The following images show the site in its current context. The aerial photograph below shows the site outlined in red and shows the locations of the adjacent photographs.



Location key - aerial site photograph



1. View looking north-west towards the site



2. View of the front elevation facing the corner of High Road and Packet Boat Lane



3. View looking south towards the junction of High Road and Packet Boat Lane (from Google Street View)



4. View of the rear of the site, parking area and the adjacent building



2.0 SITE ANALYSIS

2.4 EXISTING CONTEXT : STOREY HEIGHTS

These birds eye views of the site show the varying mass and scale of the surrounding context.

Directly adjacent to the west of the site is a 5 storey residential apartment building and beyond there is a further 4-5 storey apartment development (Union Park) which is currently under construction.

The site is a corner plot which is visible from both the north and south some distance along High Road. The site marks a distinct change in building size and character between east and west as shown on the adjacent image.



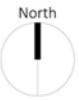
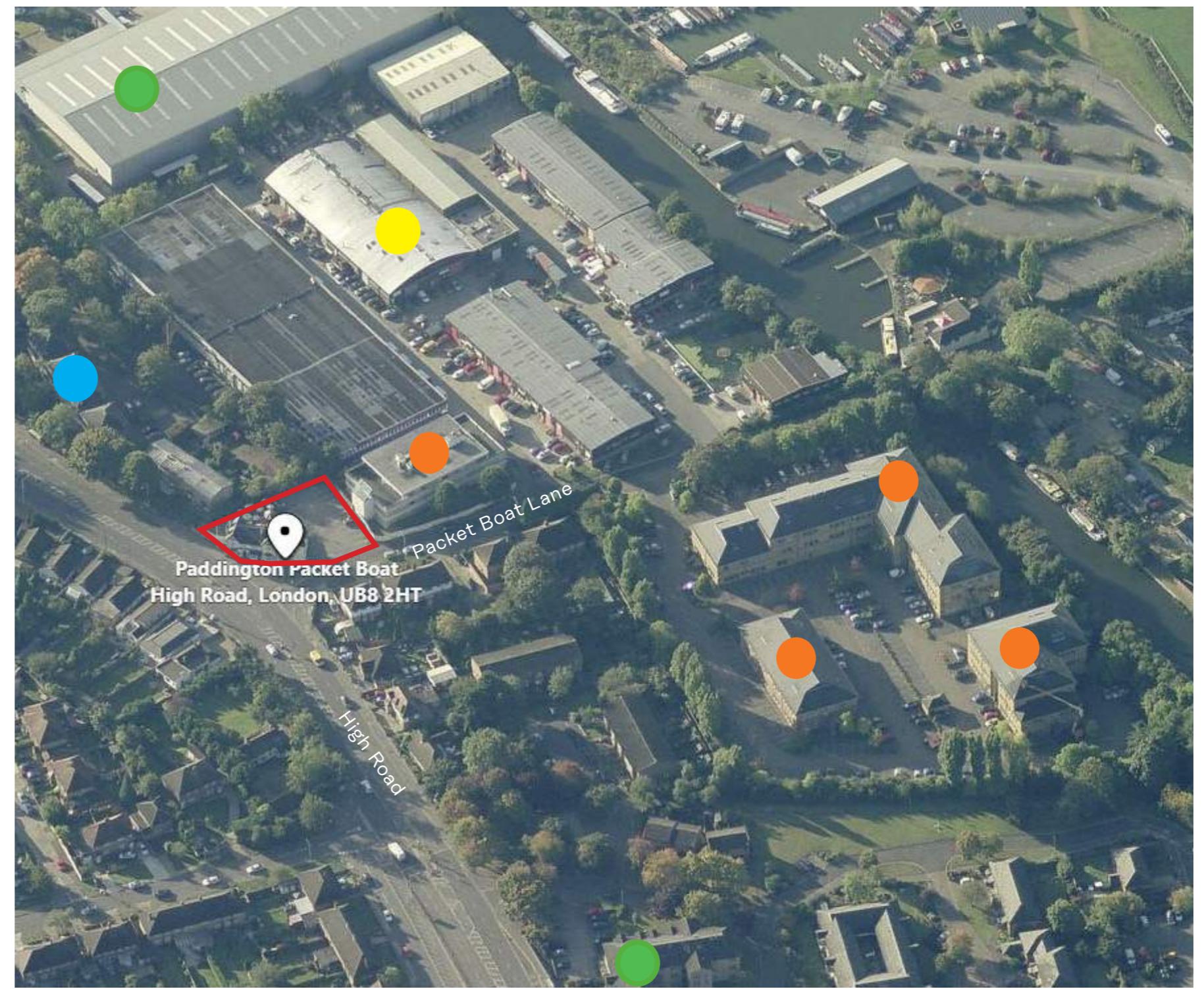
2.0 SITE ANALYSIS

The adjacent aerial photograph illustrates the heights of buildings immediately surrounding the site.

The key below identifies the storey heights of the neighbouring buildings along High Road and Packet Boat Lane. There are numerous tall buildings surrounding the site, including a 5 storey building adjacent to the site along Packet Boat Lane.

KEY

- 2 storey warehouse (similar to 3 storey residential)
- 3 storey
- 4 storey
- 5 storey



2.0 SITE ANALYSIS

2.5 CONSTRAINTS & OPPORTUNITIES

This diagram demonstrates the constraints and opportunities on the site that have informed the design process. It shows physical limitations such as topography, access and onlooking properties.

By recognising key constraints and opportunities on site, we have been able to establish key principles which will help shape the development.

KEY

- Site Boundary
- Sun path
- Noise from High Road
- Existing vehicular entrance to site
- Existing building on site
- Existing narrow footpath
- Adjacent building line
- Sensitive edge
- Proposed built form
- Proposed Landscaping
- Proposed highway improvements
- Proposed building entrance
- Proposed pedestrian access through site
- ↷ Views out from proposed building



Site constraints diagram



Site opportunities diagram

2.0 SITE ANALYSIS

2.6 BUILDING HERITAGE : REPORT SUMMARY

The following summary has been taken from the Heritage report prepared by Corrie Newell HBC.

Conclusion

The Packet Boat is an undesignated heritage asset that dates from the early/mid nineteenth century. The shell retains some interest from that period, but the interior and design is incomplete and the building has been substantially changed and extended.

It has a low level of heritage value and is not eligible for Local or Statutory listing. The building has been assessed for listing and failed to reach the level of historic or architectural significance required - it is of low significance/heritage value. The building does not reflect the age, history or character of either of these listed buildings and does not add to the appreciation of significance (NPPF, Annex 2: Glossary Setting).

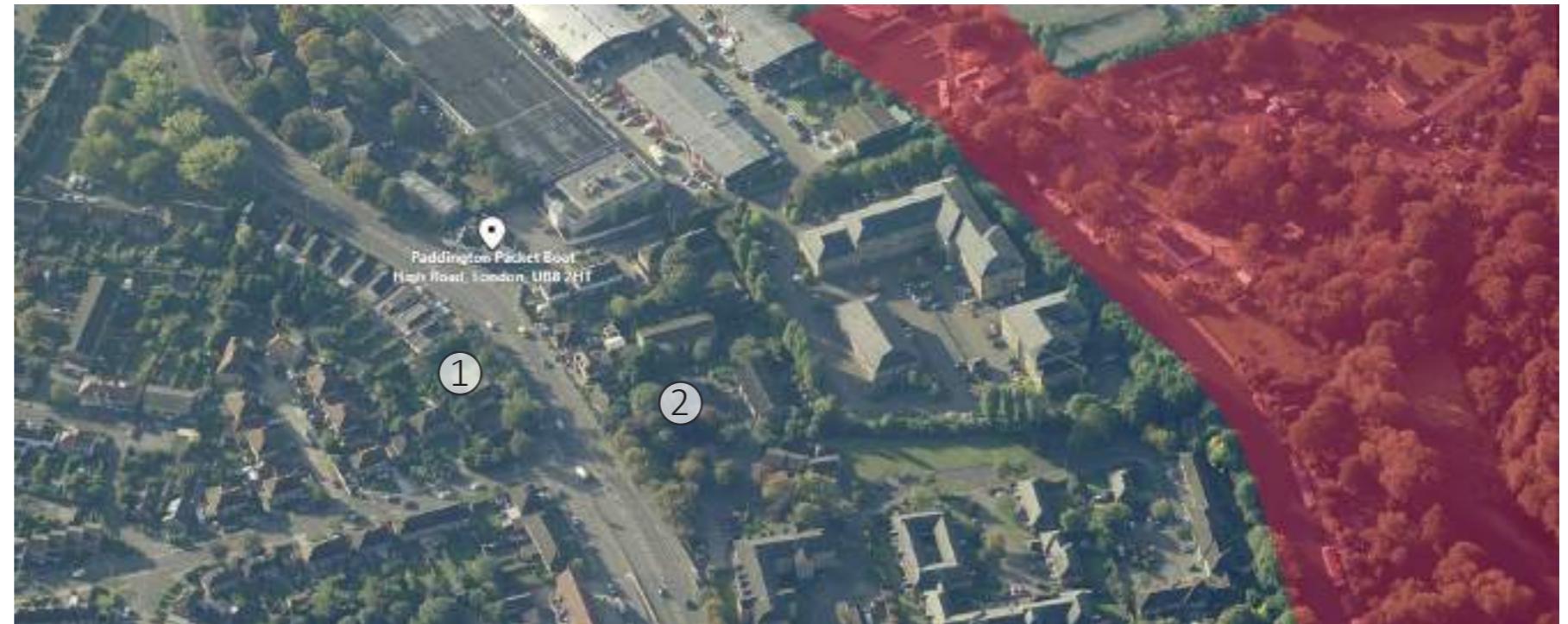
The loss of the building is 'less than substantial harm'.

As it is not a designated heritage asset, there is no specific requirement to balance harm against public benefit. As it is not a statutorily protected asset, the building can be demolished.

The surroundings of the Packet Boat do not contribute to the character and appearance of the nearby Conservation Area, so there is no shared setting that would be significantly changed.

① Name: BARNACRE
Designation Type: Listing
Grade: II
List UID: 1358423

② Name: THE OLD COTTAGE
Designation Type: Listing
Grade: II
List UID: 1193946



Map showing Cowley Lock Conservation Area, plus locally listed buildings. Site location to the west (Source of Map - Bing)



2.0 SITE ANALYSIS

2.7 UNIVERSITY AND LOCAL AMENITIES

As outlined within the Travel Plan Statement by Cotswold Transport Planning, there are a number of local amenities within the surrounding area for students to use - supermarkets, local shops, newsagents, library, bakery, cafe, takeaways, gym, etc.

Brunel University is located a 1.6km walking distance, north-west of the site. The university can be reached via a 20 minute walk or 5 minutes cycling. Due to the close proximity of the university to the site, the location of the site for student accommodation is ideal and students can reach the university quickly while remaining close to other amenities - shops, supermarkets, etc.

The development provides a disabled car parking space on the site for ease of access.

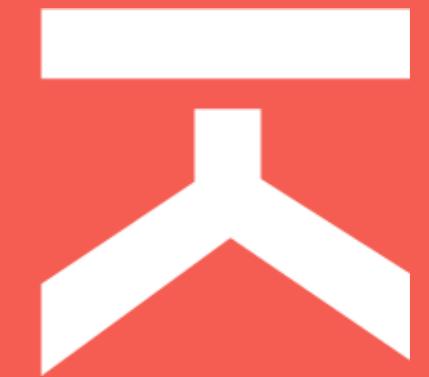


Image of Brunel University



Google Aerial view showing the site to the south (small red circle) and Brunel University complex to the north (large red circle)

3.0 PLANNING CONTEXT AND HISTORY



3.0 PLANNING CONTEXT

3.1 PLANNING HISTORY

This section outlines the planning history and feedback of the scheme from the first Pre-Application submission in February 2018, through to the most recent Pre-application submission in September 2020. Detailed on this page is a timeline since our involvement on the scheme.



TIME-LINE OF CURRENT APPLICATION (1058/APP/2021/3423) (See chapter 3.7 for further detail)



3.0 PLANNING CONTEXT

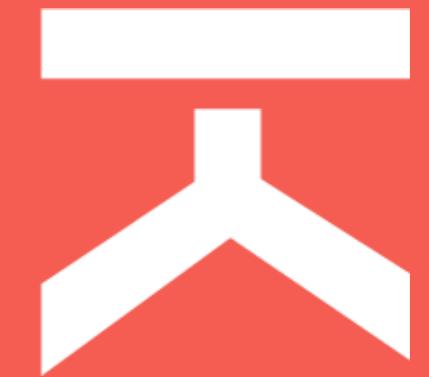
3.2 APPROVED SCHEME

The approved scheme for 61no. apartments is shown adjacent. The following chapter shows the scheme in more detail.



CGI View of the Development

4.0 DESIGN STRATEGY AND PROCESS



4.0 DESIGN STRATEGY & PROCESS

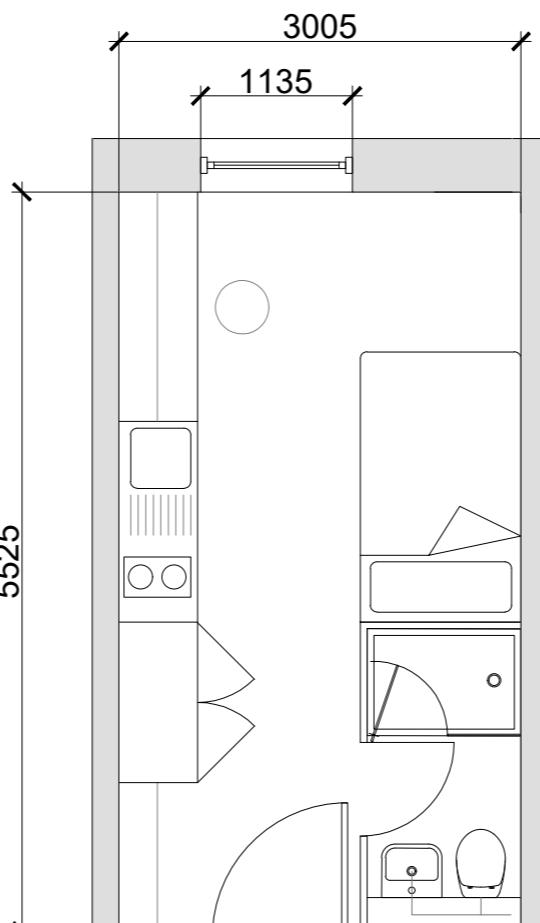
4.1 PROPOSED USE & THE STUDENT ROOM

A shortage of Purpose Built Student Accommodation (PBSA) for the Brunel University has been identified. The scheme has received strong support from the Accommodation team at the University. The clients have also teamed up with an expert in the PBSA field, to gain their expertise on room sizes, amenity and room type mix. All of which have been integrated into the proposed scheme.

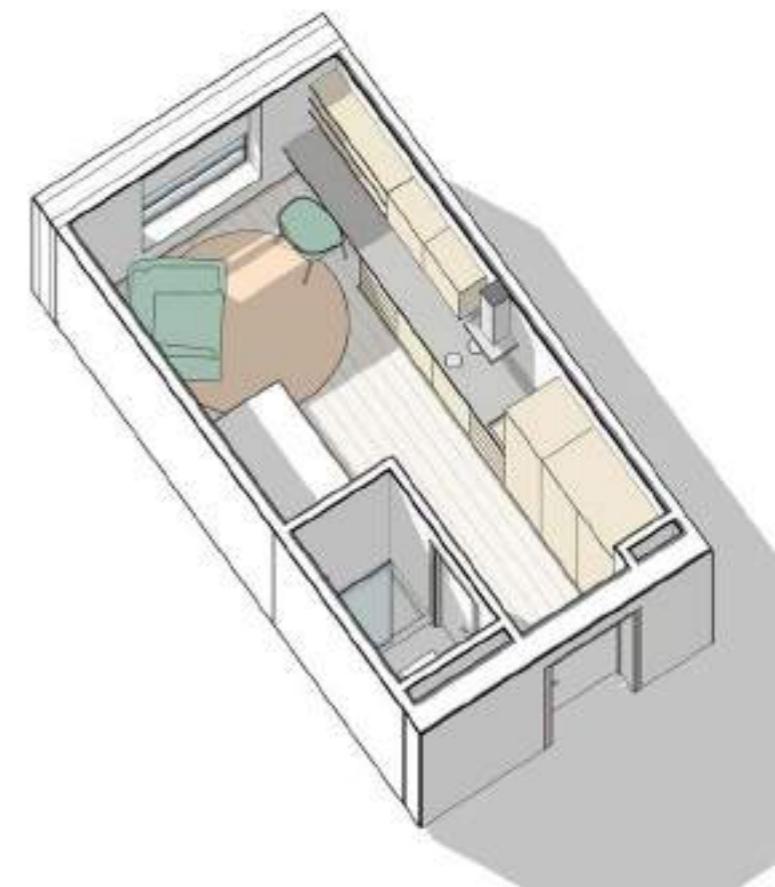
The clients see this development as a financial benefit to the local economy.

By understanding the set out of the studios, a typical plan can be made and repeated throughout the scheme. In addition to the typical studio, accessible studios will be provided to meet the London Plan guidance.

The diagram adjacent show studio typology A - the typical studio. The layout is most efficient, and includes all the essentials including a bed, study and living area, kitchenette, shower room and storage. Within each floor plan, there are small variations to some of the units to work more effectively with the elevation layouts.



TYPICAL STUDIO UNIT - TYPE A
16.7 SQM
PLAN



TYPICAL STUDIO UNIT - TYPE A
16.7 SQM
AXONOMETRIC

4.0 DESIGN STRATEGY & PROCESS

4.2 PLAN DEVELOPMENT

A floor plan is developed in line with the constraints and opportunities set out earlier in the document as well as the feedback from prior planning applications.

The proposed building follows the line of the adjacent buildings, and internally repeats the typical studio units next to one another. The simple design allows for a corridor running through the middle of the building, giving all units a view out onto the road or landscaped garden.

The entrance, reception and communal areas are located at the corner where the wings of the building meet. The core is located at the rear and provides access to all levels. Access to the communal garden is also through this core.

External bin and bike storage has been placed to the west of the development, secured behind a gate which also gives additional rear access to the building through the landscaped gardens.



Initial Floor Plan Sketch

4.0 DESIGN STRATEGY & PROCESS

4.3 BUILDING PRECEDENTS

The precedent images set out here illustrate some of the design influences based on the previous page's images, and the materials that are being considered for the development. High quality contrasting and tonal facing brickwork and glazing is proposed.



Flat parapets with set backs and soldier course banding.



Light coloured brickwork with vertical windows.



Light coloured brickwork with stone copings and vertically expressed windows containing solid infills at particular locations.



Generously sized windows and proportions to the facade. The flat parapets emphasize the horizontal appearance of the building and offer the opportunity for set backs at higher level.



Three Gable House Steinstrasse 20 - Tchoban Voss Architekten
The three tones of facing brickwork on the expressed gable design separates the elements and breaks up the mass of the building. Intricate brickwork detailing on the elevations between the windows adds rhythm and depth to the facade.

4.0 DESIGN STRATEGY & PROCESS

4.3 BUILDING PRECEDENTS

The precedent images set out here illustrate some of the design influences based on the previous page's images, and the materials that are being considered for the development. High quality contrasting and tonal facing brickwork and glazing is proposed.



Light coloured brickwork with light coloured top floor which is set back to reduce the scale and massing of the development.



Light coloured brickwork with light coloured top floor which is set back to reduce the scale and massing of the development. Vertically rainwater pipes located to the front of the building.

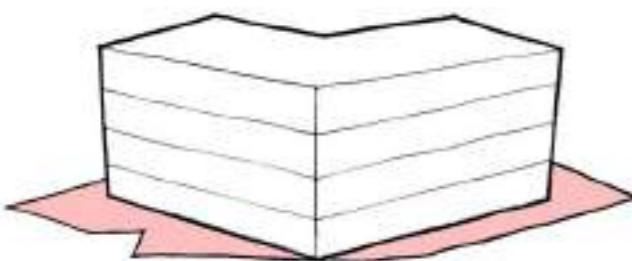


Buff/beige coloured brick with dark frame window and doors create a strong sense of contrast. The entrance is set within the building, along with the balcony openings to create a varied and informal arrangement.

4.0 DESIGN STRATEGY & PROCESS

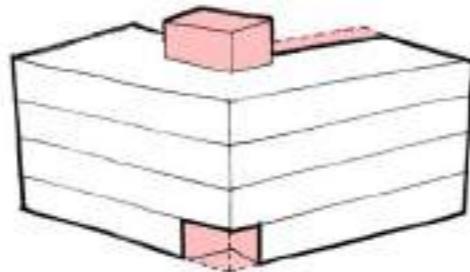
4.4 MASSING

The following massing diagrams highlight the progression of design development resulting in the proposal being brought forward in this document.



A | A four storey building is positioned on site in line with the adjacent buildings and running parallel to both roads. With the landscaped garden and amenity space at the rear of the site an L shape block is formed.

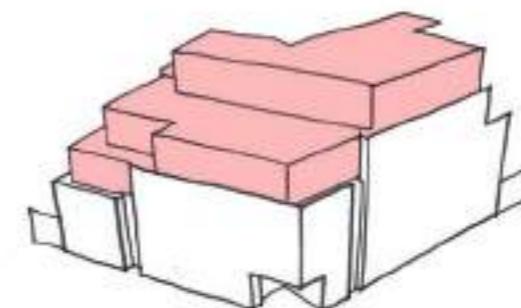
The space between the building and boundary line will allow for a green buffer and pavement on both sides.



B | The location of the main entrance is best suited to the corner of the block, where the two wings of the block meet. The ground floor is recessed here to give cover to the entrance as well as making a clear point of arrival.

At the rear on levels 2-3, the building is setback approx. 2m to avoid overlooking into neighbouring properties, as well as allowing more light into the garden.

Additionally, a stair and lift core access has been added to roof level.



C | Responding to the wider context, and the form of the surrounding buildings, the floor plates are cut back and set back to reduce the scale, mass and height of the proposal. The creation of the flat roofs at each floor level reduces the bulk of the proposal. The upper floors of the building are finished in a lightweight material which reduces the visual impact of the proposal from the street-scape.

This creates for a more interesting street scape, breaking down the extensive horizontal form, as well as hiding the lift and stair core from street level.



PREVIOUS DESIGN PROPOSAL MASSING