

DESIGN AND ACCESS STATEMENT

PLANNING APPLICATION
FOR THE PROPOSED DEVELOPMENT OF
THE SHERATON HEATHROW HOTEL

15th September 2011



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CLIENT

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INTERIOR DESIGNER



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INTRODUCTION



This Design and Access Statement is submitted on behalf of T.M Hotels (Heathrow) Ltd. in support of a Full Planning Application, for the development of the Sheraton Heathrow Hotel.

The purpose of this statement is to:

- Provide a concise overview of the principal design and access issues affecting the proposals and a brief history of the previous planning history of the site.
- Explain the design principles and concepts which have informed the development.
- Show the necessity for the development.
- Demonstrate the relationship with the existing site context.
- Respond to the visions and aspirations of the City Council and local community.
- Address key issues with regard to access.
- Show how traffic is managed throughout the scheme.

This statement forms an integral part of the submission and provides important and detailed information about the scheme which has been written for a wide target audience, including the Planning Authority, Local Residents and Businesses, Statutory and Non-Statutory Consultants and Elected Council Members.

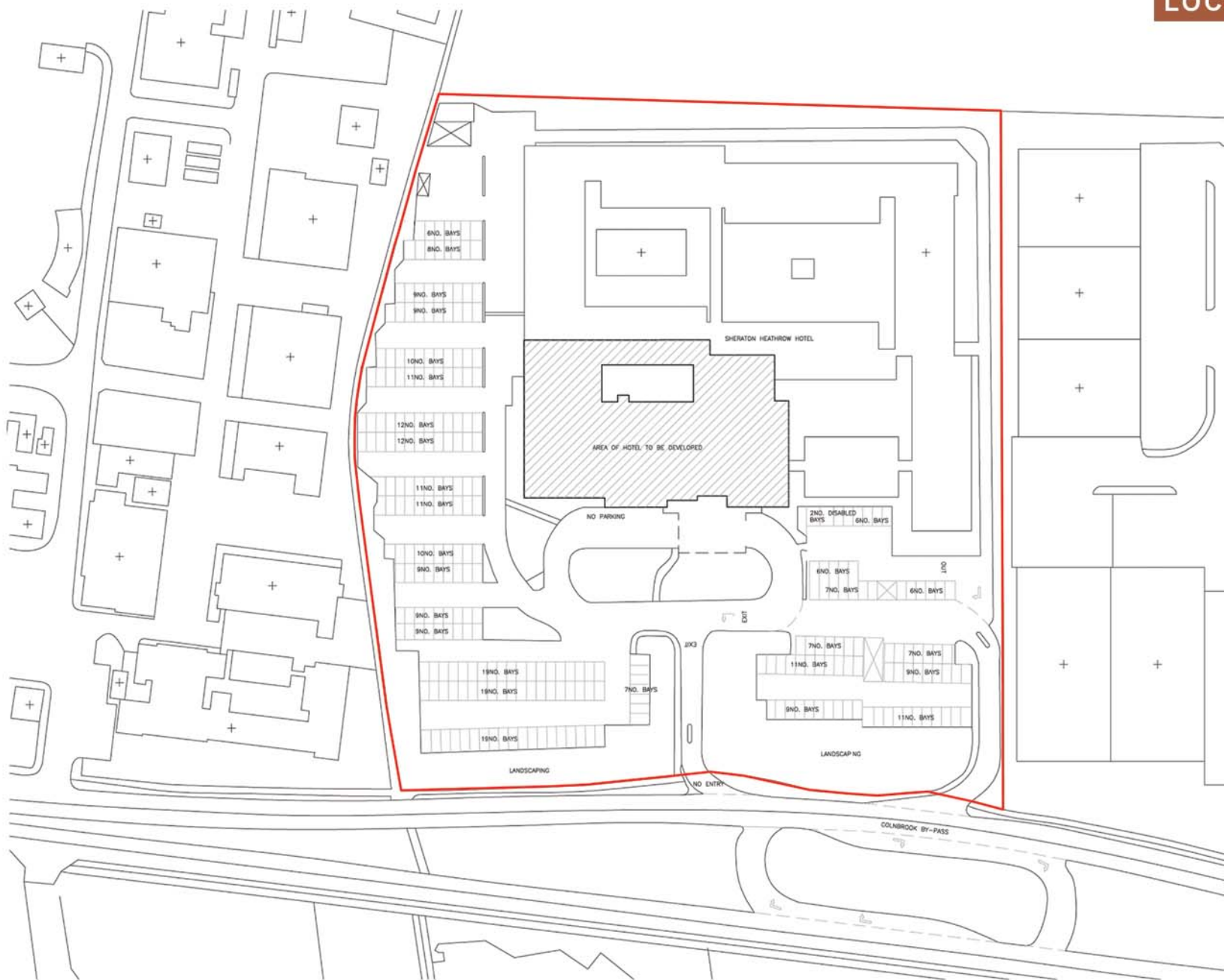
LOCATION

The hotel is located approximately 1km from the nearest Heathrow Airport Terminal, Terminal 5. It has a single access off the A4 Colnbrook By-Pass that brings guests to an onsite car park which includes in excess of 281 parking spaces. The hotel itself comprises of linked buildings that provides over 400No bedrooms and 4* guest facilities, including meeting rooms, bars, and restaurants. The main public areas are housed within the south-western quarter of the complex, with the surrounding wings being used for guest accommodation.

The existing hotel has a covered main guest entrance along the south-facing frontage and is around 70m from the Colnbrook By-Pass. Vehicles turning off from Colnbrook By-Pass enter a one-way system that leads to car parking zones and to the drop-off under the covered entrance. There is no parking allowed at this entrance. On exit, vehicles circulate around the turning circle in front of the entrance and return back onto Colnbrook By-Pass heading east, with a nearby turning circle to allow vehicular access heading west.

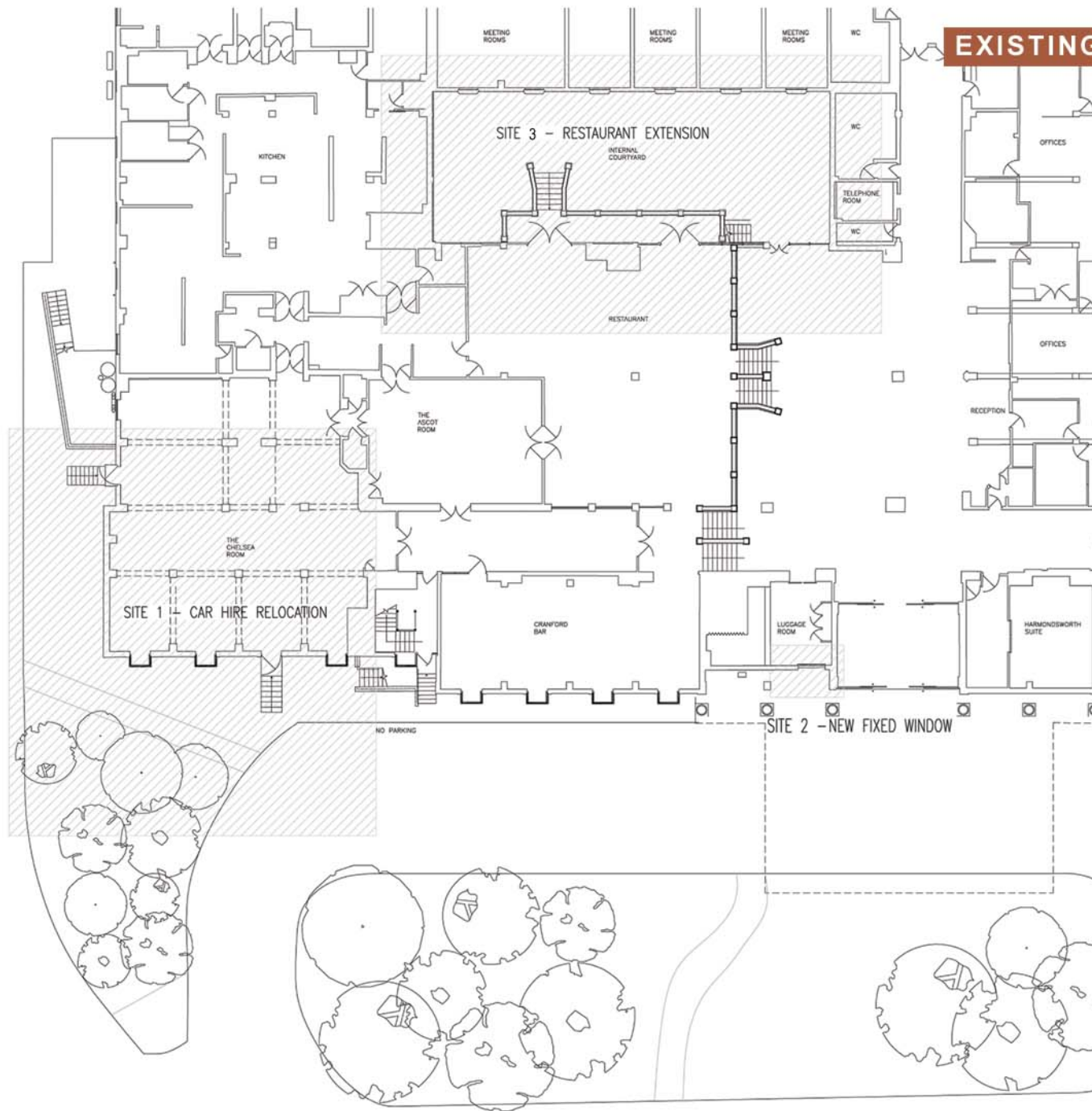
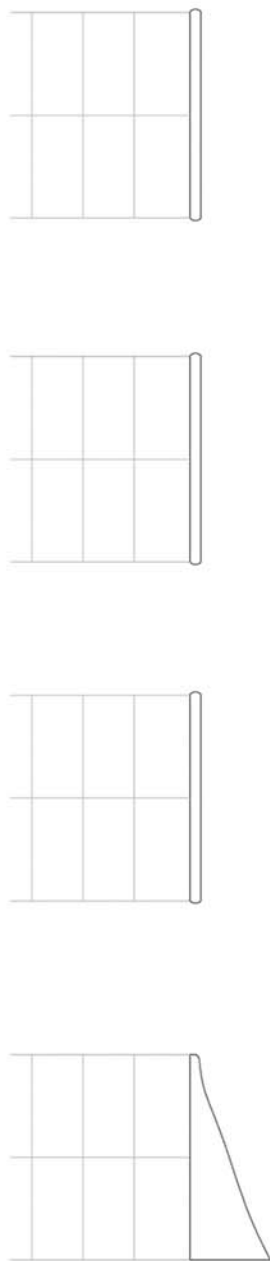
At present, there is a car hire service that is housed in an office within the hotel. They currently use the hotel entrance turning circle as a drop-off / pick-up point. For pedestrians accessing the site from surrounding streets and the onsite car park, a banded pedestrian crossing is located directly south of the hotel entrance, allowing access across the entrance/exit turning circle.

The building is predominantly 3 storeys high, with occasional 2 storey blocks within the four internal courtyards of the complex. This is typical of the surrounding context as there is a cluster of 3 storey London Heathrow Airport buildings to the west and 2 storey storage / industrial buildings to the east.



SITE LOCATION
SCALE 1:2000 @ A4





EXISTING CONTEXT

There are two main existing sites that are to be developed in accordance with internal layout changes, these are as follows:-

Site 1 - The south-western corner of the building that currently houses The Chelsea Room. External landscaping to the south and west of building.



Site 2 - Considerations have also been made with regards to the sliding luggage room window adjacent to the main entrance.

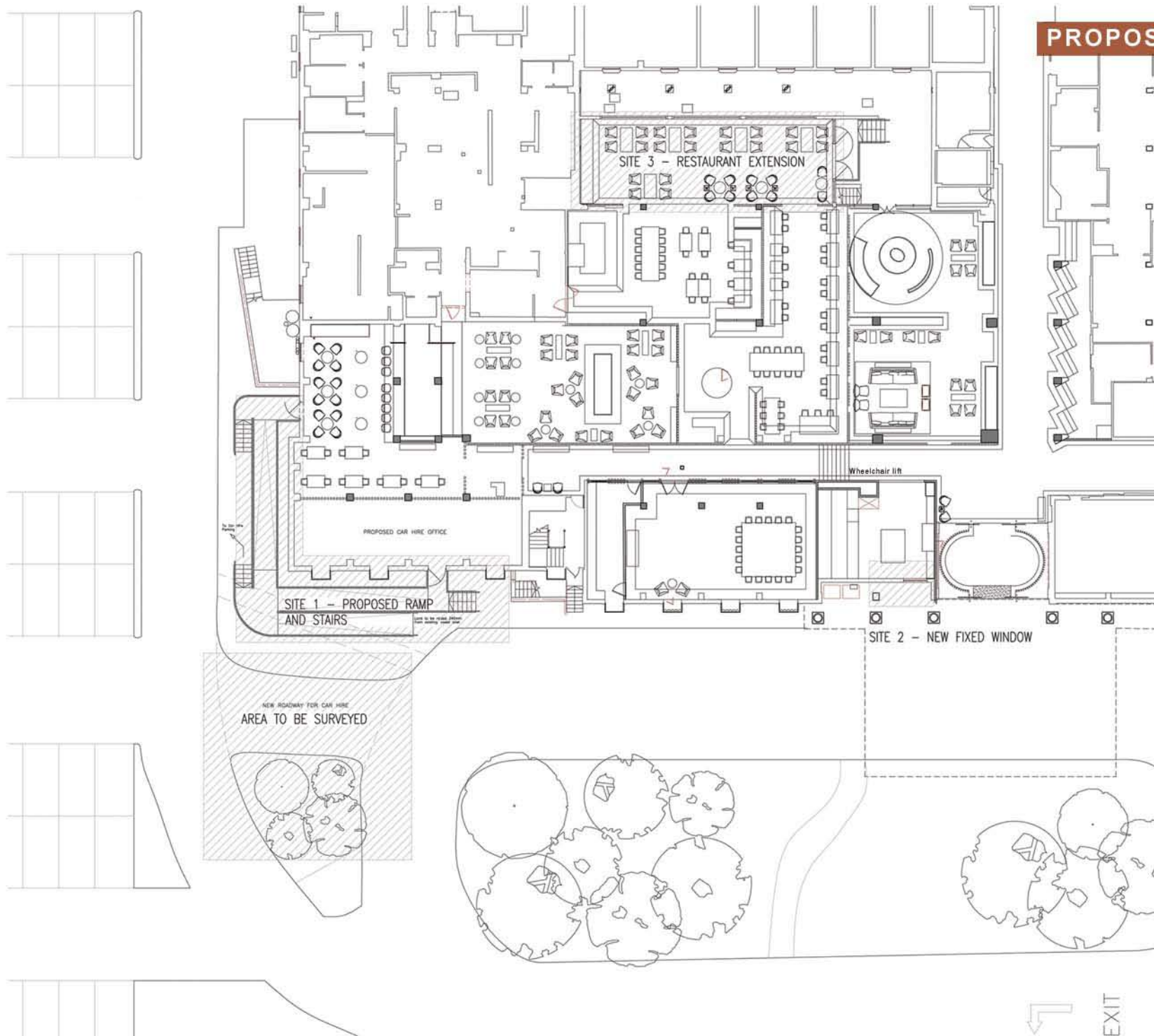


Site 3 - The internal courtyard that at present has stepped access from the restaurant.



SITE PLAN
SCALE 1:500 @ A4





PROPOSED SITES

AMOUNT

The existing hotel accommodates over 400No bedrooms together with substantial guest and staff facilities. The 90sqm additional area is to be provided to the reception lobby restaurant. No additional bedroom accommodation is to be created. All the remaining works do not provide further accommodation and are predominantly interior design alterations. These new interiors address the current regulations for accessibility for staff and guests. In most instances the works will also ensure that the changes will meet the In summary the extent of works to be included and which will require approval within the planning application are :-

Site 1 - Formation of new ramped and stepped entrance platform to access on-site car hire offices and adjacent bar area, where existing door openings are maintained.

Site 2 - Introduction of a new fixed window in the existing facade adjacent to the entrance.

Site 3 - Construction of a 90sqm single storey restaurant extension within an internal courtyard of the hotel. Materials: to be light weight panel and glass with a modern appearance.

SITE PLAN
SCALE 1:500 @ A4



EXISTING - SITE 1

USE

The existing Chelsea Room is part of the conference and dining facilities that are provided alongside the Ascot Room, Cranford Bar, Restaurant and Kitchen.

At present there are no provisions externally for disabled access into any of these spaces,

There are two stepped entrances that rise approximately 1.2m above ground level into the Chelsea Room. Both doorways open outwards with little clear opening distance at the top stair landing. The entrance stairs to the west have paving that leads directly onto the car park road, while the stairs to the south are also face straight onto the entrance/exit turning circle road. This is of real concern due to the frequent number of vehicles passing by these points and the lack of alternative pedestrian routes. A single red line road marking indicates that there is no parking permitted along these boundaries.

Externally, immediately south of the Chelsea Room are 10 No. of trees the closest of which is less than 5m from the building edge. With the majority of parking concentrated to the south and west of the building, the view of the main entrance is interrupted and is further disconnected for visiting guests. In addition, these trees block southern sunlight from entering the lower levels of the building.

The majority of the proposed public areas refurbishment will be internal, with new finishes and fittings and rationalised divisions of spaces. The current layout sees a maze of doors, stairs and internal rooms that are difficult to navigate around, particularly for new guests or those using the conference meeting facilities for the first time. The intention of the scheme is to ensure that all guests are catered for, this means providing more coherent circulation routes that take into consideration disabled access and means of escape.

SITE PLAN
SCALE 1:200 @ A4



THE
CHELSEA
ROOM

NO PARKING

AREA TO BE SURVEYED

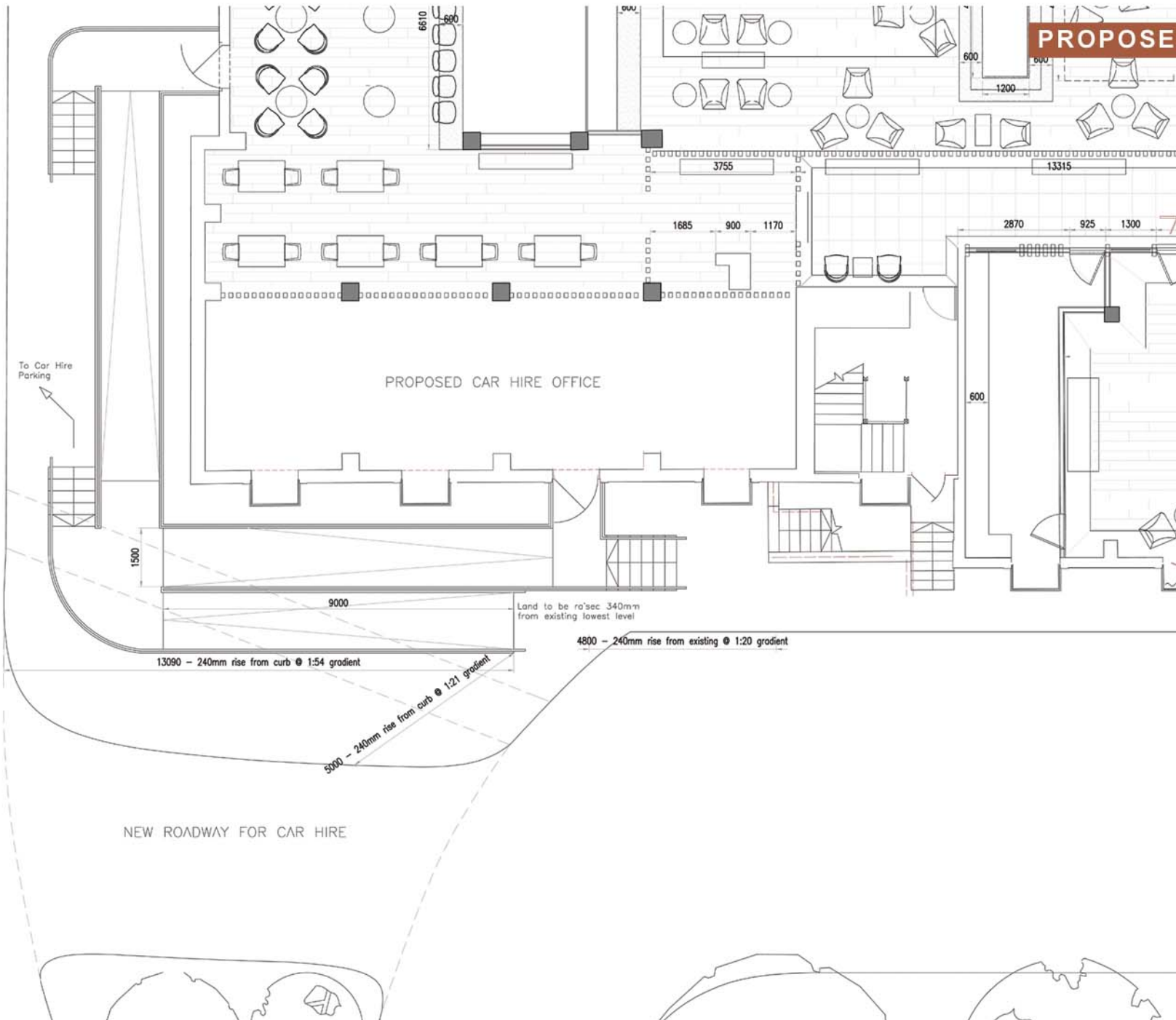
PROPOSED - SITE 1

LAYOUT

The proposed introduction of the car hire office to the south-western corner will relieve vehicular and pedestrian congestion and the hotel entrance as the proposed roadway will provide a designated space for hire cars to be picked up and dropped off. As the roadway will be in close proximity to the existing car park, the car hire staff can make the most of the car parking spaces to the west of the hotel thus freeing up spaces for guests to use the more convenient spaces to the south of the hotel.

At present the array of trees and pavements that lead into roadways make for an incoherent and unsafe environment for pedestrians to circulate. With the intended removal of trees directly in front of the proposed car hire office, guests arriving at the hotel will have a clear view of entry points from the adjacent car park.

The proposed ramp and stairs will define the corner of the building, providing a link between the main entrance, entrance to the bar area on the western façade of the building and the car parking areas. The ramp provides additional landing space in front of both entrance doors to the car hire office and bar, reducing the risk of guests colliding with outward opening doors. The proposed ramps and steps will facilitate all the necessary tactile paving and be designed in accordance with current building regulations.



SITE PLAN
SCALE 1:200 @ A4



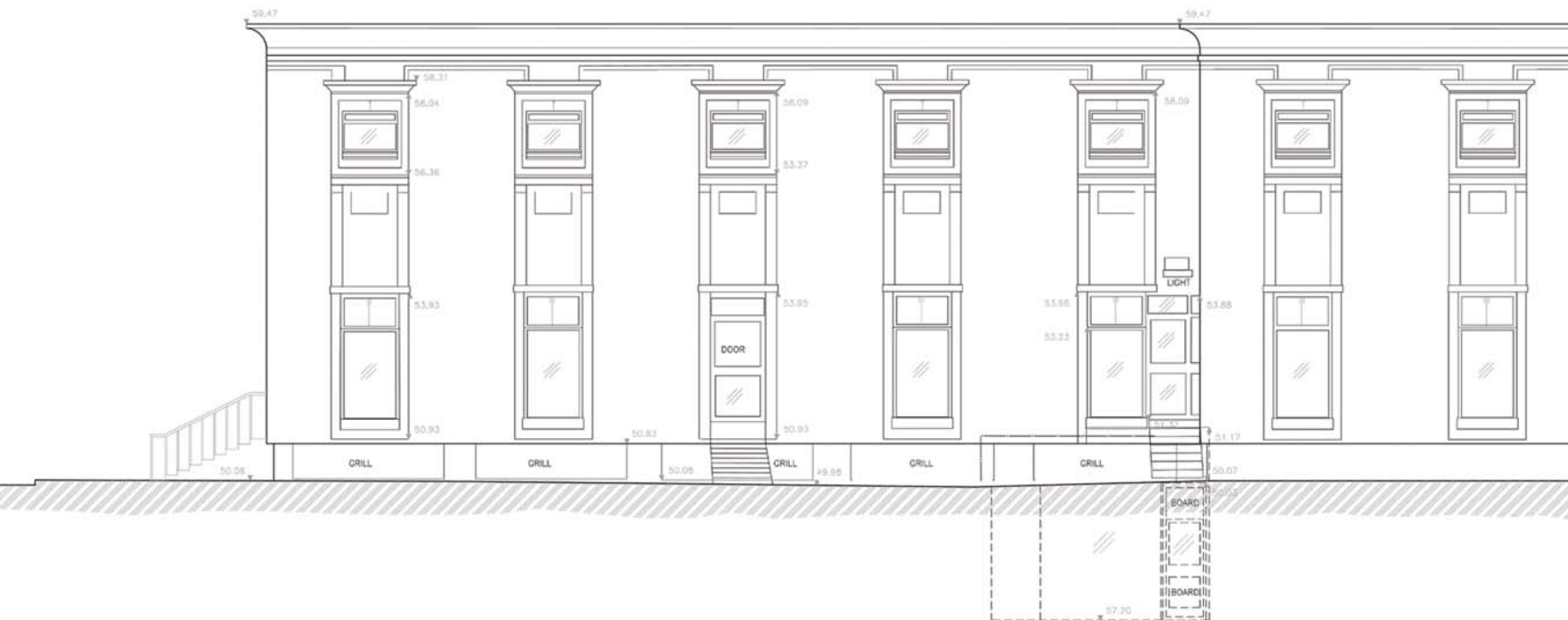
EXISTING - SOUTH ELEVATION

APPEARANCE

The main south facing elevation composes of a series of window boxes that appear to puncture the masonry clad façade.

A series of low level grills approximately 800mm high and 3m wide cut into the façade at ground level, where they are used to service the basement floor. As a result the ground floor at the south-western corner expresses its raised plinth and exposes its structural components.

At present, the steel stairs that lead to the Chelsea room lack a sense of integration and are simply 'plugged' onto the building.



45_D04 ARBITRARY SATURN



South entry into existing Chelsea Room

SOUTH ELEVATION
SCALE 1:200 @ A4

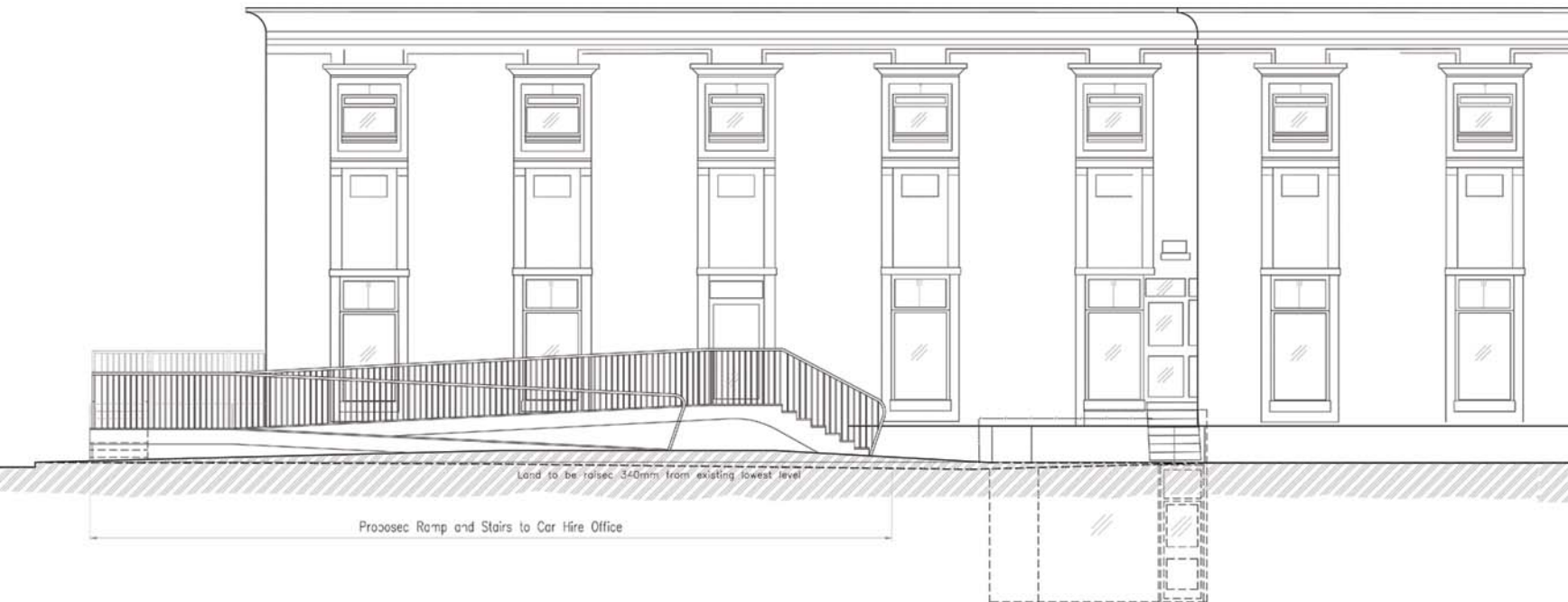
PROPOSED - SOUTH ELEVATION

APPEARANCE

The proposed ramp and stairs will wrap around the building corner providing an elegant solution to accessing the raised level. It is intended that black coated steel could be used to visually integrate with the protruding black window boxes at ground level and the nearby stairs that lead to the basement. As the ramp has a shared landing to allow change in direction towards the west entrance of the proposed bar, this element will act as a visual tie across the material change.

The proposed ramp and stairs will mask the unsightly grills at ground level. It will however be set back to avoid obstructing the window boxes and grills.

It is envisaged that signage will be introduced above the existing doorway so signify to guests the individual function of the car hire office as well as indicating it as a fire escape.



SOUTH ELEVATION
SCALE 1:200 @ A4

EXISTING - WEST ELEVATION

APPEARANCE

The masonry cladding wraps around to the west elevation and terminates shy of the side entrance into the Chelsea Room. Here the building sets back and introduces a change in material, with exposed ribbed concrete continuing along the rest of the façade. The joint line between panels continues transversely across the two materials.

In addition to the 'plugged-on' stairs that also features on the west elevation, there is also a low level air conditioning unit that at ground level that is connected to the more desirable masonry panelled portion of the building.



West entry into existing Chelsea Room

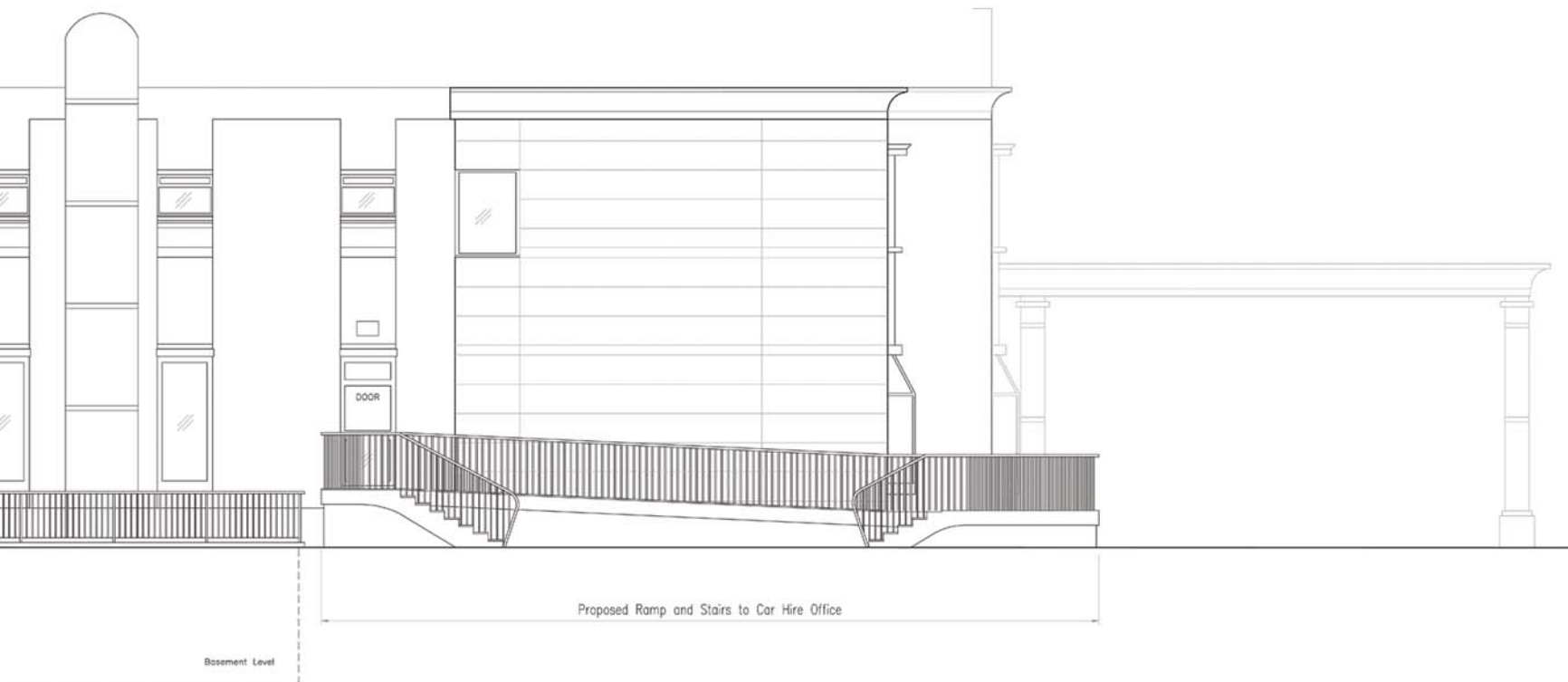
WEST ELEVATION
SCALE 1:200 @ A4

PROPOSED - WEST ELEVATION

SCALE

The existing car hire offices are within the car park in a lightweight insecure facility. The new car hire secure office is required in addition to the existing facility and will be located within the extent of the existing building. The area of bar within the restaurant will be redesigned into the interior design for the public areas.

There will be no works affecting the existing south and west facade to accommodate the car hire offices. Minor alteration will be required for the construction of the proposed ramp and stairs to allow wheelchair access into the hotel which is approximately 1.2m above external ground level.

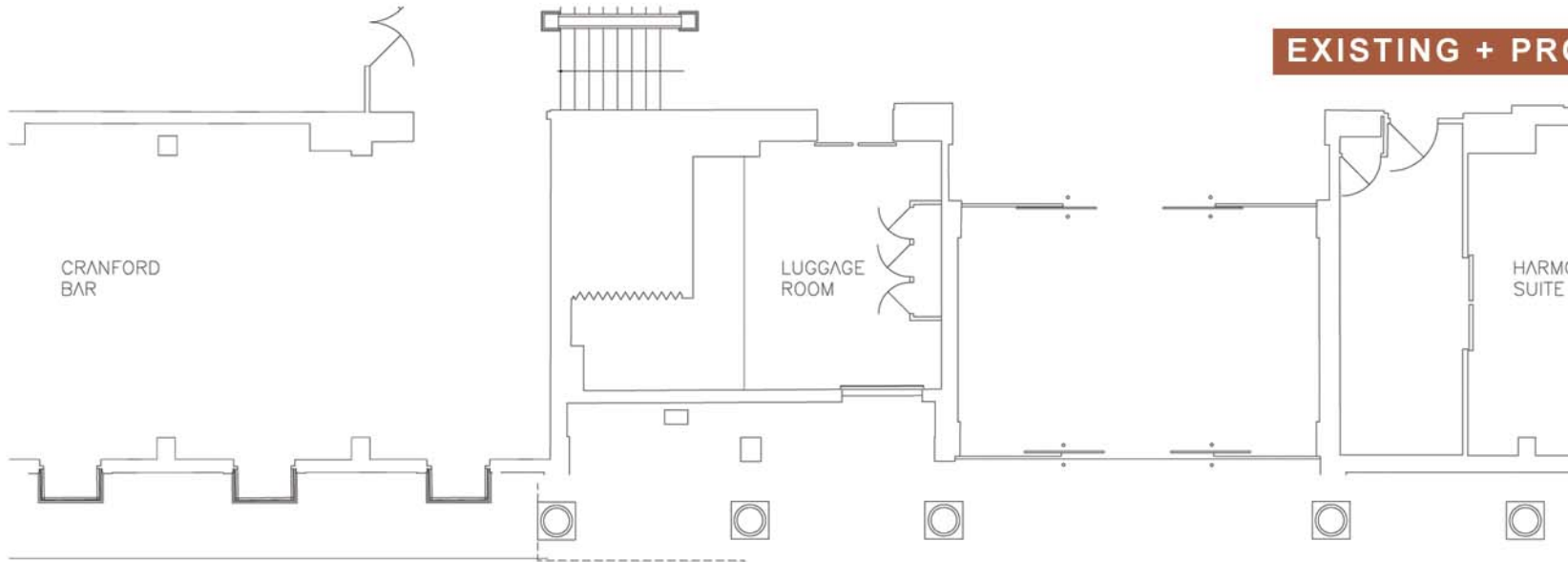


WEST ELEVATION
SCALE 1:200 @ A4

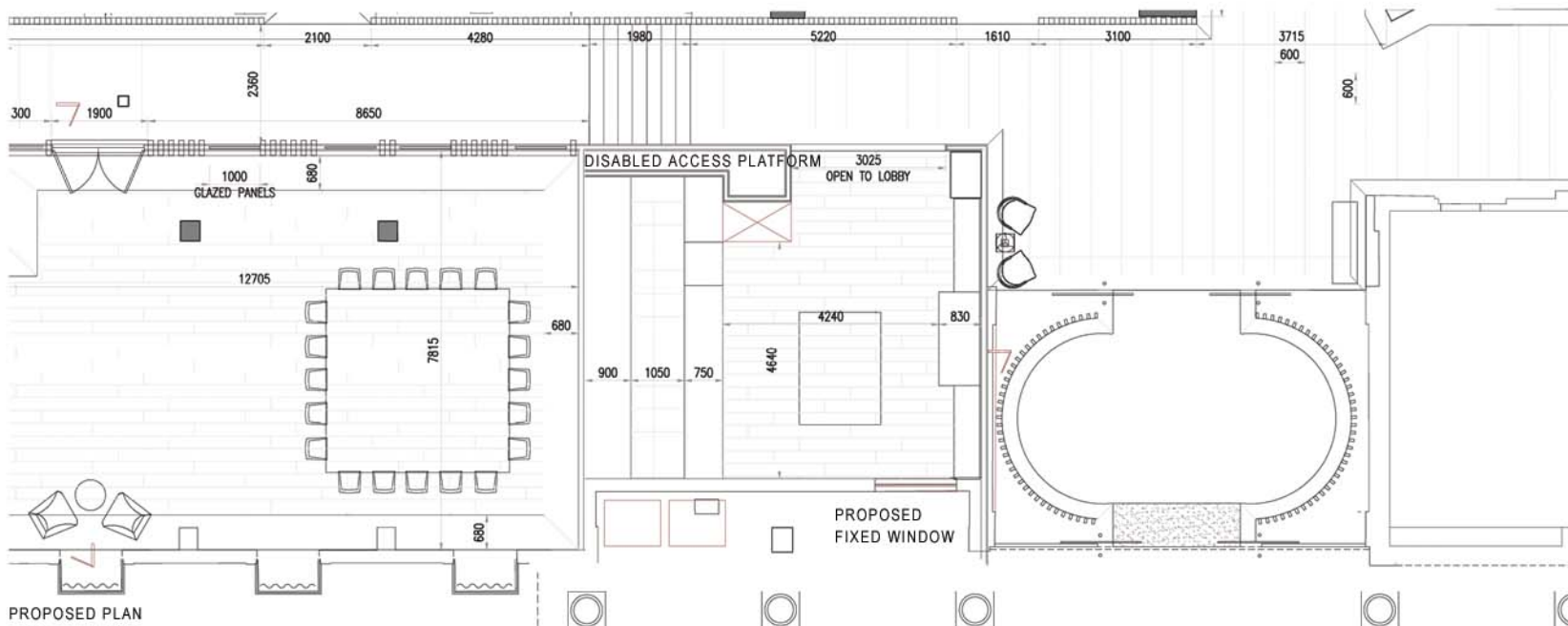
EXISTING + PROPOSED - SITE 2

USE

The existing raised deck is accessed via a wheelchair stair lift which allows for wheelchair bound guests to independently reach the 1.2m high raised deck facilities. This facility is to be retained and which will retain the fully inclusive access arrangements to meet current regulations. The stair lift is directly adjacent to a set of access steps to allow for assistance by guests and by fully trained hotel staff.



EXISTING PLAN



PROPOSED PLAN

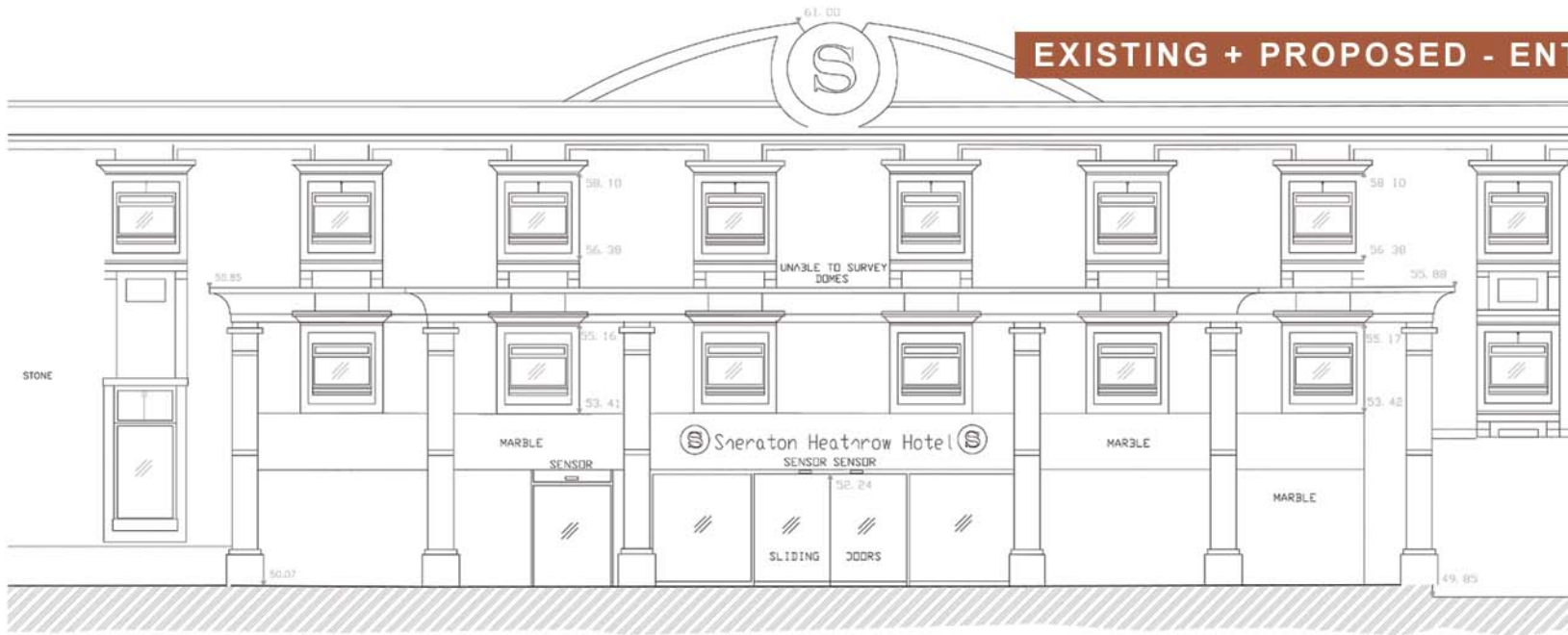


EXISTING + PROPOSED - ENTRANCE ELEVATION

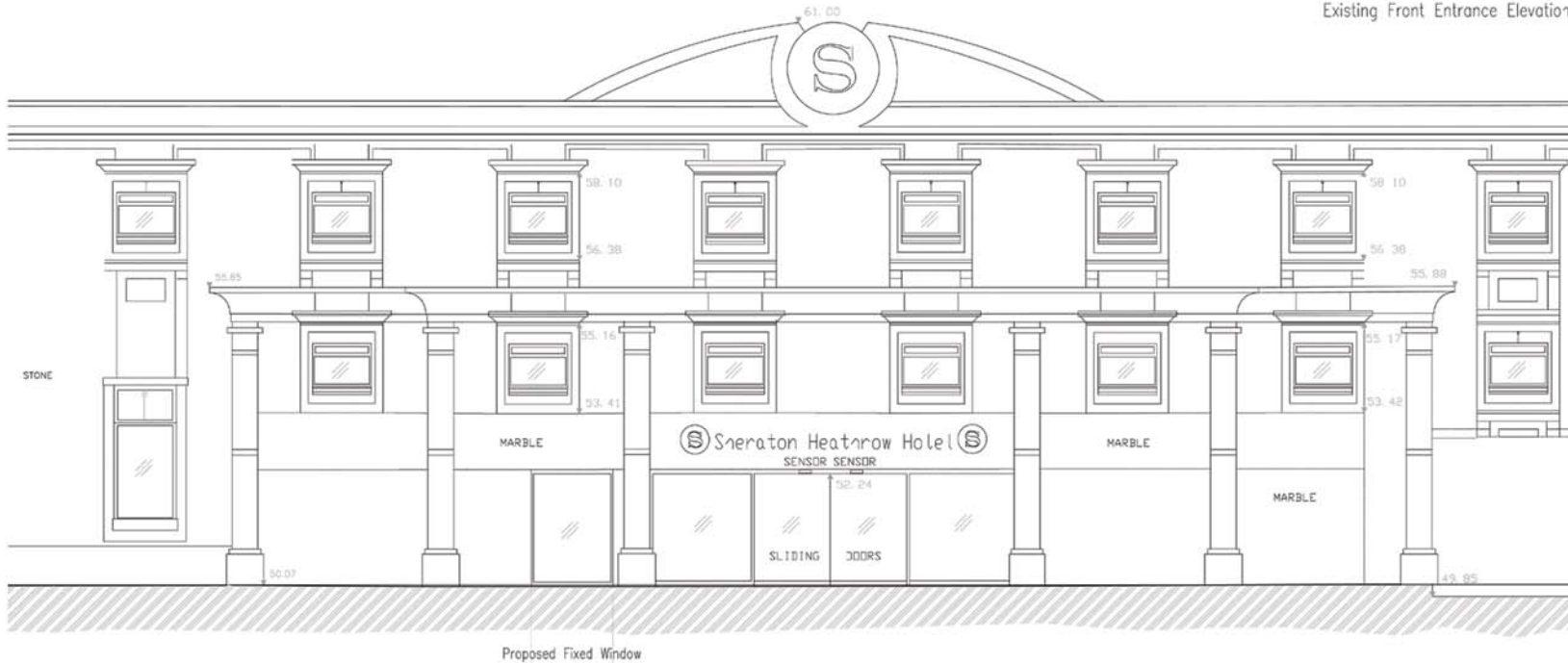
APPEARANCE

Adjacent to the entrance is the luggage room which at present has a sensor operated sliding door that opens out to the drop off point.

Due to proposed internal changes designating this area for use as a deli, a fixed window allows for a secure internal facility whilst minimising alterations to the external elevation.

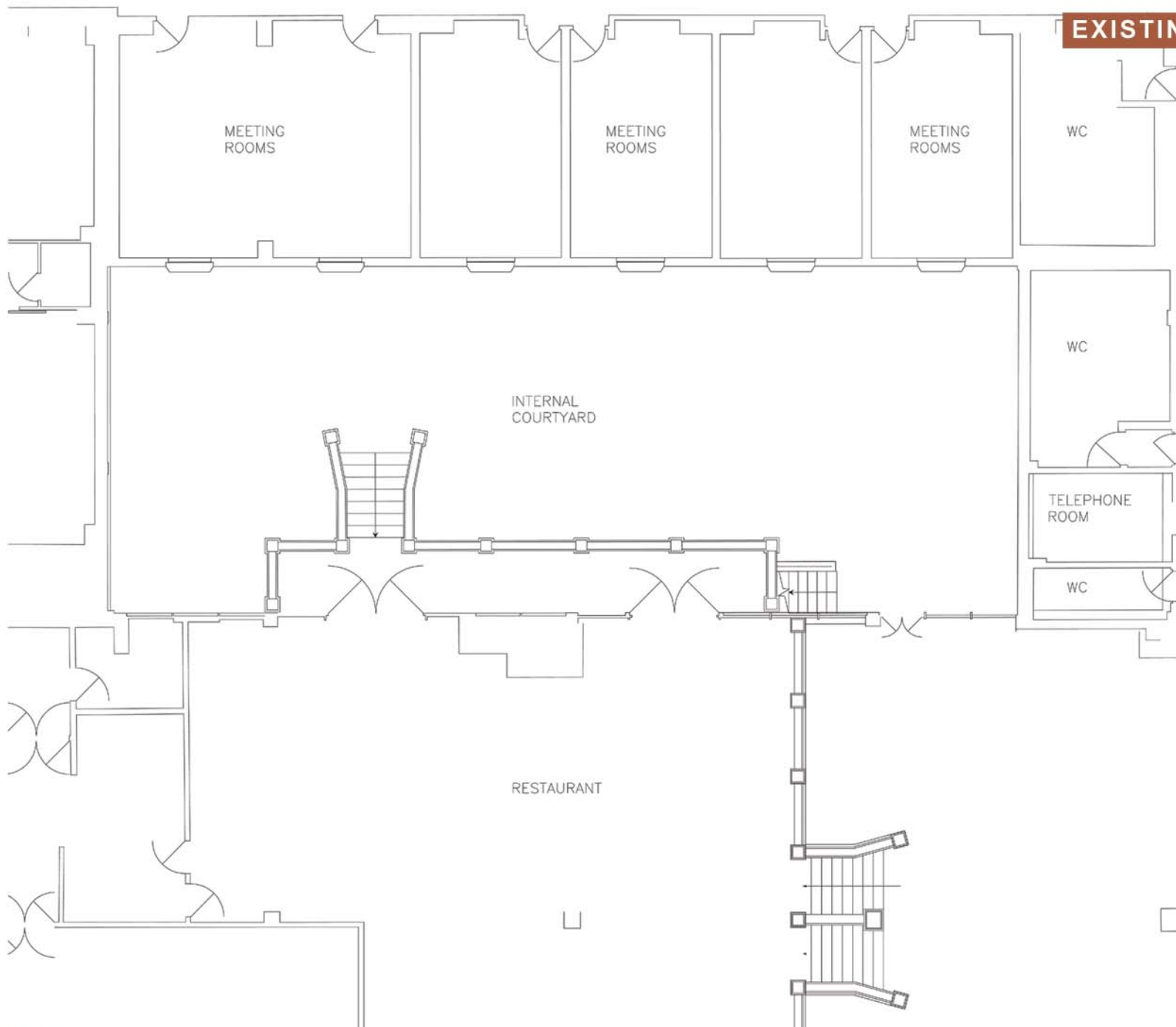


Existing Front Entrance Elevation



Hotel entrance and adjacent luggage room door.

ENTRANCE ELEVATION
SCALE 1:200 @ A4



EXISTING - SITE 3

USE

The restaurant is raised approximately 1.2m above ground level and has direct stepped access from the adjacent main entrance lobby. The restaurant itself is served by the kitchen which is also at this raised level.

The restaurant has two sets of double doors that open out onto a narrow terrace within an internal courtyard and at present this space only serves as a lightwell, with no dedicated function. There is only a 2m usable width provided by the terrace which is insufficient for any particular use, and this is enforced by the obstruction caused by the two sets of outward opening double doors.

The courtyard can be accessed at ground floor from the main entrance lobby. There are stairs within the internal courtyard that descend below the existing terrace and provide access to the basement level 2.8m below ground.

SITE PLAN
SCALE 1:200 @ A4



[illegible]

The proposed single storey extension into the internal courtyard meets the rising demand for more seating in the existing restaurant and utilises the underused external space.

The extension will stop at least 3m away from the adjacent meeting rooms windows, overcoming issues of reduced sunlight to these spaces. The meeting rooms are infrequently used and when they are it is often during the late morning / early afternoon period where the sun is at its highest point.

The glazed facades of the proposed extension will allow for natural sunlight to penetrate the deep plan and illuminate the restaurant area.

As the proposed extension continues the raised floor level at approximately 1.2m above ground level, the existing basement can still be accessed from the stairs that descend from the internal courtyard underneath the extension.

The proposed stairs that connect the restaurant extension to the internal courtyard are located close to the nearest fire escape to maintain fire escape distances and the fire escape strategy. It also provides coherent circulation between the basement level and the restaurant level, via the internal courtyard.

SITE PLAN
SCALE 1:200 @ A4



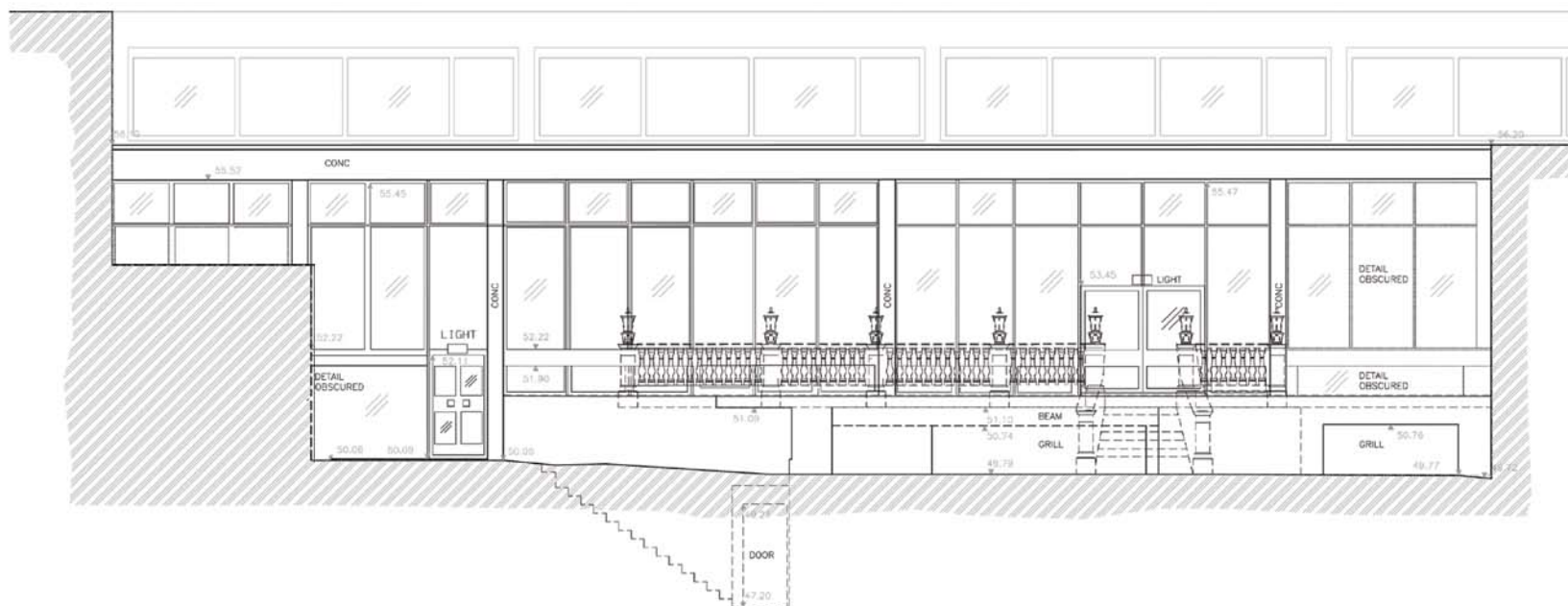
EXISTING - COURTYARD ELEVATION

APPEARANCE

The existing raised single storey restaurant has an array of glazed windows and double doors with a high level infill panel that masks aspects of the roof structure. The glazed elements have mullions at 1200mm centres with occasional intermittent concrete pillars.

As with the external south elevation, there are grills that vent the basement area. At present, a combination of the terrace and foliage largely mask these from view. The terrace itself, is constructed from cast concrete components that appear heavy and bulky.

The proposed extension will match the existing restaurant roofline and have large fixed windows to permit the maximum natural daylight into the restaurant.



45.00M AMBIENT DATUM



Inside courtyard facing south-east

COURTYARD NORTH ELEVATION
SCALE 1:200 @ A4

PROPOSED - COURTYARD ELEVATION

SCALE

As the proposed extension of the restaurant faces inwards to the internal courtyard and is of a similar scale to the existing building. As a result, its impact is of no consequence to the existing street scene from Colnbrook By-Pass.

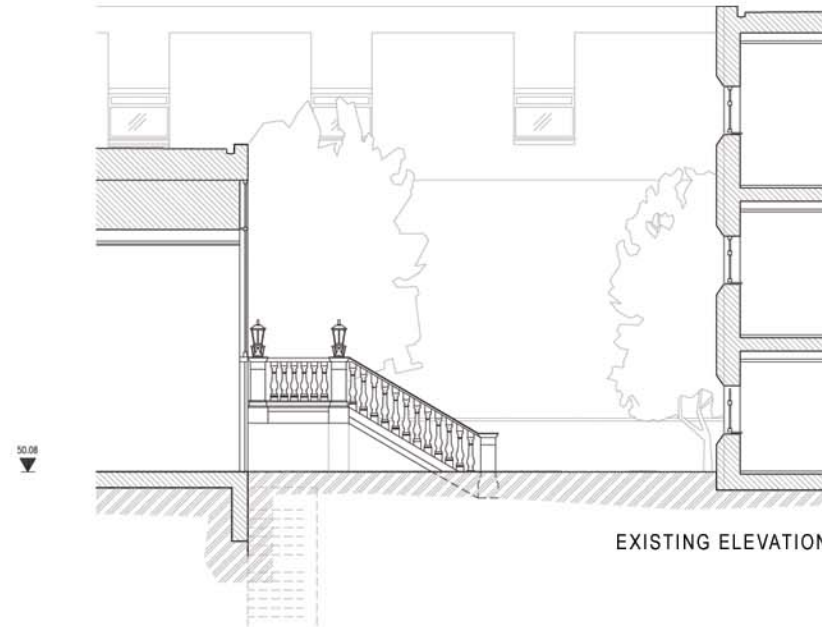
A typical storey in the building is approximately 2.7m. The public areas of the hotel are double height (including services space) with single storey above. The majority of these public spaces, including the proposed restaurant, have been raised 1.2m above ground in order to provide ventilation grills to the basement below.

The proposed glazed extension to the restaurant will be perceived as a single storey space that has a height of approximately 5.8m from ground level. This will have a dual aspect as providing the additional floor area required for the car hire and to create a dedicated partially covered controlled area for guests using the outside space.



COURTYARD NORTH ELEVATION
SCALE 1:200 @ A4

EXISTING + PROPOSED - COURTYARD ELEVATION



EXISTING ELEVATION



PROPOSED ELEVATION

APPEARANCE

At present the internal courtyard had a mix of raised single storey, 2 storey and 3 storey elevations. The internal north elevation continues the exposed ribbed concrete panels that largely feature on the external west elevation. All other courtyard elevations have exposed concrete that is worn and weathered. The only exception is the single storey, windowless internal west elevation that appears to have sandy-grey, stretcher bonded brick.

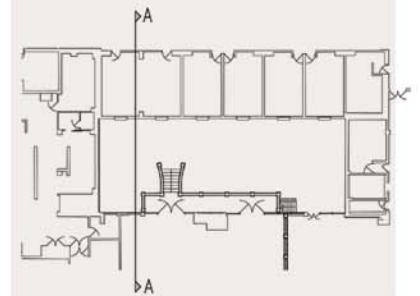


Stairs to basement

COURTYARD EAST ELEVATION
SCALE 1:200 @ A4

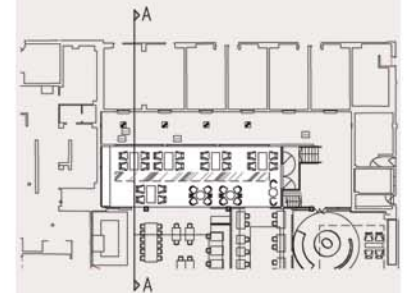
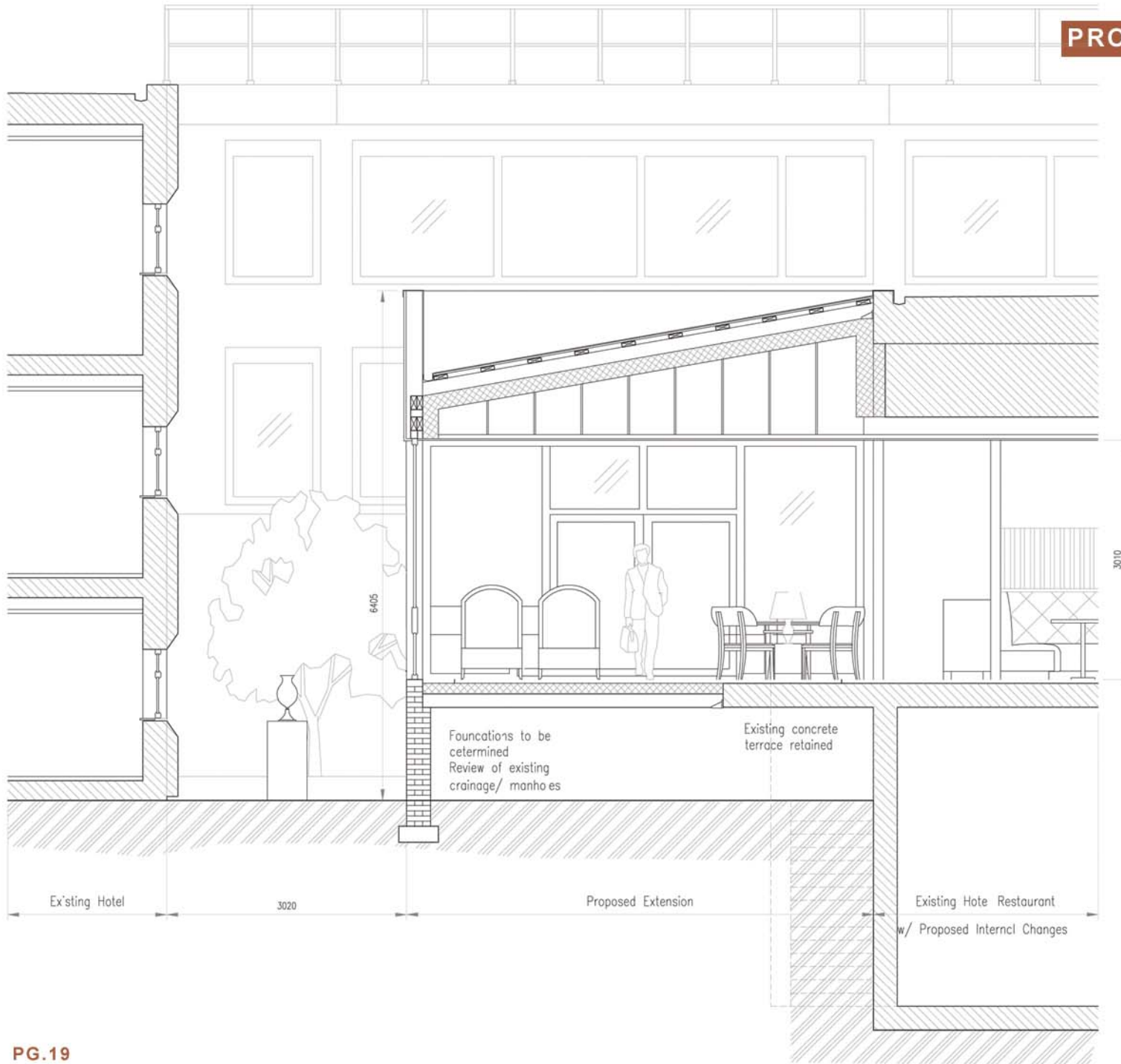


EXISTING - SECTION



SECTION A-A
SCALE 1:100 @ A4

PROPOSED - SECTION



51.26

49.79

SECTION A-A
SCALE 1:100 @ A4

STRATEGIC LEVEL ACCESS

Access issues have been considered to ensure that the proposed ramp and stairs and new restaurant extension will take full account of the needs for the disabled and all relevant statutory regulations as set out in the following chapters below. This statement has been prepared to identify the disability issues directly related to this proposal.

PART M BUILDING REGULATIONS

The building has been designed to be in accordance with Part M (2004) of the building regulations including compliance with section 1 – Access to buildings other than dwellings, whereby the required stair and ramp dimensions will be met.

THE BUILDING GENERALLY

The building/development, including related external works, will be designed and constructed and use materials in compliance with and conformity to the following.

a) The requirements of the local planning authority in respect of the Full Planning Approval including any Conditions.

b) The requirements of the Building Regulations, as they relate to the Developer's Works including any conditions of the Building Control Officer in respect of the Developer's Works.

c) The requirements of the Local Authority, Utility Supply Companies and of competent organisations reporting to the Local Authority, including the Fire Officer requirements, as they relate to the Developer's works.

d) British Standards Specifications and Codes of Practice (except as noted elsewhere by exception). Where revisions, additions and deletions are made to the Standards by the British Standards Institution, the versions of the Standards current at the date of commencement of the construction of the development on site shall be the relevant Standards.

e) Local Authority requirements for disabled access and escape (i.e. direct and level access via all external entrances and internally at each floor level to lifts, accommodation and car parking).

f) Health & Safety Regulations including CDM Regulations.

g) All other relevant codes and standards applicable to such a development.

SITE ACCESS AND SERVICES

The hotel currently requires vehicular access from the single entry off Colnbrook By-Pass. Both guests and service vehicles enter a one way system whereby parking is available to the south and west of the building. A service road continues around the perimeter of the building and eventually leads to the exit which is located off a small turning circle. This turning circle is used to bring vehicles underneath the entrance canopy where guests can be dropped off and picked directly in front of the main hotel entrance. The existing car hire office also use this point as a car pick up and drop off which can often lead to congestion and confusion.

The proposed inclusion of a car hire roadway at the south west corner of the building will allow for a designated location for car hire operations directly in front of the newly located office. A number of small trees will be removed in order for this to be implemented. This new roadway will relieve congestion from the main entrance and the removal of trees will improve visual connection between the car park and entrance.

There are onsite car parking facilities which includes in excess of 281 parking spaces. These will remain under the proposed scheme, with the only alteration being the possible allocation of spaces to the west for car hire vehicles.

At present there are 2 No. disabled parking bays to the east of the main entrance. These are for use by guests and will not be affected by the proposal.

Pedestrians can access the site by walking along the pavement besides Colnbrook By-Pass. Both guests who have entered from this point or have parked in onsite car parking can directly access the main entrance via the pedestrian crossing located along the turning circle.

ACCESS FOR THE DISABLED

At present the external layout does not permit independent access to the raised public areas that are 1m above ground. The proposed ramp at the south west corner of the building will allow for disabled access to the newly located car hire office as well as an entrance into the hotel via the bar area. Proposed internal alterations have included for a dedicated lowered reception desk for disabled and wheelchair bound guests and visitors to the hotel. The access to the upper deck area currently uses a wheelchair ramp adjacent to the existing stairs across from reception.

This facility is to be retained to allow both independent and assisted access to the upper areas of the ground floor. In addition the proposed ramps to the exterior of the hotel will permit assisted entrance to the bar and car hire if necessary.

Parking facilities will be retained as currently exist with 2 No. disabled parking bays adjacent to the main entrance.

ENTRANCE DOORS

The main entrance to the hotel is at ground level where the entry doors lead into a reception lobby. There are two sets of stairs adjacent to the reception, but no ramped access to the 1m raised public areas.

Entrance to the proposed car hire office and bar at the raised 1m level will have a minimum clear width of 1000mm and will be easy to use for those in wheelchairs, with buggies or carrying luggage. Staff access will largely be via the main entrance with a proposed internal ramp that will permit access to the raised public areas.

INTERNAL DOORS

All new doors and doorways will be sized in accordance with Part M of the building regs to achieve the minimum clear opening width to allow a wheelchair to pass. In the case of doors with glazed vision panels these will be provided in accordance with Part M.

INTERNAL AND EXTERNAL HORIZONTAL CIRCULATION

The minimum width of the proposed ramp is to be 1500mm, allowing for clear access for wheelchairs to pass and a 1800mm wide section at the half landing allows for changes direction.

In the proposed restaurant, passageways between furniture within the public area will be maintained by the management at a minimum of 1000mm.

INTERNAL AND EXTERNAL VERTICAL CIRCULATION

With the requirement to access the car hire office and public areas at 1m above ground, the area around the proposed ramp and stairs is to be raised 200mm. Vertical circulation for wheelchair users will be via the new ramped access at gradients and associated maximum lengths in accordance with Part M.

The proposed external stairs to access the car hire office will be in accordance with Part M with a width of flight of 1200mm, a rise between 150mm and 170mm and a going of 280mm. There will be a maximum number of 6 risers per flight.

FINISHES

Finishes throughout the proposed areas will be selected in order to provide an adequate level of visual contrast between horizontal and vertical surfaces, between doors and the surrounding wall, the door furniture and the door, between electrical switches and other controls and the surrounding wall and between stair nosings and opening and their background. Stair handrails will also be slip resistant and not cold to the touch. Lighting throughout will be designed to give good colour rendering of surfaces without creating glare or strong shadows. Where possible, floor finishes will also contrast in texture to indicate a change of function with the space such as from circulation route to seating area.

FIRE STRATEGY

The hotel has been designed with reference to the following legislation: The Building Regulations Part B 2004, Fire Precautions Act 1971 (amended), Fire Precautions (Workplace) Regulations 1997, BS 5588 Part 5 2004 Access for Fire Fighting, BS 5588 Part 8 Escape for Disabled People, BS 5529 Part 12 Managing Fire Safety. Fire resistances of components and assemblies, the means of warning, the means of escape and the means of access for the Fire Service will all be in accordance with the above documentation.

MEANS OF ESCAPE

The existing building is served by 2No fire escapes that lead to the front of the building, one being via the ground floor entrance, the other serving the raised 1m public areas. There is an additional escape stair to the west of the public areas.

The new extension will have 2No fire escapes, one via the main public areas corridor and out to the front of the building, the other being via the internal courtyard which passes the reception and out through the main entrance. Ramped access between the two exits will allow for change in direction and use of alternative means of escape.

The proposed car hire office will have one means of escape via the proposed ramp and stairs that lead to the front of the building.

EMERGENCY ALARM

Emergency alarms throughout the hotel will consist of an integrated detector sounder unit, some with an inbuilt flashing beacon in the event of the emergency alarm being activated. These integrated units will be fitted in the guestrooms and corridors and in the public areas and the back of house.

RESEARCH, RISK ASSESSMENT AND CONSULTATION

The hotel operator is committed to the policy of obtaining and collating information from first hand experiences of the guests, especially guests with special needs, and guest surveys are undertaken on a regular basis and the results analysed. In addition there will be a formal procedure that reception staff will adhere to for the recording and monitoring of guest comments and comments that are relevant to the provision of facilities or services to guests with special needs will be acted on immediately. Consultation will also take place on a wider scale with local interest groups or the organisations which represent them with meetings arranged between the hotel management and user groups who will be able to view and comment on the facilities and services.

CONCLUSIONS

The Strategic Access Statement will be further developed as the scheme design, including the interior designers input into the development of specific details, become available and the operational policy for the completed hotel is developed by the operator. Further development of the Access Statement in association with the operator will take into consideration the relevant provisions of the Disability Discrimination Act and Building Regulations with regard to the provisions for those with special needs.



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